

A Draft Documentary History of
Industries along the E.T. & W.N.C. and Clinchfield Railroads through Johnson City

Part 6:
Industries Along the E.T. & W.N.C. and the Clinchfield near the Carnegie Addition,
1882 – Present

By William C. Schneck, Jr. (Revised 24 September 2024)

1. Introduction

This is a draft documentary history of the industries along the Clinchfield spur through the Carnegie Addition. At present, I am planning to produce a total of seven documentary histories on trackside industries in and around Johnson City. For the present document, I have attempted to collect relevant documents and place them in roughly chronological order for each geographic location. Maps and figures of the exterior of the buildings for each location are generally grouped together and presented first, while figures of the interior and advertisements are generally integrated with the text and are presented second. There are some facilities addressed by this documentary history that may be of particular interest to modelers: the Watauga Tannery, the Southern Bending Company, the Wilder block, the Hotel Carnegie, and the Carnegie Furnace.

Changes to the text are marked “Revised” (with date) and additions are marked “Added” (with date). Based on input from Chris Ford, corrections were made to the captions of some of the photographs of the Carnegie Furnace. Some advertising for the Paty Lumber Company, the Johnson City hosiery mills, the General Shale Corporation, and the Empire Furniture Company have also been added. Minor changes, such as the corrections of typographical errors, are not annotated.

Regrettably, I have failed to find much information on some of these industries. Perhaps more can be accomplished on this portion of the subject. I invite other members of the Historical Society to add any missing material to this document and discuss/correct any deficient interpretations of the information at hand (particularly the dating and interpretation of the photographs). For those who wish to research the documents and photographs further, I have provided links, when available, to expedite such attempts. I wish to thank Chris Ford for his assistance and encouragement to bring this document into a presentable state.

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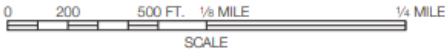
Map 1. The Western Side of the Carnegie Addition of Johnson City.¹

ET&WNC Railroad

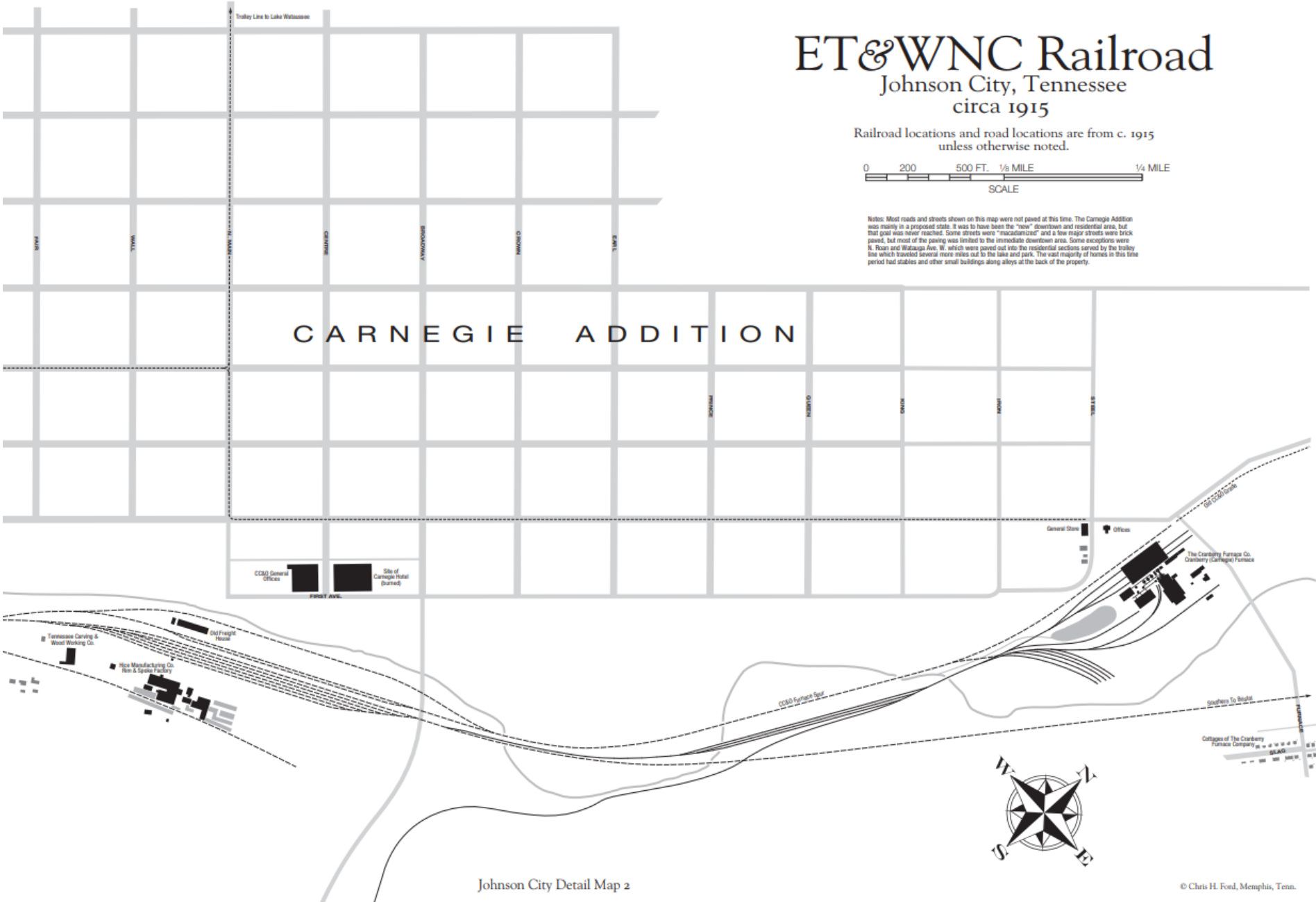
Johnson City, Tennessee

circa 1915

Railroad locations and road locations are from c. 1915 unless otherwise noted.



Notes: Most roads and streets shown on this map were not paved at this time. The Carnegie Addition was mainly in a proposed state. It was to have been the "new" downtown and residential area, but that goal was never reached. Some streets were "macadamized" and a few major streets were brick paved, but most of the paving was limited to the immediate downtown area. Some exceptions were H. Ryan and Watsuga Ave. W, which were paved out into the residential sections served by the trolley line which traveled several more miles out to the lake and park. The vast majority of houses in this time period had stables and other small buildings along alleys at the back of the property.



Johnson City Detail Map 2

© Chris H. Ford, Memphis, Tenn.

Map 2. The Eastern Side of the Carnegie Addition of Johnson City.²

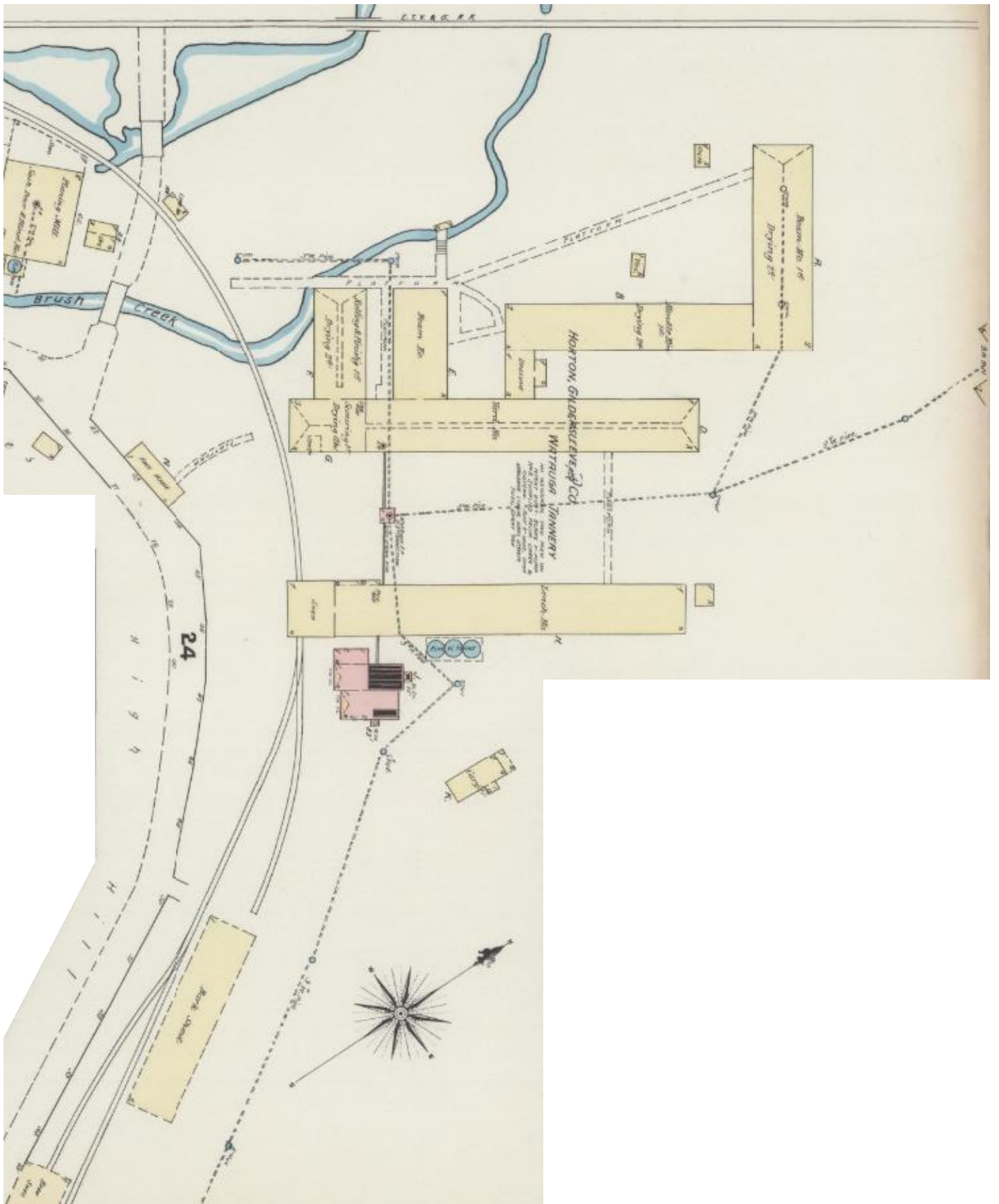
2. The Area Occupied by the Watauga Tannery and Related Enterprises, 1882 – Present

Horton, Yocum, and Company Tannery (Watauga Tannery), 1882 – 1889
Horton, Gildersleeve and Company, Watauga Tannery, 1889 to at least 1897
United States Leather Company, Watauga Tannery, 1900? to at least 1913
Union Tanning Company, Watauga Tannery and Johnson City Extract Company,
1915 to at least 1922 and perhaps 1925
Watauga Tannery, The United States Leather Company, 1927 to at least 1931?

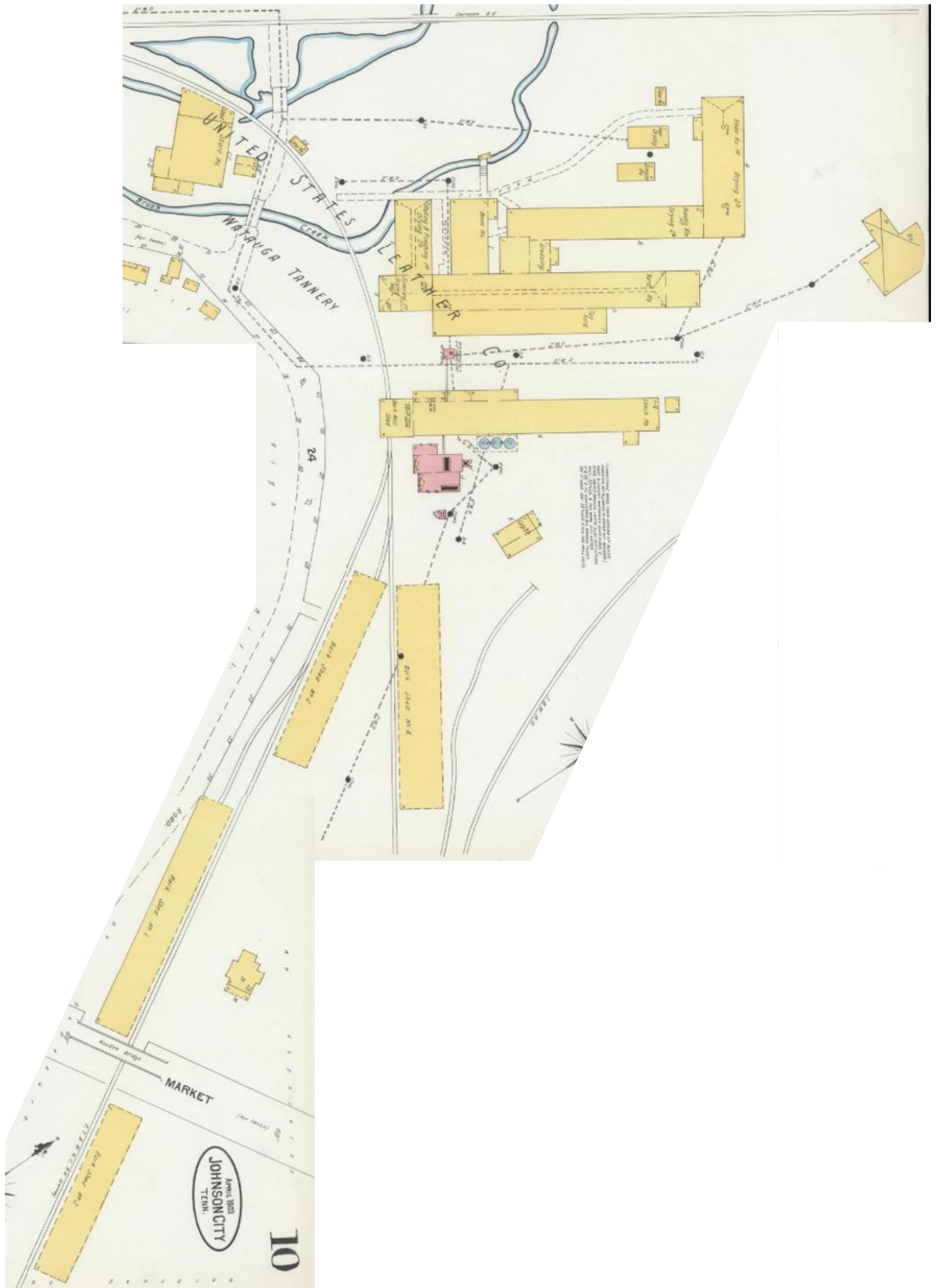
Paty Lumber Company, 1932 to at least 1998

The Watauga Tannery was served by the E.T. & V./E.T.V. & G./Southern since its construction; the E.T. & W.N.C. had trackage to it by 1891, and the South & Western/Clinchfield had trackage to it by 1903.

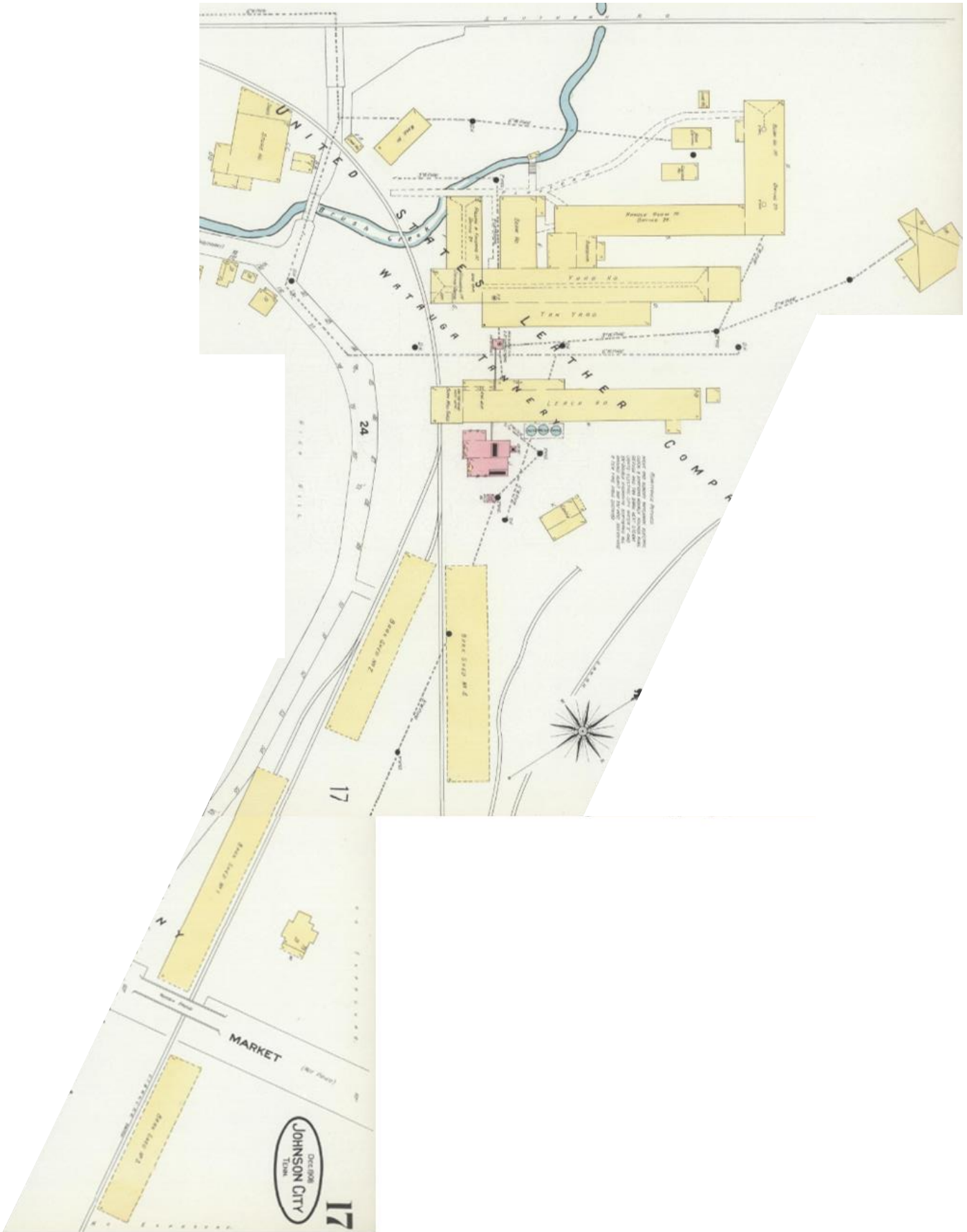
From 1932 to at least 1998, the Paty Lumber Company, at the intersection of East Main Street and Division Street, occupied a portion of the area covered by the Watauga Tannery. The area occupied by the tannery is currently occupied by S.R.M. Concrete, the Johnson City Purchasing Department, and Johnson City Solid Waste Services.



Map 3. "Horton, Gildersleeve and Co.," "Watauga Tannery" in January 1891.³



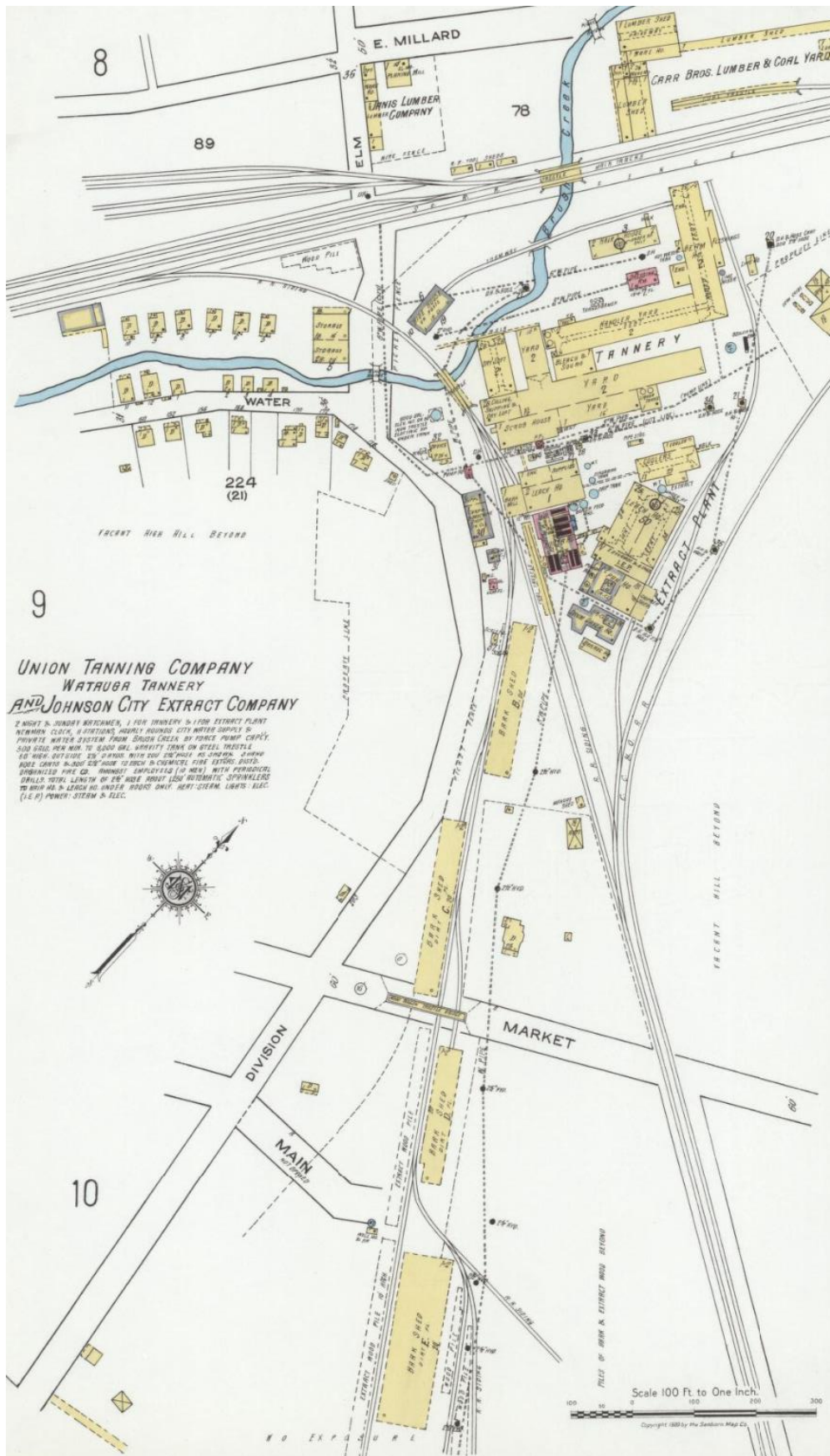
Map 5. The "United States Leather Co.," "Watauga Tannery," in April 1903.⁵



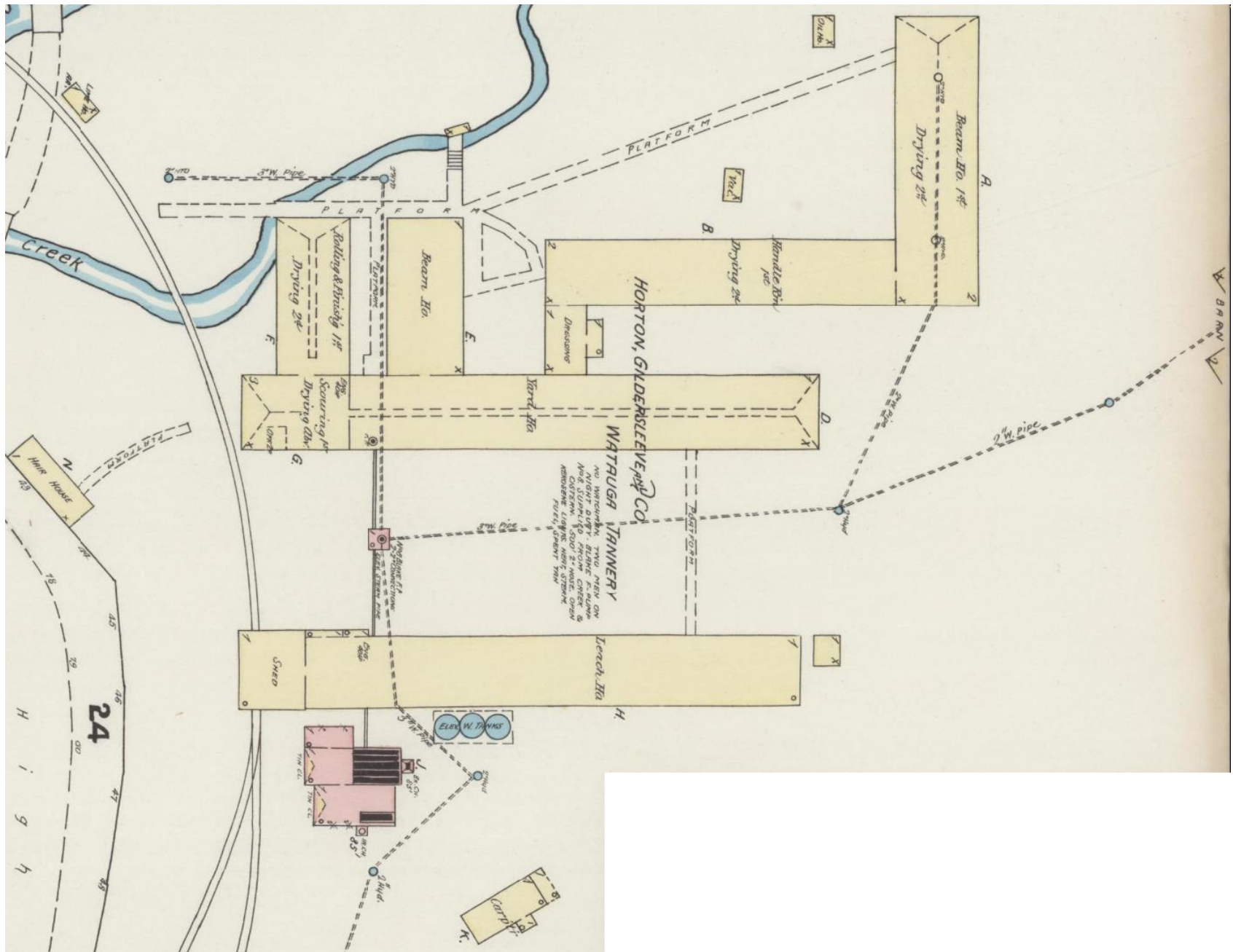
Map 6. The “United States Leather Company”, “Watauga Tannery”, in December 1908.⁶



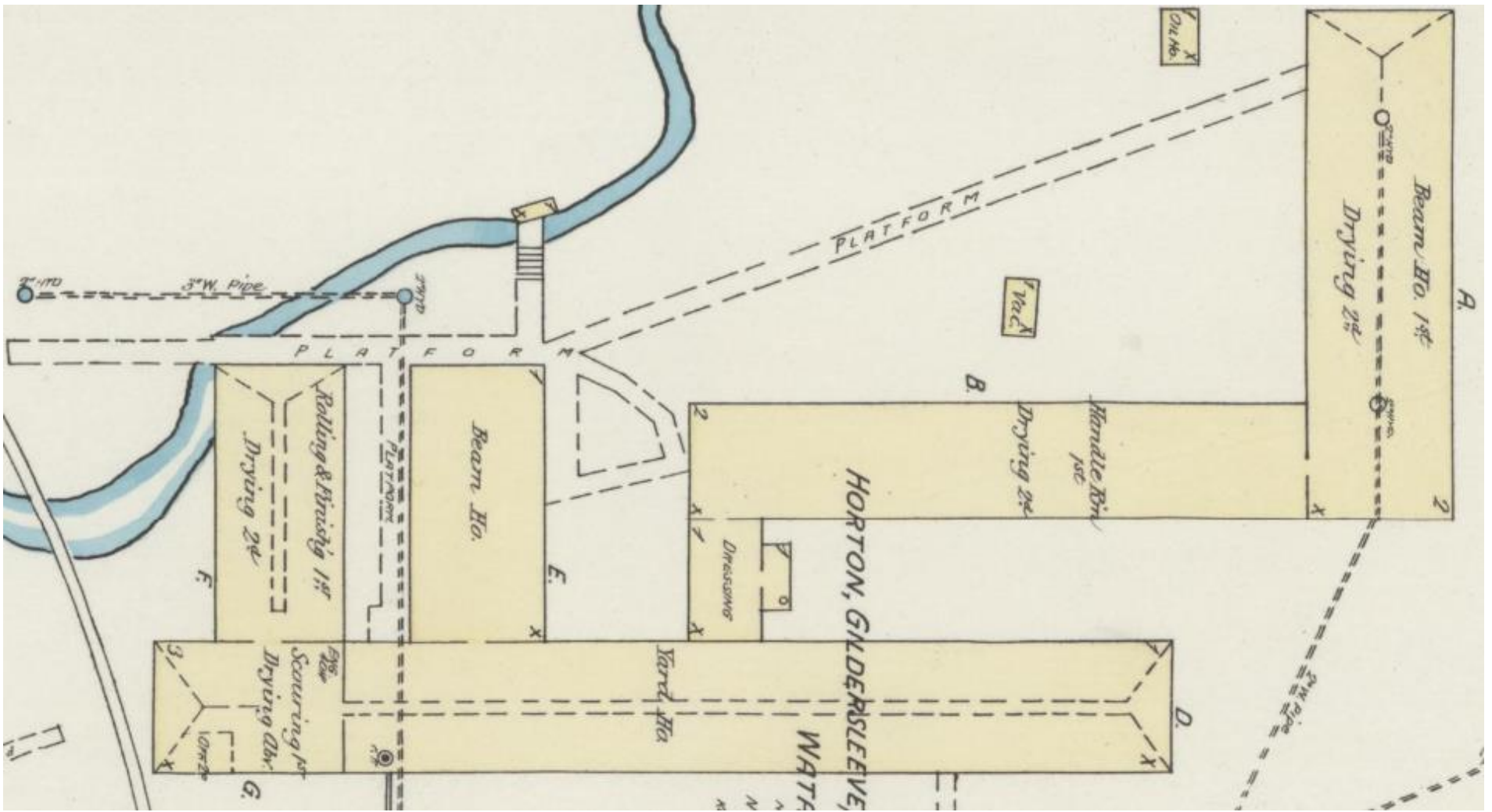
Map 7. The “United States Leather Co.”, “Watauga Tannery”, in July 1913.⁷



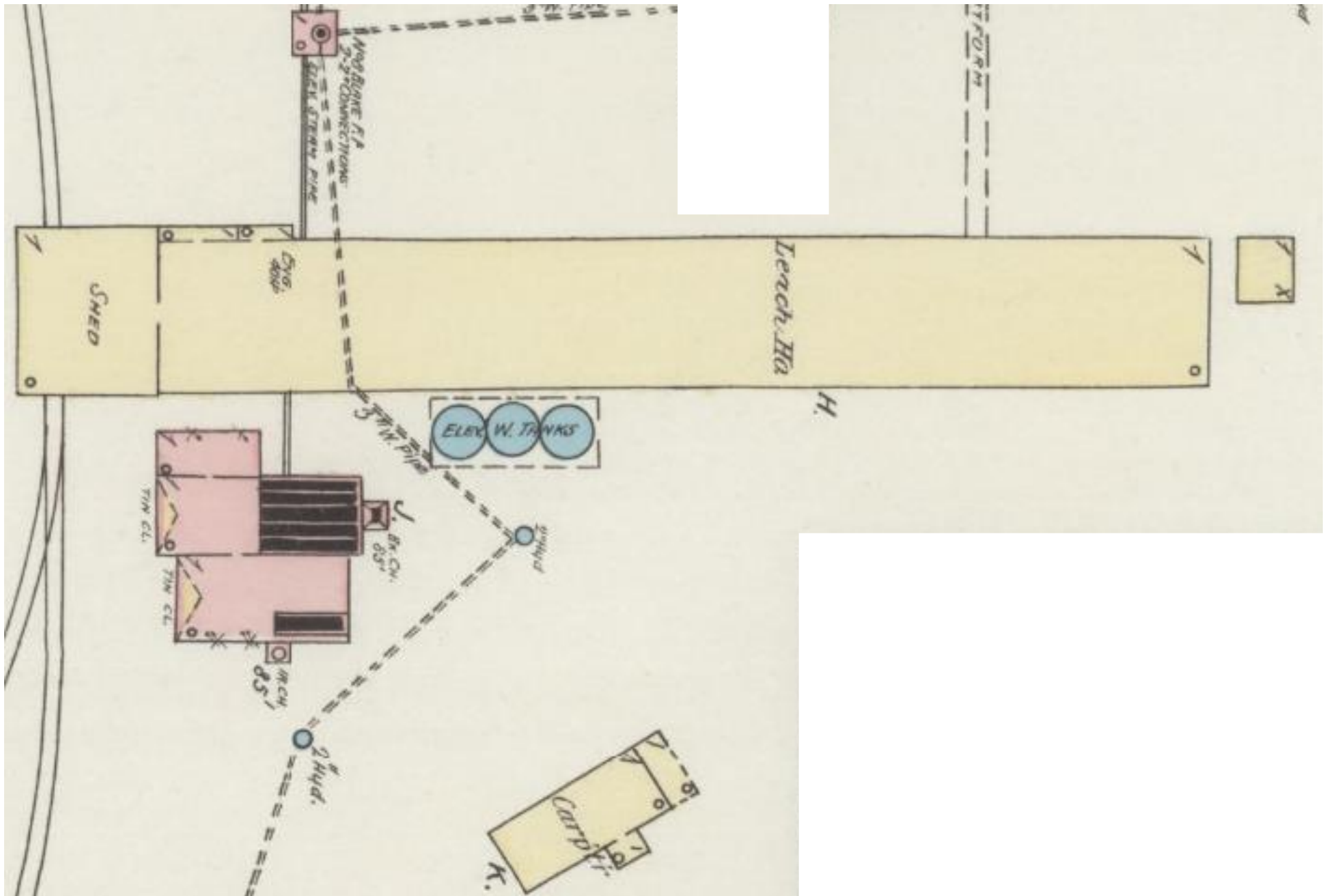
Map 8. The “Union Tanning Company, Watauga Tannery and Johnson City Extract Company,” in February 1920.⁸



Map 3A. A detail of the Watauga Tannery, January 1891, depicting the “Yard Ho.,” “Beam” houses, and the “Leach Ho.”



Map 3B. Another detail of the Watauga Tannery, in January 1891, depicting the “Yard Ho.” and the “Beam” houses. “Beam house work involved the preparation of hides for tanning, including the removal of hair from one side of the hide and flesh from the other side of the hide.”⁹



Map 3C. Another detail of the Watauga Tannery, in January 1891, depicting the “Leach Ho.” and the brick boiler house (which was tin clad).

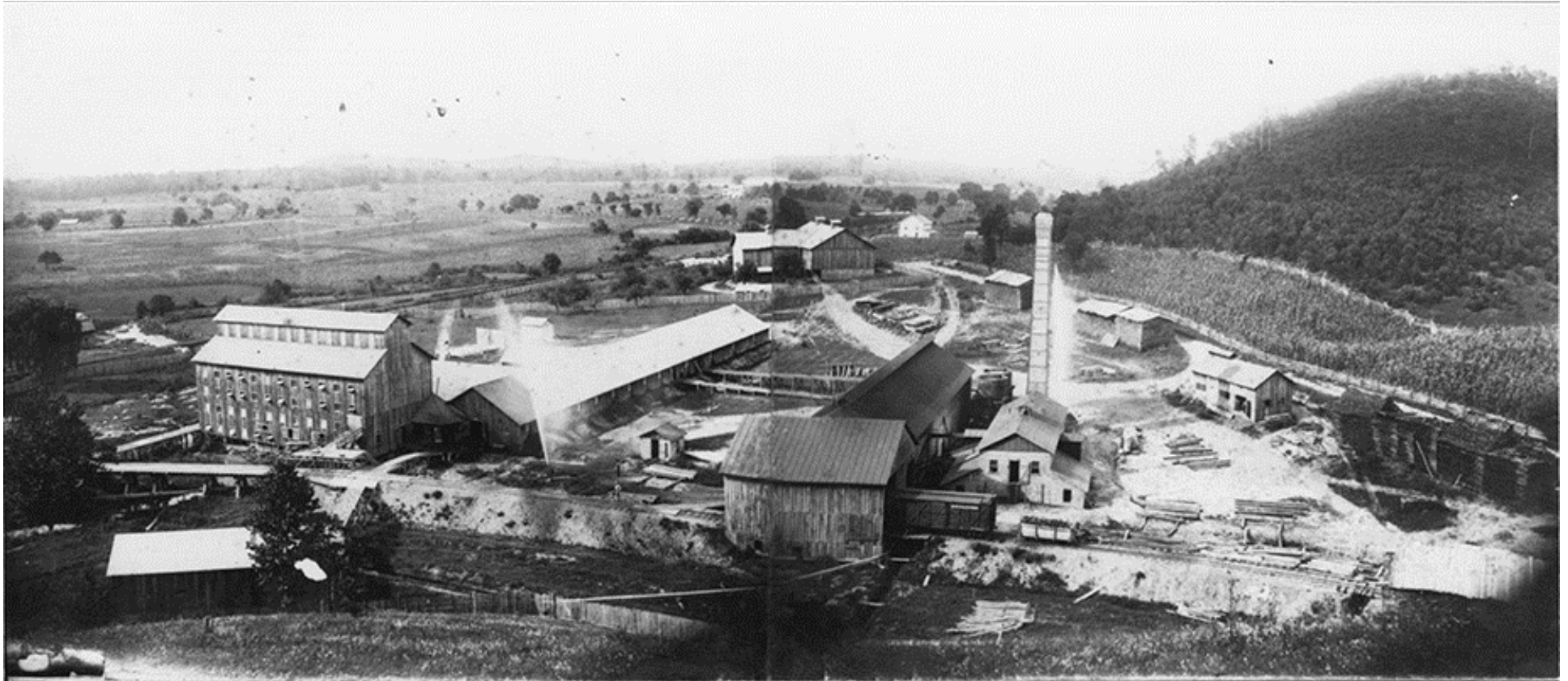


Figure 1. "Horton, Yokum, and Company Tannery (Watauga Tannery), Established November 1883 at Water and Division Streets".¹⁰ This photograph appears to predate 1891, see Map 3A.



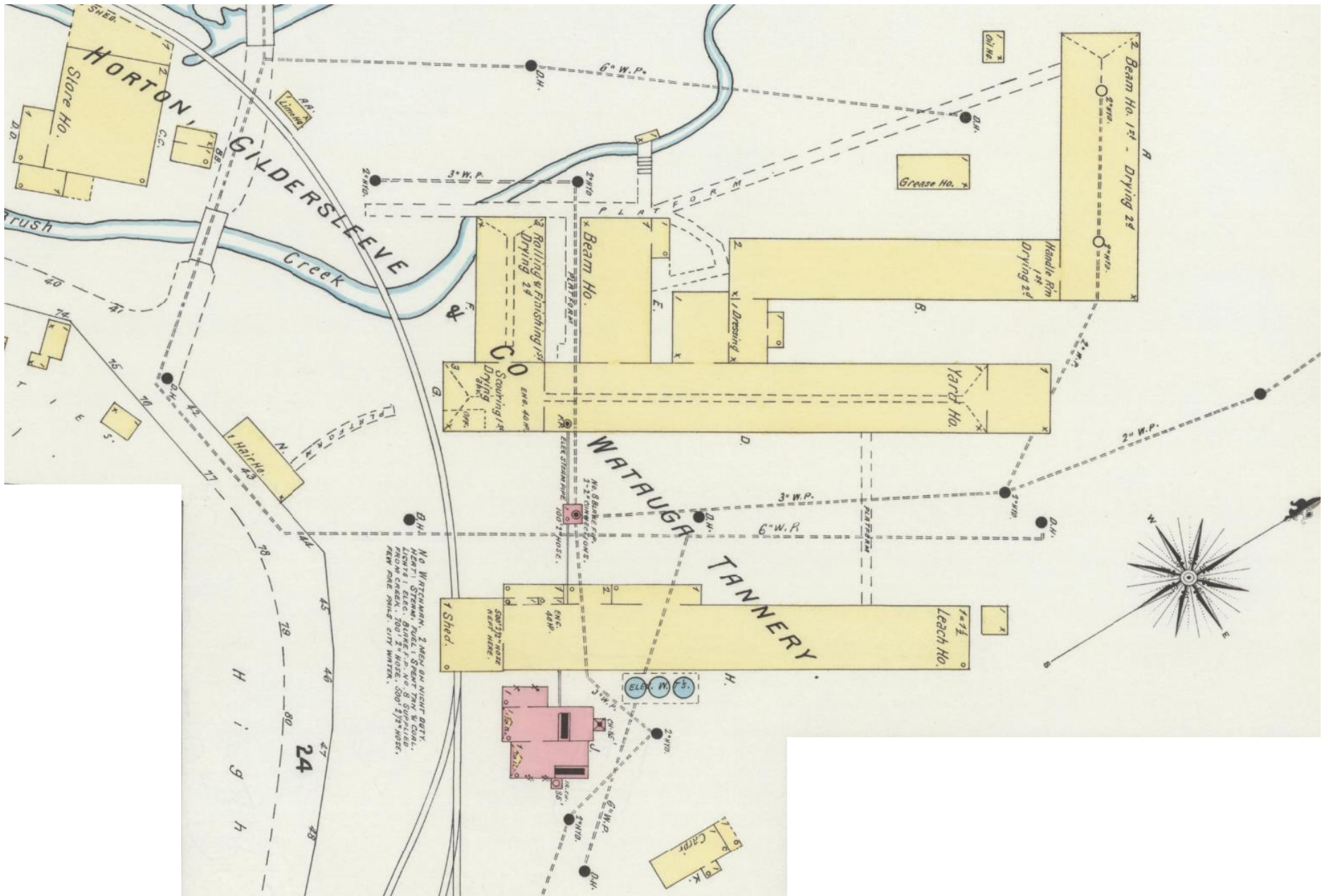
Figure 1A. A detail of the Watauga Tannery. The building in the left foreground appears to be the “Hair House” (labeled “N” on Map 3A), which has a platform leading across the railroad to the multi-story building (labeled “F”). This multi-story building had “Rolling & Finish’g” on the “1st” floor and “Drying” on the “2d”. The long building (labeled “D”), to the right of the multi-story building, appears to be the “Yard Ho.” The roof of the “Beam Ho.” (labeled “E”) appears between the multi-story building and the Yard Ho.” The small building to the right of the “Yard Ho.” appears to be the brick one, which is not labeled on Map 3A. The mainline of the E.T.V. & G. railroad is also visible behind the multi-story building. Note that the buildings labeled “A”, “B”, and “G” on Map 3A, dated January 1891, should be visible in this photograph, but are not present; consequently, this photograph appears to predate 1891.



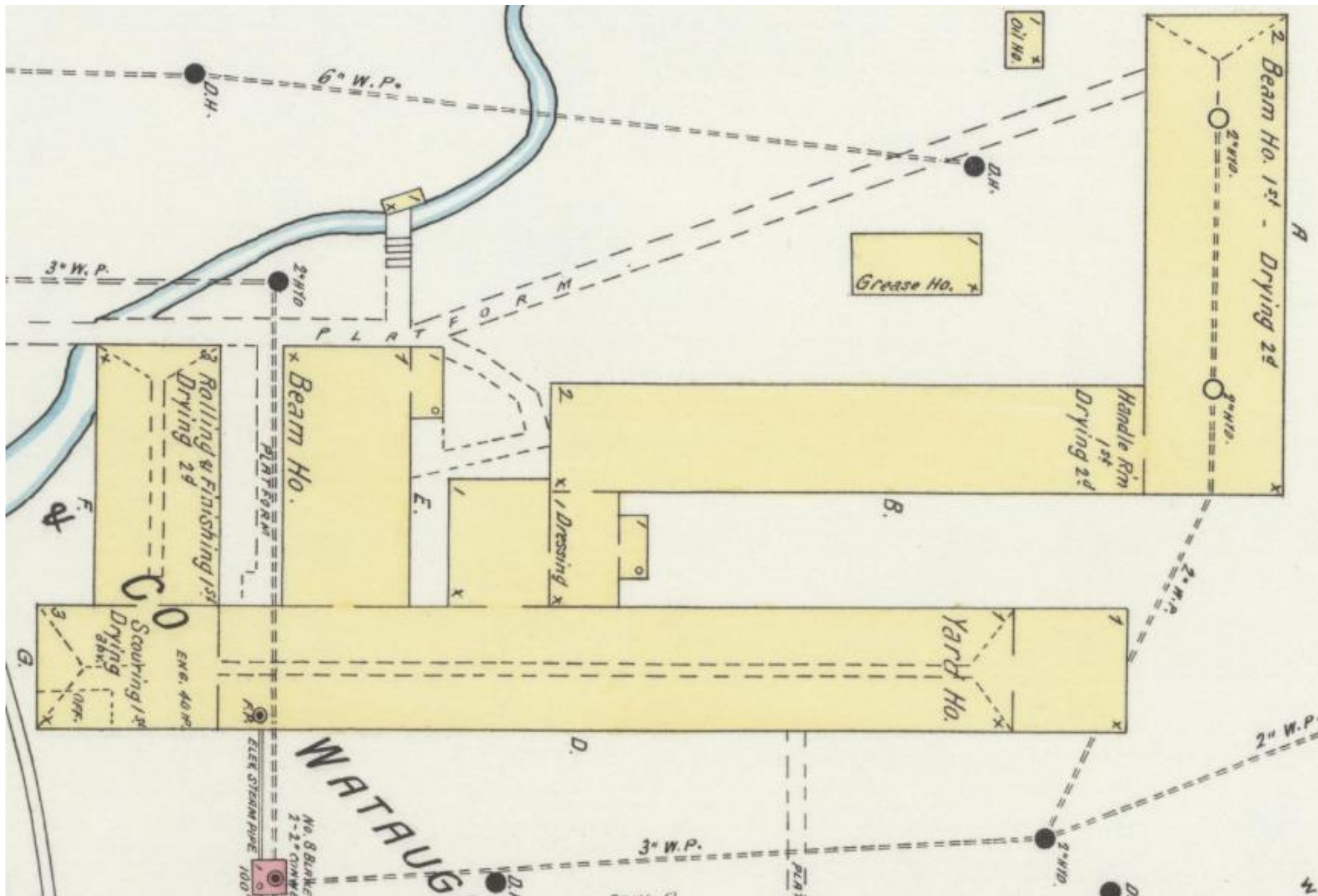
Figure 1B. Another detail of the Watauga Tannery. The building, in the left foreground, with the railroad track through it, appears to be the “Leach Ho.” (labeled “H” on Map 3A), note that it does not appear to be as long as it was at the time the map was made and also note that only two (not three) elevated water tanks are present in this photograph, to the right of the “Leach Ho.” The building to the immediate right of the “Leach Ho.” appears to be a boiler house (labeled “J”), which had an 85’ high brick chimney, note that Map 3C indicates that this building has been changed since this photograph was taken. “These buildings were probably painted barn red, like most tanneries.”¹¹ The light-colored building to the right of the boiler house appears to be the “Carp’t’r” building (labeled “K”). The barn in the left background is partially depicted on Map 3A, but the buildings between it and the “Carp’t’r” building are not, perhaps indicating that they had been removed by January 1891.



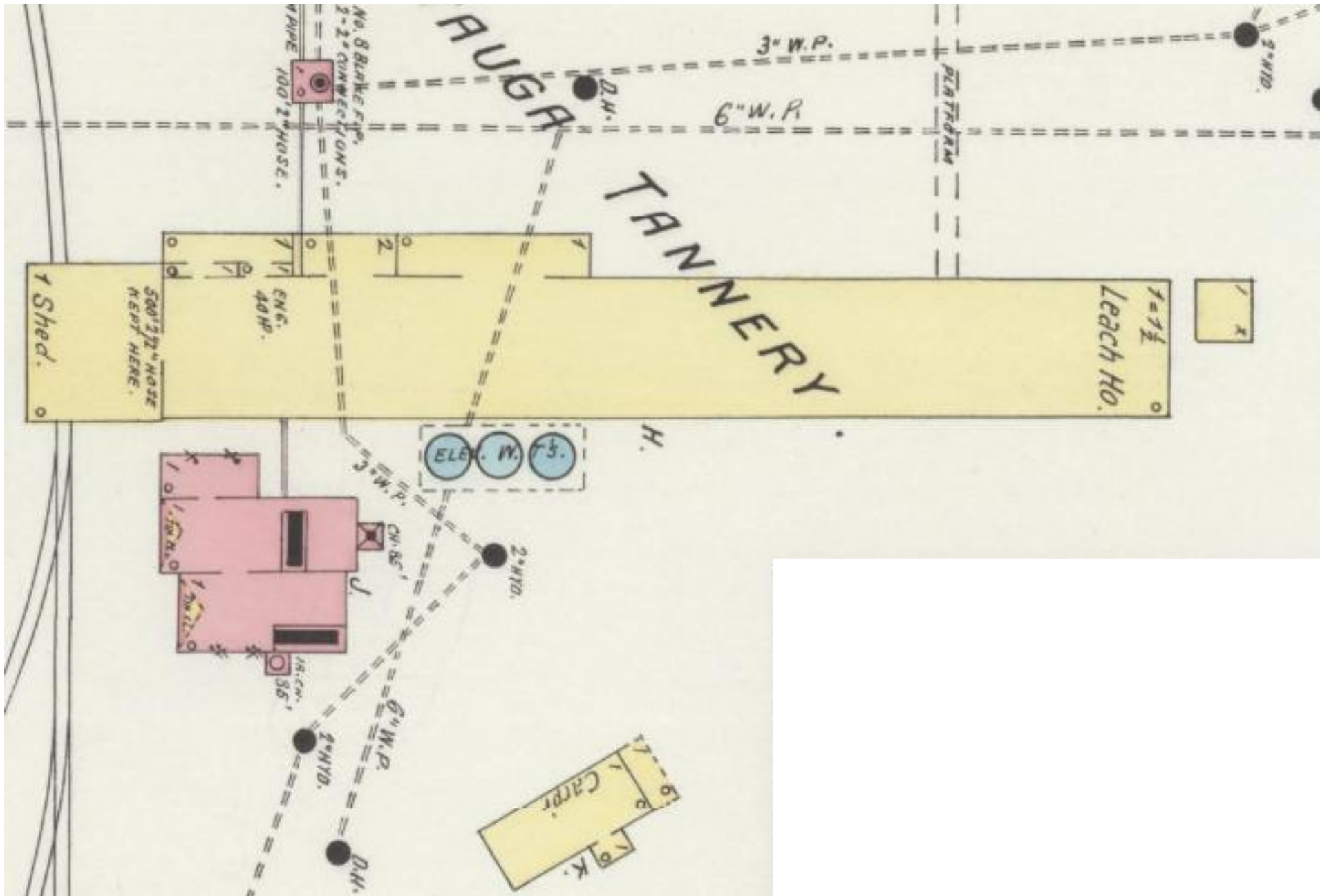
Figure 2. “Old Tannery Bridge and the old Gildersleeve home. On this site has now been erected J.E. Green Company Office Building.”¹² According to the Johnson City Comet, construction on the Gildersleeve residence began by 11 July 1889, while this bridge, over the E.T. & W.N.C. tannery spur, was completed by 26 February 1891. Note the top of the structure designated “Bark Shed No.1” (according to the Sanborn maps of 1897, 1903, and 1908) and later “Bark Shed C” (according to the Sanborn maps of 1913 and 1920) to the left of the bridge. Also note that the bridge appears to have two lanes.



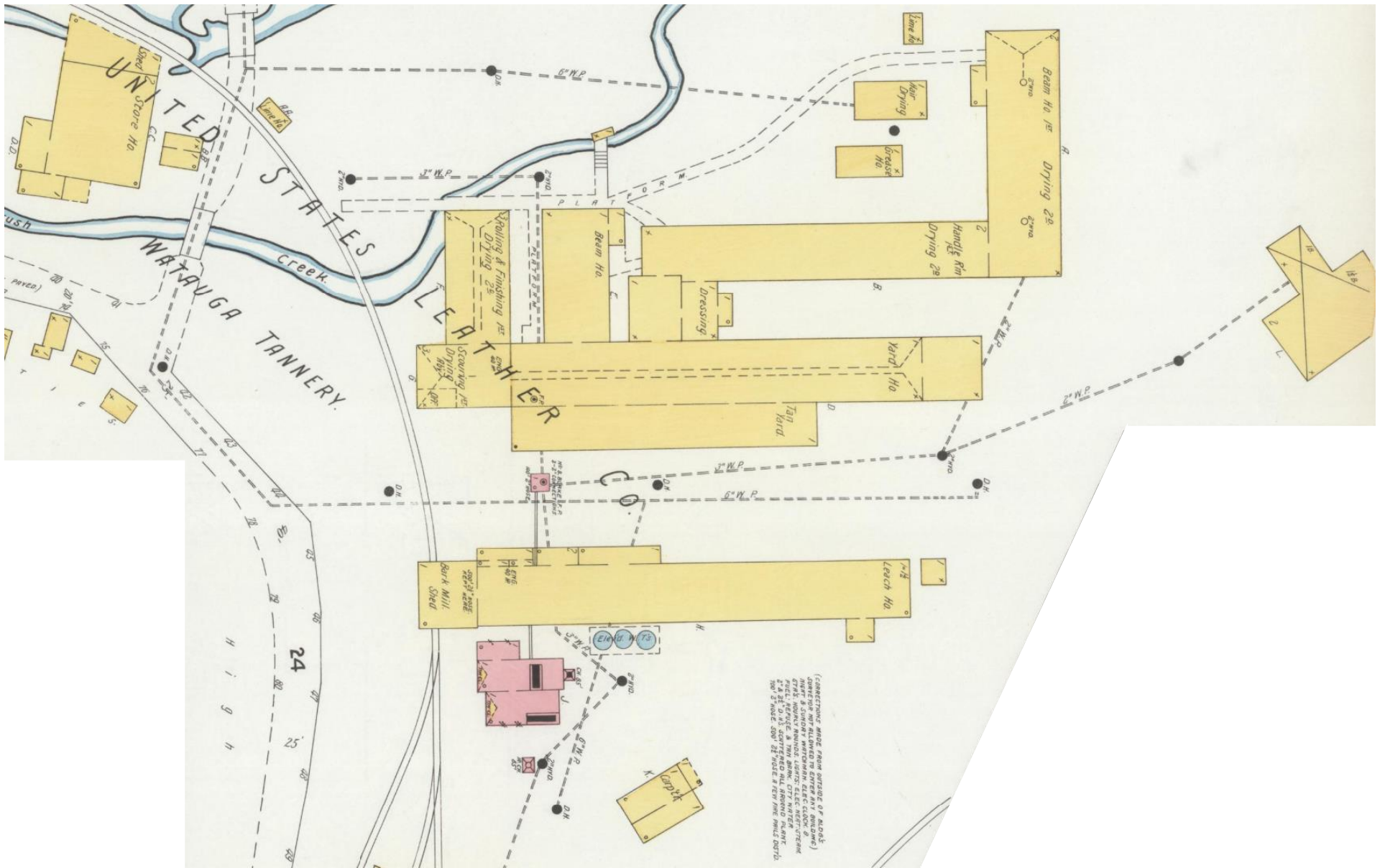
Map 4A. A detail of the Watauga Tannery, May 1897, depicting the “Yard Ho.,” “Beam” houses, “Store Ho.,” and the “Leach Ho.” Note the “Store Ho.” in the top left of this image, which was acquired from the Watauga Lumber Company, see Map 3.



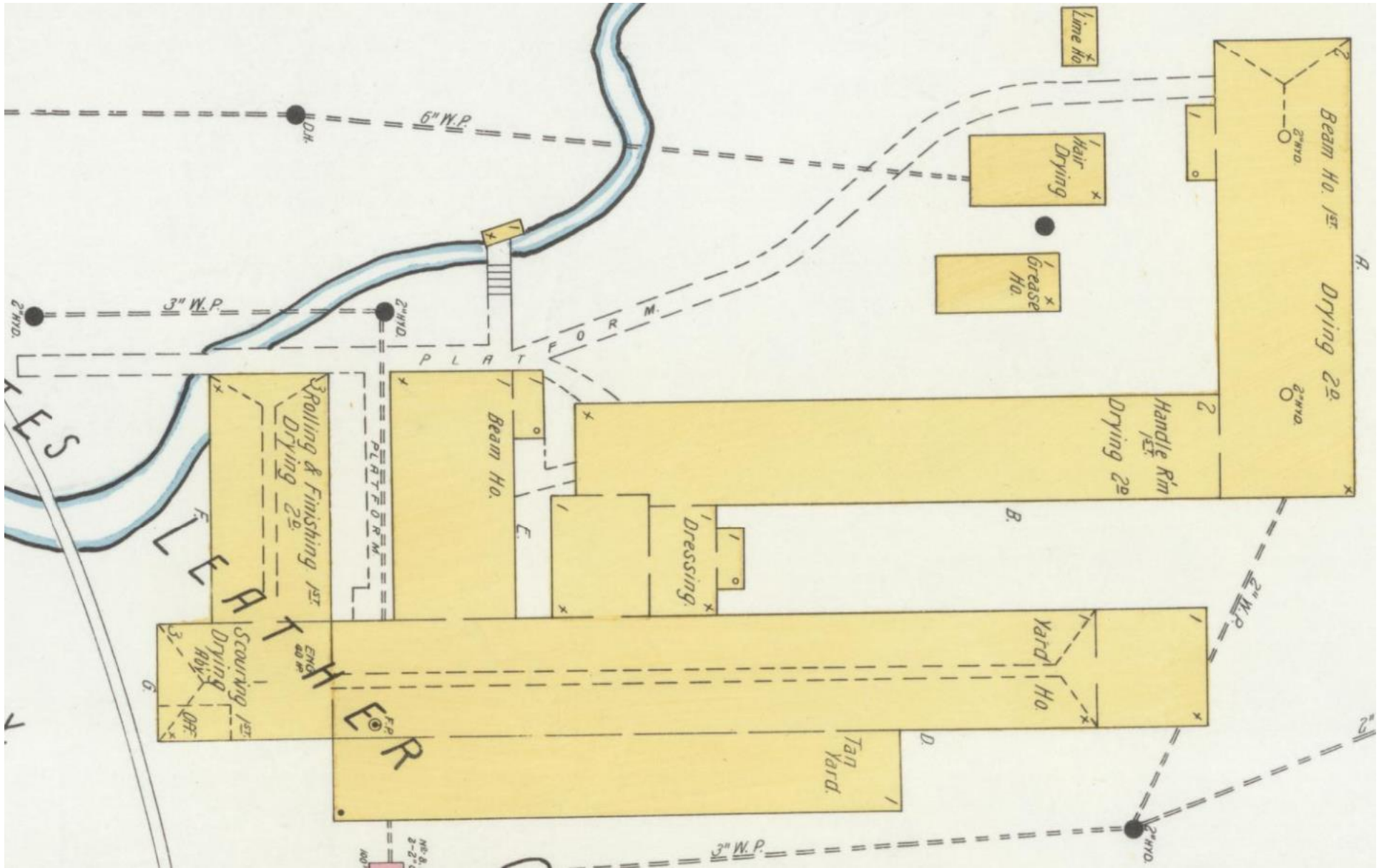
Map 4B. Another detail of the Watauga Tannery, in May 1897, of the “Yard Ho.” and the “Beam” houses. Note the addition of a “Grease Ho.”



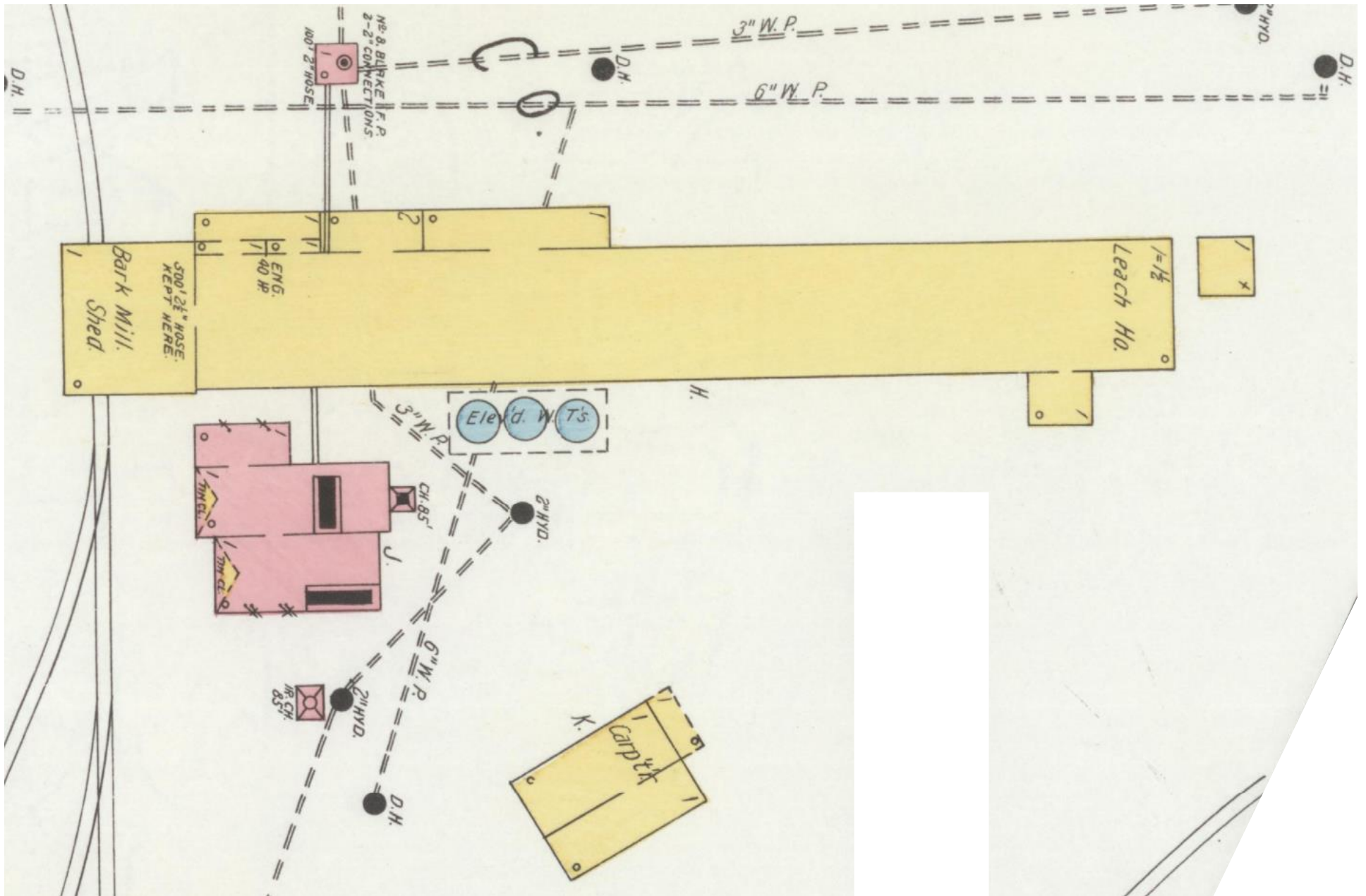
Map 4C. Another detail of the Watauga Tannery, in May 1897, of the “Leach Ho.” and the brick boiler house (which was tin clad).



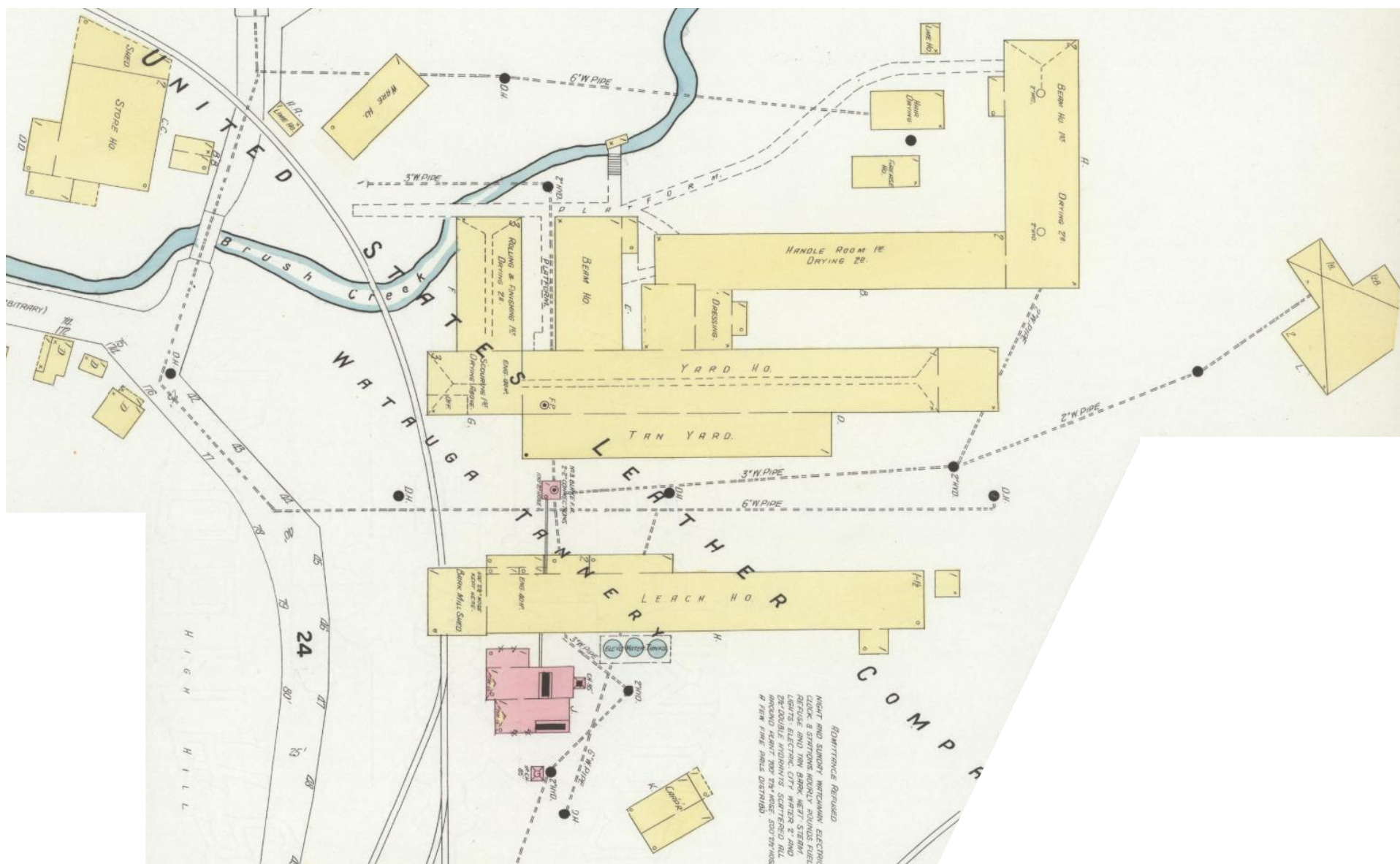
Map 5A. A detail of the Watauga Tannery, April 1903, depicting the “Yard Ho.,” “Beam” houses, “Store Ho.,” and the “Leach Ho.” Note that the “Hair Ho.,” which was located to the left (west) of the Yard Ho.” and “Leach Ho.” has been removed.

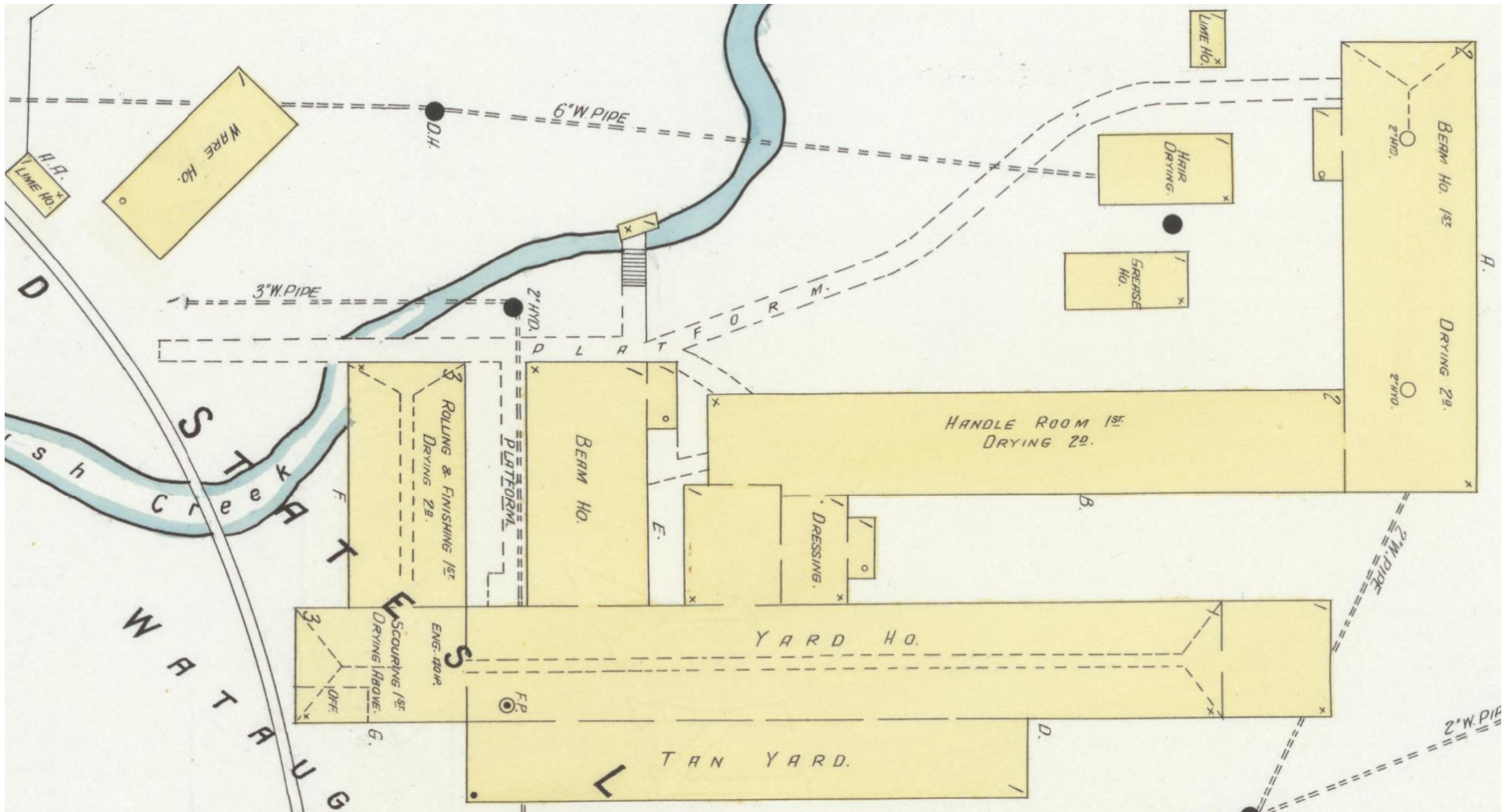


Map 5B. Another detail of the Watauga Tannery, in April 1903, of the “Yard Ho.” and the “Beam” houses. Note that a “Hair Drying” building has been added, and that a “Tan Yard” had been added to the Yard Ho.” Also note that the “Oil Ho.” has been converted into a “Lime Ho.”

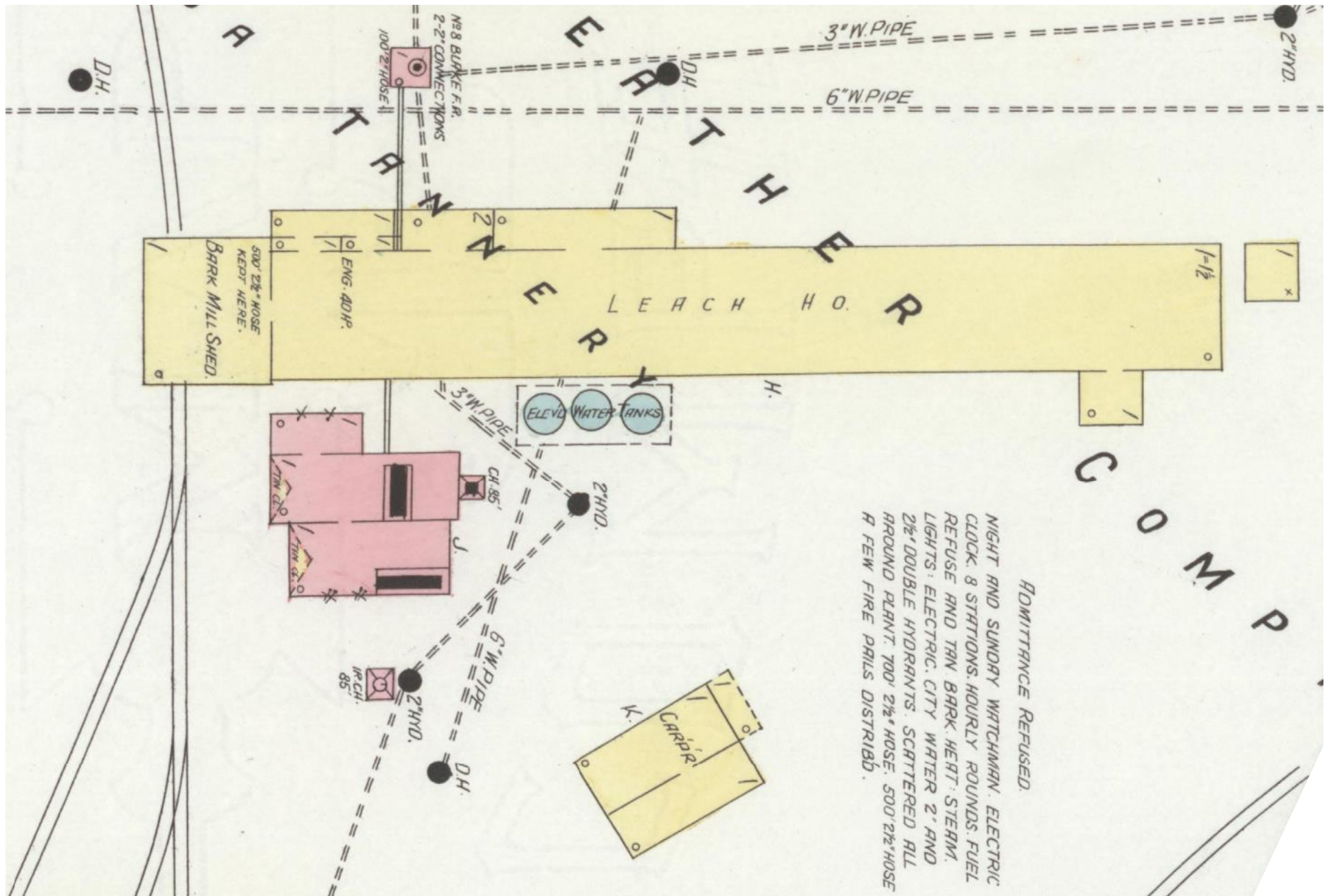


Map 5C. Another detail of the Watauga Tannery, in April 1903, of the “Leach Ho.” and the brick boiler house (which was tin clad). Note that the shed over the railroad tracks has been relabeled “Bark Mill. Shed.”

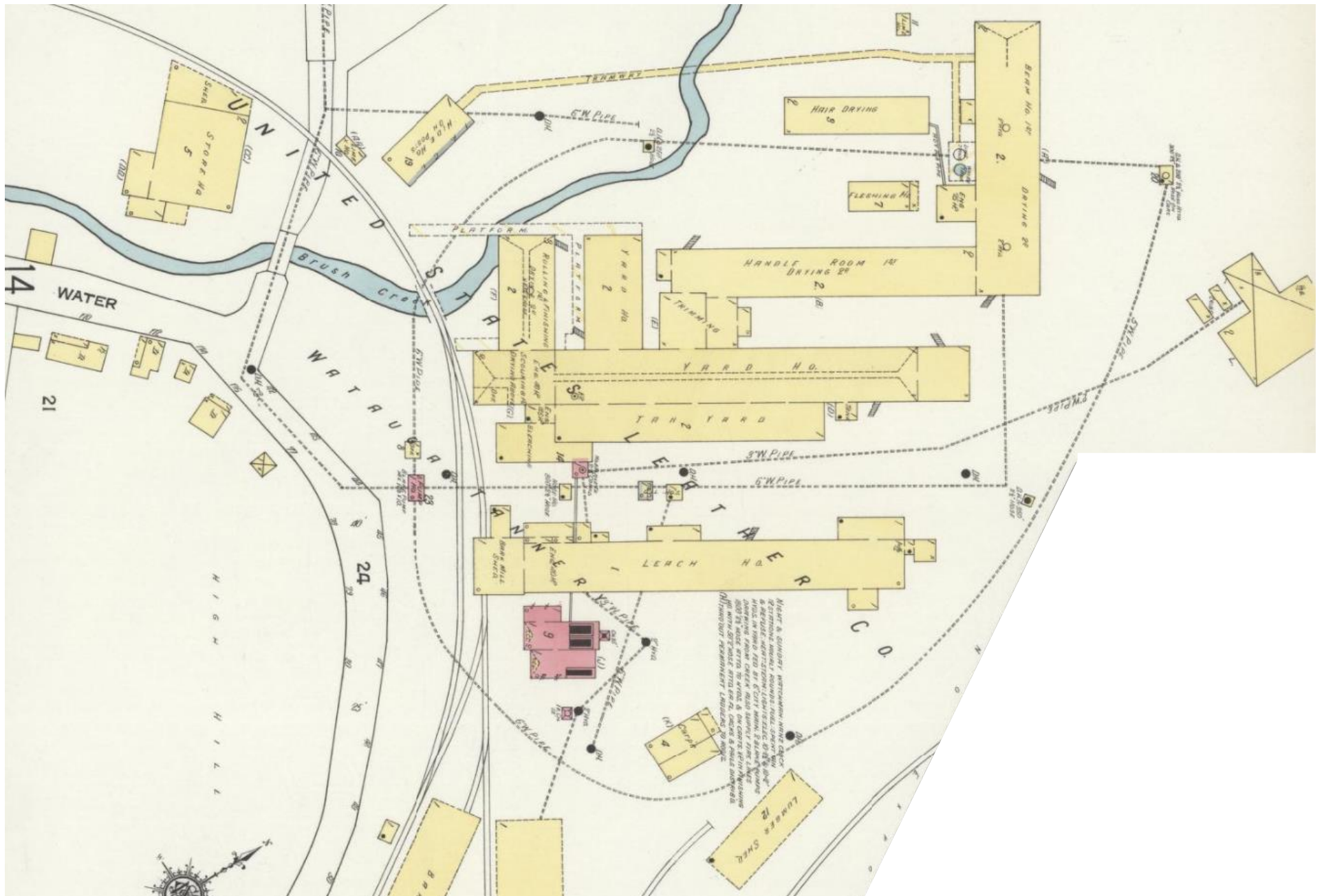




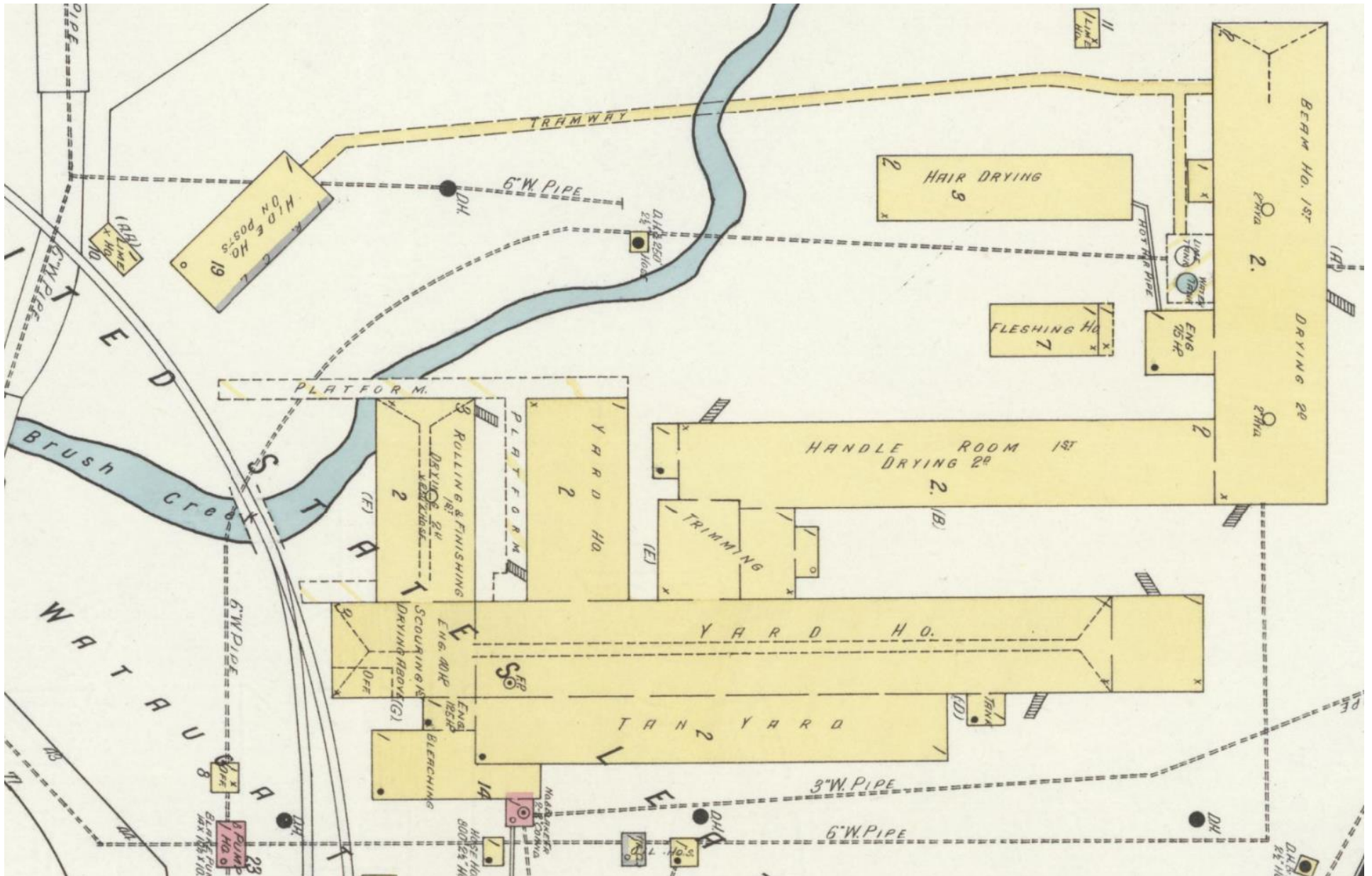
Map 6B. Another detail of the Watauga Tannery, in December 1908, depicting the “Yard Ho.,” a “Ware Ho.,” and the “Beam” houses. Note the addition a “Ware Ho.” in the top left of this image.



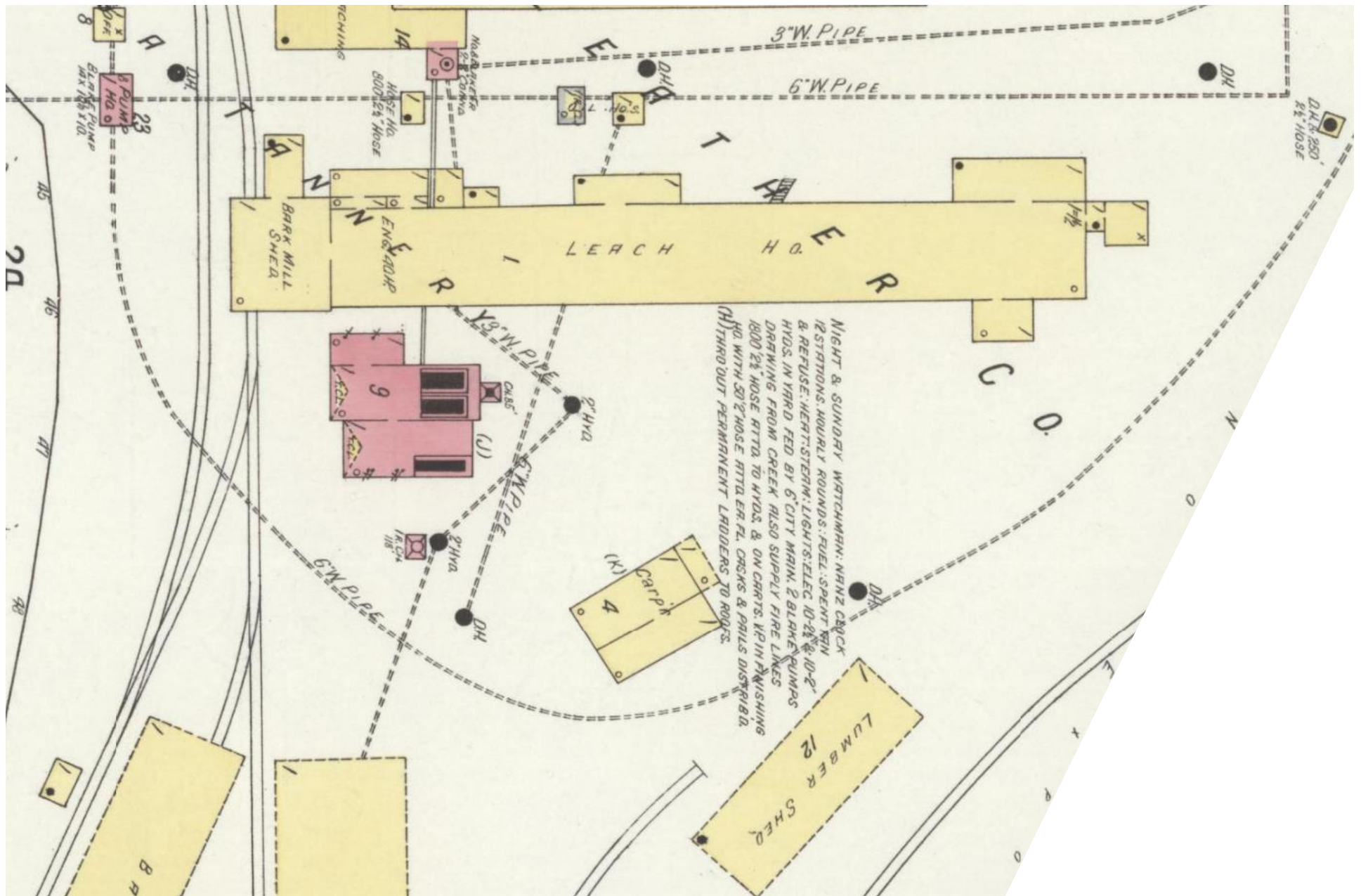
Map 6C. Another detail of the Watauga Tannery, in December 1908, of the "Leach Ho." and the brick boiler house (which was tin clad).



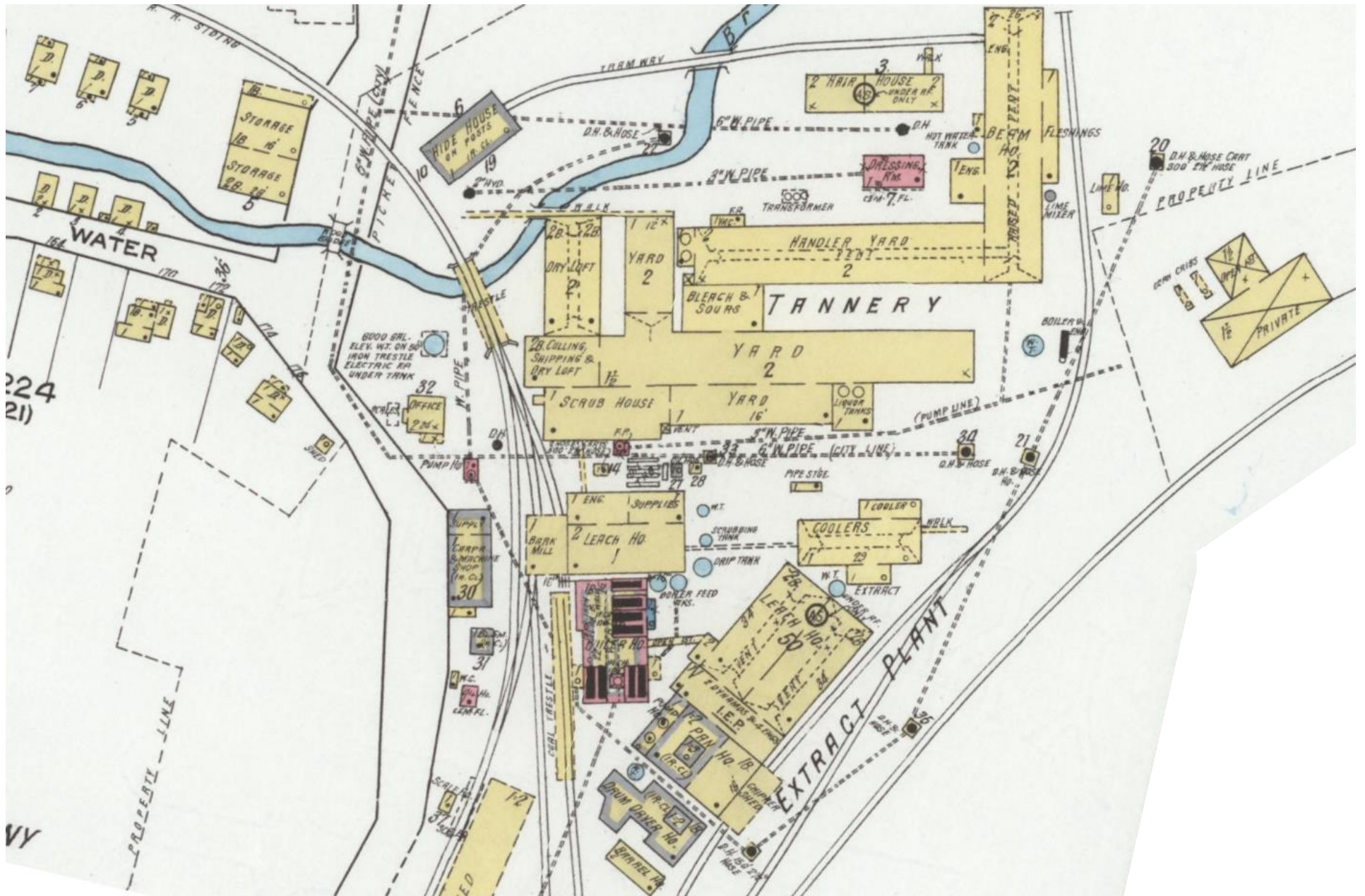
Map 7A. A detail of the Watauga Tannery, July 1913, of the “Yard Ho.”, the “Beam Ho.”, the “Store Ho.”, and the “Leach Ho.”



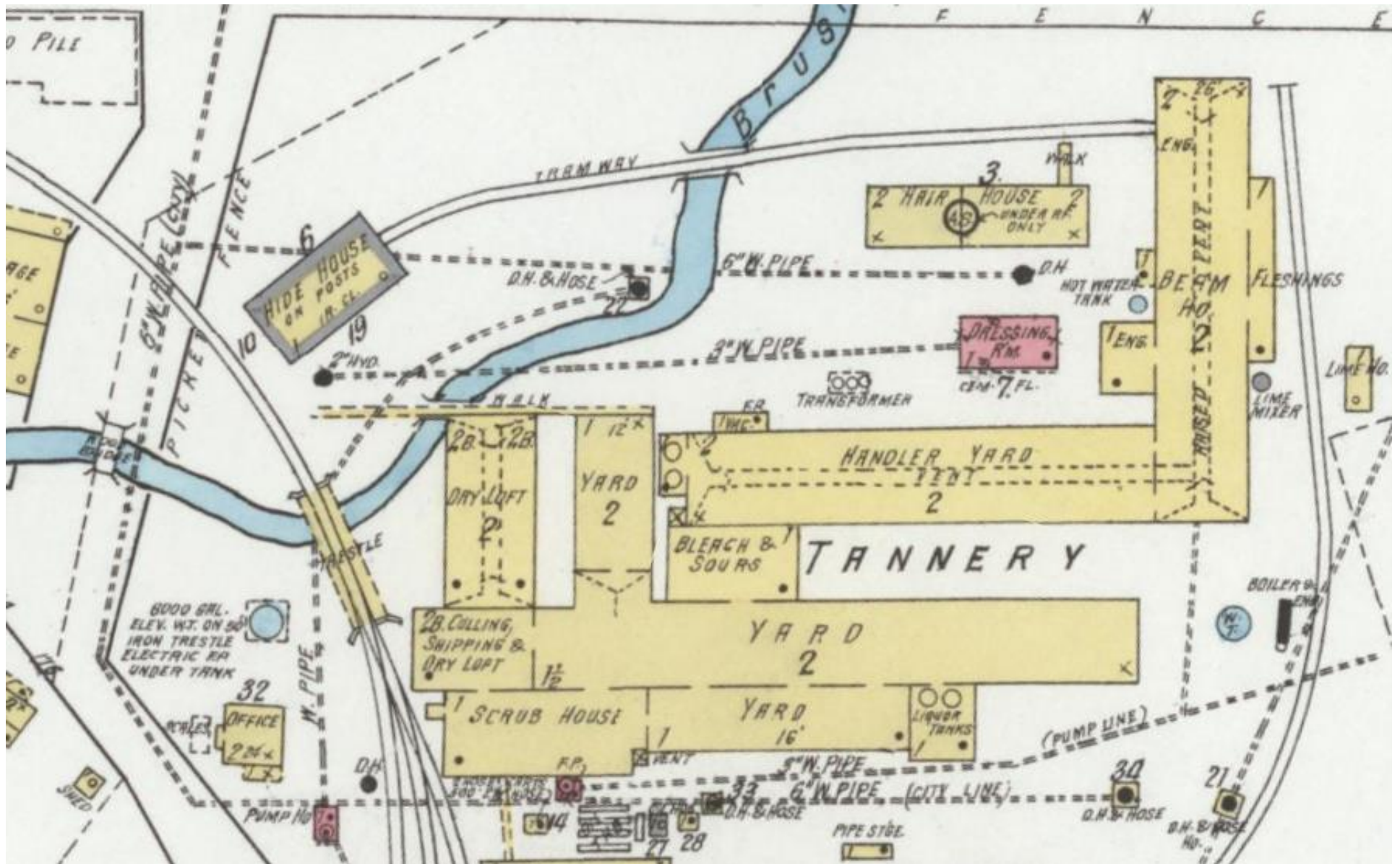
Map 7B. A detail of the Watauga Tannery, July 1913, depicting the “Yard Ho.,” the “Beam Ho.,” and the “Hide Ho.” Note that the “Ware Ho.” in the top left of this image, has been converted into a “Hide Ho.” and been partially iron clad, while a tramway now connects it with the “Beam Ho.” Also note that the “Grease Ho.” has been converted into a “Fleshing Ho.,” as well as other changes near the “Bream Ho.”



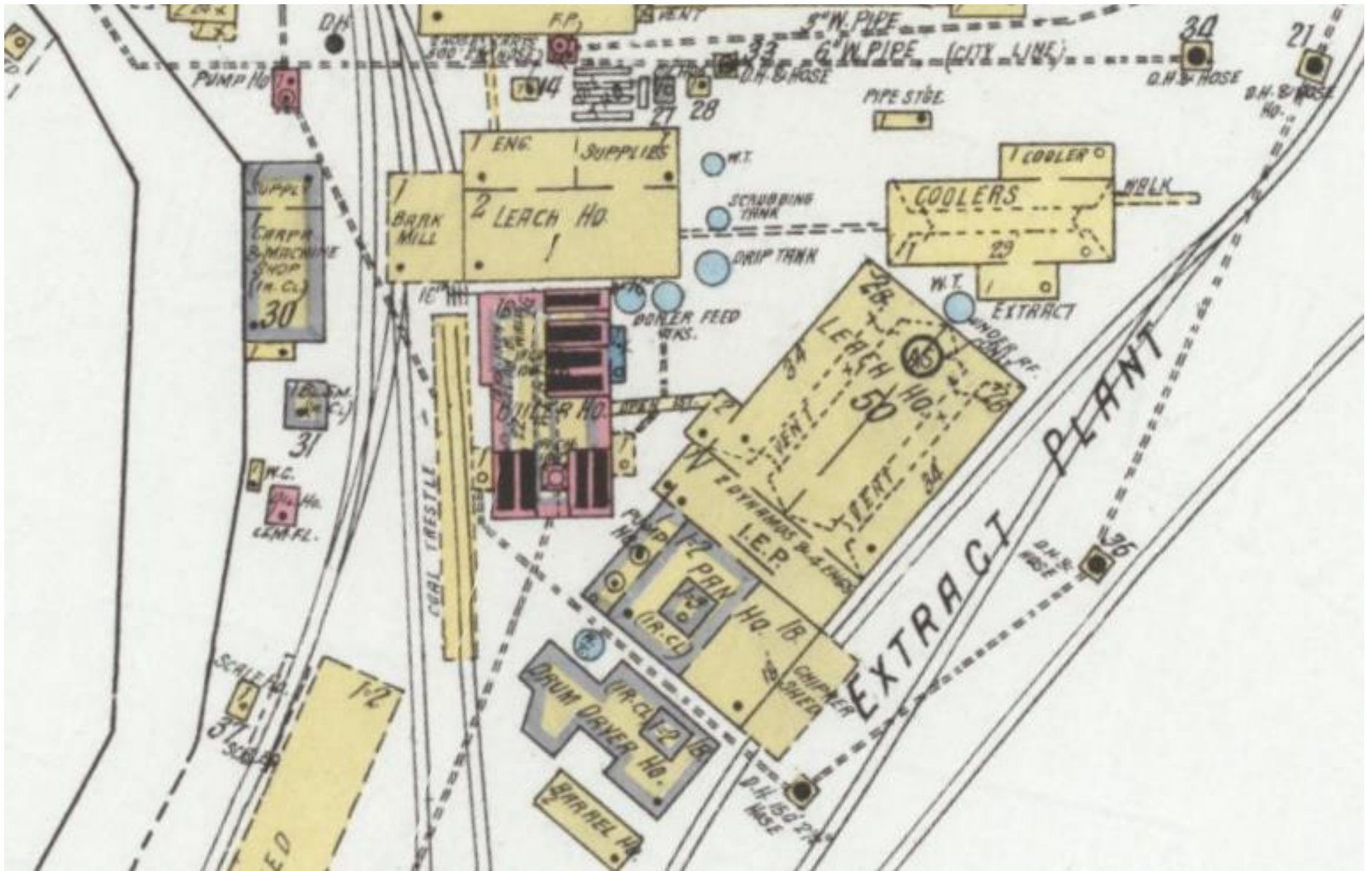
Map 7C. Another detail of the Watauga Tannery, in July 1913, depicting the “Leach Ho.” and the brick boiler house (which was tin clad). Note that the three “Elev’d Water Tanks” have been removed and that a “Lumber Shed” has been added.



Map 8A. A detail of the Watauga Tannery and the Johnson City Extract Company, February 1920, of the Yard building, the “Beam Ho.,” the “Store Ho.,” the “Leach Ho.,” the “Boiler Ho.,” and the new extract plant.



Map 8B. Another detail of the Watauga Tannery, February 1920, of the former “Yard Ho.,” and surrounding buildings. Note that the “Lime Ho.” near the “Bream Ho.” has been moved to the east side of the building. Also note that the “Fleshing Ho.” has become a brick building that is used as a “Dressing Rm.” and that a transformer has been added nearby. Finally, note that the office, on the east side of this image, has been expanded and a 6000 gallon water tank on a 50 foot iron trestle has been added, along with numerous other changes in and around the yard house.



Map 8C. Another detail of the Watauga Tannery that includes the “Leach Ho.,” “Carpr. & Machine Shop,” “Coolers,” and “Boiler Ho.,” as well as the Extract Plant that includes the “Leach Ho.,” “Pan Ho.,” “Drum Dryer Ho.,” and “Barrel Ho.,” in February 1920. This entire area has been significantly changed since 1913, see Map 7A.



Figure 3. “View of Johnson City, looking north from Tannery Knob (hill above current city hall)”. Note the Southern Railway Freight Depot behind the freight cars on the left and the “Store Ho.” of the Watauga Tannery on the right. This Southern Railway Freight Depot was apparently built in 1913, or perhaps, a little later, see Map 1. Consequently, this photograph appears to date to 1913 or later.¹³



Figure 3A. A detail of the “Store Ho.,” which was depicted on Sanborn Insurance maps from 1891 to at least 1920. This building initially belonged to the Stratton & Stratton operation, begun by 1887, and renamed the Watauga Lumber Company by 1890. This building had been built by January 1891.¹⁴ The Watauga Tannery purchased this store house by 2 May 1895, according to the Johnson City Comet.



Figure 4. It appears that this photograph, taken by 1904, looks over the American Cigar Box Lumber Company and along Division Street, north across the E.T. & W.N.C. Railroad mainline, toward the Gildersleeve residence (the light-colored building to the right of the water tower) and Bark Shed No. 3 of the Watauga Tannery, on the right, see maps 5 and 6.¹⁵



Figure 5. This view looks southeast over the intersection of Division and East Main streets, toward the Harris Manufacturing Company plant in the background. Note what appears to be “Bark Shed E” with “Extract Wood Pile 10’ High”, along the E.T. & W.N.C. tannery spur, on the left of this image, see Map 8. Given the buildings in the foreground, this photograph appears to date to 1920 or later.¹⁶



Figure 5A. A detail of Bark Shed E. in the left foreground. Note what appears to be trestle work between this bark shed and the Harris Manufacturing Company plant.

Horton, Yocum, and Company Tannery (Watauga Tannery), 1882 – 1889
Horton, Gildersleeve and Company, Watauga Tannery, 1889 to at least 1897
United States Leather Company, Watauga Tannery, 1900? to at least 1913
Union Tanning Company, Watauga Tannery and Johnson City Extract Company,
1915 to at least 1922 and perhaps 1925
Watauga Tannery, The United States Leather Company, 1927 to at least 1931?

Paty Lumber Company, 1932 to at least 1998

See also: “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., which contains technical information on the process of turning raw hides into leather.¹⁷

2.1. Horton, Yocum, and Company Tannery (Watauga Tannery), 1882 – 1889

20 December 1882. Morristown Gazette.

“Dots from Carter. Milligan College, Tenn., Dec. 6. To the Editor of The Morristown Gazette. The thermometer stands at 6° below zero this morning, and we feel a little delicate venturing out after news. Your writer is stationed five miles from Johnson City, on the East Tennessee and Western North Carolina railroad... Johnson City is still building up. An immense tannery is being built there, \$100,000 capital invested. Morristown must wake up, or our City; will soon be the metropolis between Knoxville and Bristol.”

1883. A History of the New York Swamp.

“In 1883, with his son, G. [Gordon] B. Horton and his nephew, Eugene Horton, he [Charles Horton] built the Watauga tannery at Johnson City, Tenn.”¹⁸

18 July 1883. Morristown Gazette.

“From Johnson’s City, Tenn., July 13, To the Editor of the Morristown Gazette... Johnson City is destined to be a manufacturing center. There is being invested at the present time about two hundred and fifty thousand dollars in an oak tannery. The company engaged in the enterprise has purchased 20,000 acres of mountain woodlands, from which they will get the bark they use, paying cash for it. Thousands of dollars have recently been invested in lands in and around the city by capitalists on which to erect large manufacturing establishments.

The citizens have high hopes of the near future of this place. The lumber trade is carried on with great activity. Building and improvements are going on rapidly. The Narrow Gauge railroad which connects this place with the Cranberry Iron Works, N.C., is said to be the most romantic and picturesque route in the South. It is doing a live business in freighting lumber, iron, etc., two trains daily running over it.

Wheat crops are good. Corn crops look well. Fruits and berries are very abundant. Yours truly, L.”

11 October 1883. Jonesborough Herald and Tribune.

“Johnson City’s Progress. Enterprise. A correspondent writing from Cranberry to the Lenoir, (N.C.) Topic, thus speaks of Johnson City:

‘At Johnson City, a town 32 miles over the border in Tennessee at the Junction of the East Tenn., Va. & Ga., and E.T. & W.N.C. railroad, it is thought large manufacturing enterprise will be started. An extensive tannery and stove factory are already up and it is the general opinion that the Cranberry company will establish machine shops for working up much of their pig iron. Johnson City has a future before it.’ R...

The tannery company will probably be ready for operation in about six weeks, the work being pushed rapidly. Johnson City’s Future is a bright one, she is bound to be a manufacturing city as all admit.”

22 November 1883. Jonesborough Herald and Tribune.

“Johnson City. Enterprise, Nov. 17... The sound of the big whistle at the tannery was heard Thursday, for the first time.”

29 November 1883. Jonesborough Herald and Tribune.

“Johnson City’s Big Tannery. Enterprise. The steam tannery is now almost completed. The large engine was fired up last Saturday, and the whistle was heard far and near. The large stack – reaching 65 [should read “85”] feet in the air, is a grand looking piece of mechanism, the work of which was under the supervision of J.J. Adams, brick mason and contractor. It stands as a great recommendation to his mechanical genius. The large four story building as is several of the rest is nearing completion, and the company will soon be ready for the manufacture of leather at an enormous rate. They are erecting a reservoir which will be near, or about the centre of the yard, with which they can throw water over all the building in case of fire.

This is a big thing for East Tennessee and is a great thing for Johnson City, and should be seen to know its exact worth and extent of its capacity; as a description is hard to get at.”

“Johnson City. Enterprise, Nov. 24... Horton, Yocum & Co., have recently walled up, cleaned out and fenced in the big spring, and it now presents a neat appearance. The spring is worth many thousand dollars to our town and we are glad to see it in such good shape.”

13 December 1883. Jonesborough Herald and Tribune.

“Johnson City. Enterprise, Dec. 8... The large steam Tannery is now under headway, using only fifty or seventy-five hides per day.”

20 December 1883. Jonesborough Herald and Tribune.

“Johnson City. Enterprise, Dec. 15...Messrs. Horton, Yocum & Co., the gentlemanly proprietors of the extensive steam tannery at this place, and who are full of enterprise and have a decided interest in the growth and upbuilding of Johnson City, are ‘showing their faith by their works.’ They have donated two acres of ground on the corner near the E.T. & W.N.C. railroad to a Pennsylvania firm for the erection of a spoke and hub manufactory, and for dressing and fitting lumber for most any kind of manufacturing purposes.”

Regrettably, the author has not been able to identify this enterprise.

15 March 1884. Johnson City Comet.

“The Horton, Yocum Tannery recently established in this city is one of the largest in the United States. The company have bought an immense body of land – 20,000 acres or more – for the chestnut oak timber. And every spring they give employment to hundreds of laborers, at splendid wages, in peeling bark for tanning purposes, besides keeping a large number of men employed the year round at the tannery. Col. Yocum the chief manager of the business was a Colonel in the Federal army and is an ex-Congressman from Pennsylvania. Mr. Gildersleeve who is also connected with the enterprise, is a genial gentleman, and Johnson City and East Tenn. may well be proud of such men as citizens. Let Tennessee rejoice when such men come among us. Let us have more Hortons, more Yocums, more Pardees, more Grants and more Matsons, with their capital and push and snap and grit, to develop and utilize the wonderful resources of wealth, which lie sleeping here and thus to make ‘The beautiful land of mountains’ blossom like the rose.”¹⁹

17 May 1884. Johnson City Comet.

“Tan Bark. – Messrs. Horton, Yocum & Co. have about two hundred hands engaged in peeling bark on the Trigg farm in Sullivan county. They have purchased of Messrs. Litchfield & Clark, of Abingdon, all the bark on that farm. The bark is hauled to Vances Tank and shipped from that point to Johnson City. It is estimated that Litchfield & Clark will realize three or four thousand dollars by the transaction.”

5 June 1884. Rugby, Tennessee, Plateau Gazette and East Tennessee News.

“The tannery at Johnson City employs several hundred hands inside and out. Many are engaged in getting out bark. The number of hides being worked (15,000) is large. Everything inside is moved by steam and everything is utilized. The engine is run by spent tan bark and the ashes are hauled out on the farm. The hair and tallow off the hides are saved.”

4 October 1884. Johnson City Comet.

“Watauga Tannery. Two small additions to the tannery have recently been built. The tannery is in fine working order now. The authorities commenced to ship leather by the car load, to New York. 75 hides are put in the vats every day, and 150 hides of leather are finished per day.”

4 October 1884. Johnson City Comet.

“The tannery has begun to ship leather to New York. Some three or four car loads have been shipped.”

31 January 1885. Johnson City Comet.

“The Tannery company have purchased a machine for scouring leather.”

18 April 1885. Johnson City Comet.

“The Watauga Tannery have in their building about \$100,000 worth of sole leather.”

30 July 1885. Johnson City Comet.

“Johnson City. Col. Charlton in the Knoxville Tribune... Watauga Tannery. This is one of the largest tanneries in the country, tanning oak sole leather and tanning about one hundred hides a day. There is, I learn now on hand upwards of \$200,000 worth of leather. Last year there was received 10,000 cords of tan bark from the country adjacent.”

15 October 1885. Johnson City Comet.

“The Watauga Tannery Co. have a large addition to their drying house, and are now finishing about 100 hides per day.”

3 February 1887. Johnson City Comet.

“Watauga Tannery. Just now the good people of Johnson City are given to boasting, and they are scarcely to be blamed. Everything is ‘the biggest’ and ‘the best,’ in the speculative holders of corner lots, and the generality of people take up the cry. Concerning Watauga Tannery, for instance. We can find scores of intelligent men here who will assert it is the biggest concern of its kind in the ‘United States.’ That is very flattering to Messrs. Horton, Yocum & Co., but it isn’t true. There are a few – very few – perhaps not more than two or three, larger industries of the kind in the country, and if Johnson City is not at the top in this branch of manufacturing industry it is very close to the top.

Watauga Tannery was the first manufacturing interest of importance to be established here, and the large outlay of money its proprietors made, coupled with the prospect of future expenditures, has had very much to do with the establishing of their manufacturing interests here.

The Comet reporter will doubtless be erratic in this review of the interest named above. There’s every reason why he should be. One of the resident partners, Col. Yocum, recently piloted him through the establishment, and tried to teach him all about the mysterious of tanning in one lesson. The trade is one that requires several years to learn – hence, if we fall short in imparting information, the fault is more that of our ‘glad philosophers and friend,’ than of our own.

There are four co-partners in the firm: G.B. Horton, Eugene Horton, S.H. Yocum and H. Gildersleeve. The two first named gentlemen reside in New York, and at 59 Frankfort St., the house of G.B. Horton & Co. sells the product of the tannery – they also buy the raw hides that are shipped here. Messrs. Yocum & Gildersleeve are the resident partners – they are workers, and not only are they thoroughly identified with the business of the tannery but they are equally interested in the town itself. Mr. Gildersleeve superintends the mechanical operations of the tannery, – his co-partner presides over the office and as theatrical people would say, is a general utility man.

Watauga Tannery was built in 1883. The co-partners had secured large tracts of timber contiguous to their point and here they centralized their manufacturing operations. The original capacity has been materially increased since then; to-day the tannery is producing at the rate of 50,000 sides per year. All of this vast quantity of raw material is worked up into sole leather; all is chestnut – oak tanned, and not a side leaves the tannery that will not bear the closest inspection; – so far as that is concerned, better or more staple goods are not turned out by any tannery in the whole country – at least that is the verdict of the trade.

The number of laborers employed in the tannery proper is usually about 50. When the firm peels bark from its own timber the force of workmen directly employed reaches 350. This is for a short season, however, as the peeling is almost wholly done during the month of April. This leads us to remark that Horton, Yocum & Co. own not less than 25,000 acres of limbered land in the counties of Washington, Carter, Unicoi and Sullivan; they have enough bark in sight of their own

to last them a quarter of a century, and so long as others peel and deliver they don't touch their own heritage.

It takes 'a heep' of bark to run such an institution, for the annual consumption, is from five to six thousand cords per year, and to guard against possible shortage an average of one year's supply is kept on hand. One bark shed is 300 x 40 feet – the other is 180 x 40.

As we intimated, there are larger tanneries in the country but we'll gamble on the proposition that there isn't a better arranged or more cleanly one. We have visited tanneries that ought to have been suppressed as nuisances – one can't say that of Watauga Tannery. In company with Col. Yocum we visited every department, first taking the precaution to light our pipe against possible and probable stenches. Well – we let the pipe 'go out.' Actually, there was no perceptible odor, and when such cleanliness is shown in such work we are not surprised to learn that the product excels.

As we halted at the office we found this on our note-book (Dimensions of bark sheds already given.) That the 'yard,' all enclosed, is 300 x 42 feet. The dry house is 180 x 45 feet, five floors; the leach house is 36 x 100 feet, two floors. We had noticed the method of conveying refuse bark to the furnaces; which heat five 30-foot boilers, each 32 inches in diameter; we had noticed how nothing was wasted, the refuse from the tannery and the ashes from the furnaces being composted and used on the firm's farm; we had noticed what admirable provision had been made as against loss by fire, and we came to the conclusion that this was not only a business of great magnitude but one wherein the utmost system prevailed in its management, from the smallest to the largest detail.

It one wants to obtain a fair idea of this establishment he should do as The Comet writer has done, visit it; see how the latest improved machinery propelled by steam power is called upon to do what was formerly manual labor, and if a visitor is lucky enough to obtain Col. Yocum as Cicerone will guarantee a pleasant trip, one that will be full of interest, for the Col. can leak information if he feels inclined to. He used to be an Attorney when he lived in Pennsylvania, so as a matter of course, he can't always be relied upon."²⁰

"The Tannery at Johnson City".

"When built, the tannery encompassed six buildings. These buildings were probably painted barn red, like most tanneries. The vat house was 42' by 284' and housed 140 vats, each 8' square and 5' high. These vats were built of two inch thick pine and stood on top of the ground. An adjoining 42' by 50' building had eight pools. The rolling house, where the leather was finished and dried, was 44' by 100', six stories high and housed a 40 horsepower engine. The bark mill house was 40' by 48' and sheltered a 65 horsepower engine that powered the mills. The leach house had twelve leach tubs, each with a capacity of twelve cords of ground bark, and there were five steam boilers. The largest building was the bark house. It was 44' by 300' and held over 3,000 cords of bark...

Tanneries were located near logging operations because it was easier and cheaper to ship animal hides than it was to ship bark. It took nearly two-and-one-half cords of bark to tan fifty hides...

The Johnson City tannery probably tanned cow hides and pig skins brought by rail from Chicago, Kansas City, Cincinnati, and other meat-packing towns...

The two primary kinds of bark used in southern Appalachian tanneries were hemlock and chestnut. Bark was brought from mountain logging operations to Elizabethton and Johnson City by the narrow gauge. It was a major source of revenue for the ET&WNC until the 1920s."²¹

10 November 1887. Johnson City Comet.

“Horton, Yocum & Co. contemplate extending Main and Market streets through their property.”

15 December 1887. Johnson City Comet.

“The Watauga tannery will not move to Bristol before the first of the year, anyhow. They may start a branch there that will employ about 300 men but it is not at all likely they will move their entire plant from Johnson City’ All other places were duly considered before it was located here.”

15 March 1888. Johnson City Comet.

“Little Stewart, son of R.J. Lusk, fell into a pool at the tannery last Friday evening and would have been drowned but for the timely interference of Isham Hickey, who happened to see him fall and ran to his rescue.”

19 April 1888. Johnson City Comet.

“The tannery of Horton, Yocum & Co. is a very large one, making over 1,000,000 pounds of leather a year, and employing from seventy-five to 400 men. The Swadley tannery is smaller, but is a well established institution.”

3 January 1889. Johnson City Comet.

“Enlarging. The Watauga Tannery has begun the erection of an addition to their plant. The new building will be 40 x 150 ft. and two stories high. The first floor will contain 50 vats, making 100 vats in use by the company. This is one of the largest tanneries in the South and it still growing.”

2.2. Horton, Gildersleeve and Company, Watauga Tannery, 1889 to at least 1897

10 January 1889. Johnson City Comet.

“Dissolution Notice. Johnson City, Tenn., Jan. 1, 1889. The firm of Horton, Yocum & Co. is this day dissolved by mutual consent. G.B. Horton, Seth H. Yocum, Henry Gildersleeve, Eugene Horton.

Johnson City, Tenn., Jan. 1, 1889. The undersigned have this day formed a co-partnership under the firm name of Horton, Gildersleeve & Co., who will continue the business heretofore conducted by Horton, Yocum & Co., at the ‘Watauga’ Tannery. G.B. Horton, Henry Gildersleeve, Eugene Horton.”

10 January 1889. Johnson City Comet.

“The new building being erected by the Watauga Tannery is being pushed rapidly. It will be 44 x 208 feet and two stories high. It will contain 80 vats making in all 288 vats in use by the company.”

7 March 1889. Johnson City Comet.

“The new building at the tannery is almost completed and will soon be ready for occupancy when it will give employment to a large number of new hands.”

11 April 1889. Johnson City Comet.

“The Watauga Tannery Company have the frame of their new leech house nearly up. It is 50 x 150 and will hold 10 leeches.”

13 June 1889. Johnson City Comet.

“Mr. H. Gildersleeve will build a handsome dwelling on his lot near the Watauga Tannery at once.”

14 November 1889. Johnson City Comet.

“The Truth – Told by a Bristol Editor After Spending a Day in Johnson City, the ‘Coming Hub’ of East Tennessee... The big steam tannery accelerated the growth of the infant town...”

6 March 1890. Johnson City Comet.

“She’s Booming. – That’s What’s the Matter with Johnson City. It is a Healthy, Substantial Boom and No Mushroom Business About it. – The Knoxville Sentinel representative finds something to say about Johnson City. – Johnson City, Tenn., February 26...”

The reporter visited the Johnson City tannery which was found to be one of the largest in the South. Over 200 men are employed daily by this concern and an immense amount of work is done...”

1 May 1890. Johnson City Comet.

“Addition to the Tannery. The Watauga Tannery is continually enlarging. They have just recently completed a large vat and dry house 50 x 200 feet and an addition to the ooze house 50 x 100. Finding they are crowded in the beam house they will build at once an addition 50 x 150. The framing has all been ordered and work on the new addition will commence in the next 30 days.”

23 October 1890. Johnson City Comet.

“The Watauga Tannery is building an additional boiler-house and will put in a large boiler.”

26 February 1891. Johnson City Comet.

“The Market street bridge over Division street and the Narrow Gauge tannery track is completed and is a great convenience. When Market street is graded it will be the main thoroughfare leading to East Carnegie until Main street is opened through the Watauga Tannery property.”

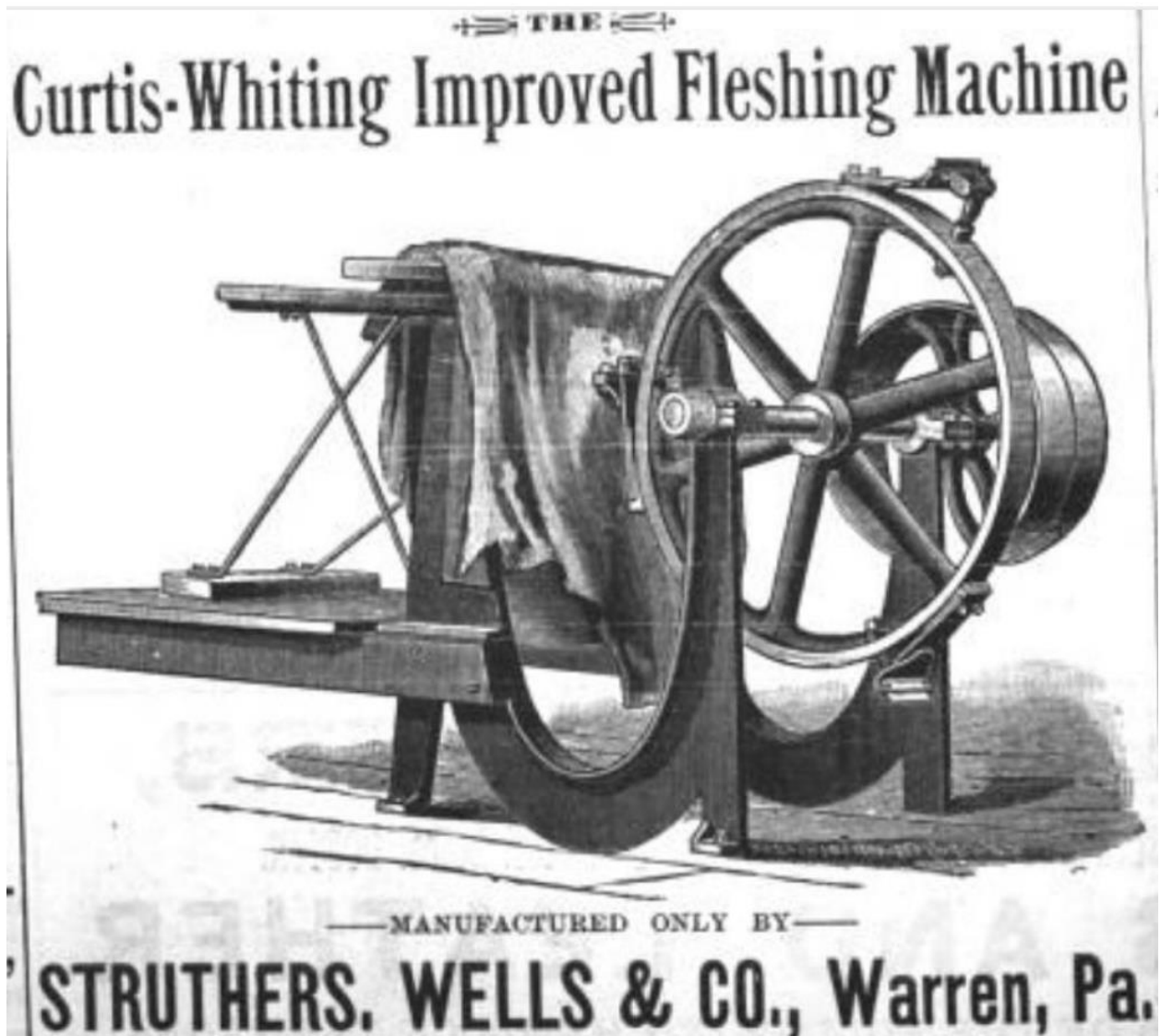


Figure 6. “A Few Users... Horton, Gildersleeve & Co., Johnson City, Tenn.”,
Shoe and Leather Reporter, 1 January 1891.²²

16 April 1891. Shoe and Leather Reporter.

“Under the firm name of Horton & Co., G.B. and Eugene Horton run the Watauga tannery, at Johnson City, Tenn., where they make fine scoured oak backs for the custom trade...”²³

28 May 1891. Johnson City Comet.

“South Watauga... The tan bark men are in a rush to get their bark while it is in prime condition. Some is being sold here and some is being loaded for Horton, Gildersleeve & Co....”

8 October 1891. Johnson City Comet.

“The Tannery Co., unloaded another car load of hides yesterday.”

Men's Patent Leather Shoes.

CLOTH TOP, LACE AND CONGRESS.

We Have Marked Them Only

\$4.00.

We have received our fall styles of the celebrated

STACY, ADAMS & CO.'S

MEN'S FINE SHOES,

And when we say they are handsomer and better than ever, it is enough said. You should see them. They are branded on the bottom

"Watauga Tannery Stock"

And the sole leather is made by Horton, Gildersleeve & Co., right here in Johnson City. There is no better leather than that made by the Watauga Tannery.

While the shoes are better than ever the prices remain the same. Give us a call.

Yours very truly,

Lyle, Hickey & Co.
CITY SHOE STORE.
Johnson City, Tenn.

Figure 7. Johnson City Comet, 15 September 1892.

4 February 1892. Johnson City Comet.

“\$1,288,051. – These are the Startling figures which Tell of the Growth and Activity of the Plucky, Magnetic City – In 1891 – Complete List of Improvements during the Year... Watauga Tannery, bark shed, [\$]600”.

15 June 1893. Johnson City Comet.

“The Watauga Tannery Company are building a new house for their hides.”

1893. “The Tannery at Johnson City”.

“In 1893, the Horton family tanneries were merged with a number of other leather companies to form the United States Leather Company, based in Philadelphia. The new company held a virtual monopoly over the leather tanning business and was one of the largest corporations in America.”²⁴

2 May 1895. Johnson City Comet.

“The Watauga Tannery has purchased the old Stratton planing mill and site and will convert the building into a store room for their finished product.”

4 July 1895. Johnson City Comet.

“Retrospective and Prospective. Too many of our people, with voices pitched unpleasantly high, never fail when an opportunity presents itself to give Johnson City a black eye, which makes her future look decidedly uninviting to strangers visiting the city with a view of locating... It is true, along with other enterprising towns and cities, Johnson City has suffered wonderfully from the effects of the great depression in business which has swept over the country for the past four years... The United States Tanning Company has an immense tannery located here employing from seventy-five to one hundred men, and turning out, at the lowest estimate, one million pounds of finished leather per annum.”

12 November 1896. Johnson City Comet.

“The Watauga tannery has resumed work of laying pipe to the big spring where the water supply is obtained. This line is being laid in place of the old wooden line which is being taken up.”

3 June 1897. Johnson City Comet.

“Tannery Improvements. I.R. Ritchie has returned from the east where he had gone to inspect the new leaching with a view of putting the same in the Watauga Tannery. He visited the tanneries at Cumberland, Pa., Newport, Pa., and Pawpaw, W. Va., and is now prepared to put in the new process by which, it is claimed, the largest per cent of liquor can be extracted from the bark.

Mr. Ritchie has been making plans and bills for lumber this week and will go to work at once and put the new process in the leachers of the Watauga Tannery.”

17 February 1898. Johnson City Comet.

“Johnson City – The Future Manufacturing Center of East Tennessee... The United States Tanning Co., employing from 50 to 100 hands...

12 May 1898. Johnson City Comet.

“Industrial Notes... The Watauga Tannery has found it necessary to purchase a ‘switch’ engine to shift bark cars, etc., on their large yard. The engine arrived last week and is now kept busy. It is under the personal supervision of Commodore Bill Bush.”

27 October 1898. Johnson City Comet.

“The Watauga Tannery has put in a large wind elevator to carry bark from the mill to the vats. It is an ingenious device, the bark being carried by a current of air to a large funnel-shaped tank when the air is extracted and the bark falls quietly into the vat.”

2.3. United States Leather Company, Watauga Tannery, 1900? to at least 1913

20 September 1900. The Johnson City Comet.

“The street car line will soon be a memory. The track is now being put in condition to run cars so the rails can be taken up and hauled to the depot for shipment up the Narrow Gauge. They have been sold to the U.S. Leather Co. and the cars have been sold to a Pittsburg company.”

18 April 1901. Johnson City Comet.

“Special Party. Eugene Horton and other members of the United States Leather Company came to Johnson City on Wednesday, in their private car ‘Pilgrim,’ and spent the day looking over their plant here.”

10 July 1901. Johnson City Comet.

“Death of G.B. Horton. G.B. Horton of 618 Carleton avenue, Brooklyn, died early last evening at Bay Shore, L.I., where he had been living with his family for a month. He had been in ill health for a couple of years, and during that period had virtually given up the cares of business. He had been prominently identified with the leather business for over thirty years and owned extensive tanneries in Pennsylvania. He was a director in the United States Leather Company and a trustee in various financial institutions... He leaves a widow and three daughters. – N.Y. Sun, July 6.’

Years ago, in the late 70s, when Johnson City was a wayside watering station, G.B. Horton found this a desirable location for a tannery, and with Col. S.H. Yocum established the Watauga Tannery. In later years Col. Yocum retired and H. Gildersleeve became a member of this firm. A few years ago the tannery sold out to the U.S. Leather Company. Mr. Horton has visited Johnson City on several occasions and will be pleasantly remembered by the older citizens, who will regret to hear of his death.”

18 July 1901. Johnson City Comet.

“Will Build New Stack. The Watauga Tannery will build a new draft stack. It will be 125 feet high and 18 by 18 feet at the base. The foundation is to be of stone and the balance of brick. Stone are now being placed upon the ground for the work and the excavation has been begun.”

14 November 1901. Johnson City Comet.

“Raising a Building. One of the biggest jobs ever undertaken in Johnson City is now being done at the Watauga Tannery. The growth of the business is constantly calling for more room, and to meet this demand it was decided to make a drying room over the old vat house. To do this the vat house is being raised 14 feet up in the air. This building is 75 feet wide and 400 feet long, and it is no little matter to push such a structure up. It is an inspiring sight to see this work being done with only four jack-screws. When it is raised the limit of a floor will be laid about the vat room and the dry room above completed. On the east of this building another vat house of equal capacity is being constructed. This really doubles the capacity of the tannery, and yet Major Gildersleeve manages it with ease. The work of raising this building is in charge of Mr. H.H. Spencer, an experienced mechanic in the employment of the U.S. Leather company, and a very pleasant gentleman.”

5 December 1901. Johnson City Comet.

“The Comet is pleased to acknowledge the receipt of a photograph of the vat house at the tannery after it had been raised 14 feet by the method invented by Mr. Harry H. Spencer, master mechanic in charge of the work. Our thanks are due Mr. Spencer for the photograph. It shows the structure after it had been raised and before the second floor had been laid on the sidewalls put on.”

5 June 1902. Johnson City Comet.

“Enlarging the Tannery. It is a dull month indeed that some addition is not made to the Watauga Tannery. A 40 x 60 foot extension, two stories high, is being put to one of the vat houses.”

23 July 1903. Johnson City Comet.

“Two Cars Derailed. A little Engine, a little boy and a little tinkering caused an expensive wreck at the Watauga tannery Thursday evening. A little colored boy climbed into the tannery switch engine and opened the throttle, and the engine backed out towards the main line at great speed. Two cars behind it were pushed off the safety switch and they are now as complete a wreck as if the work had been done by a full grown engine.”

2 September 1905. The National Provisioner.

“The Union Tanning Company, of Johnson City, Tenn., has been incorporated. Capital \$100,000.”²⁵

27 February 1908. Johnson City Comet.

“Charlie McRath Killed. Charlie McRath, a colored workman at the Watauga Tannery, was caught in the shafting Monday morning and injured so badly that he died a few hours later. He was trying to fix a broken belt when he became entangled and was horribly mangled before the machinery could be stopped.”

29 July 1909. Johnson City Comet.

“W.J. Bush Gets Painful Injury – Will J. Bush, foreman of the Watauga Tannery, received a painful fall last Thursday and has been confined to his bed since. He had cut and laced a belt and was trying to put it on a pulley while standing on a narrow plank about twelve feet from the floor. The plank slipped off the hangers and he was hurled to the floor and received an extremely painful bruise about the hips that is very slow in healing under the best possible treatment.”

September 1909. Poor’s Manual of Industrials.

“Central Leather Co. – Incorporated April 12, 1905, in New Jersey. In Sept., 1909, the United States Leather Co. was merged into the Central Leather Co...”²⁶

23 April 1910. The National Provisioner.

“The United States Leather Company has begun erection of its proposed extract plant near its tannery at Johnson City, Tenn. The cost will be around \$25,000.”²⁷

15 February 1912. Johnson City Comet.

“Tannery to Resume Capacity Operations – The Watauga tannery, which has been running with a small number of men for the past four months, will resume full operations in the near future. More than thirty years ago this tannery was put into operation and about 100 men have found ready employment there, with the exception of a few respites.”

12 March 1912. Johnson City Comet.

“The Tannery is Putting in Hides – The Watauga Tannery began this week to work a few men in the beam house filling vats with hides. It will necessarily two or three months before a full force of men are put to work in all departments, but other men will be put on as the work progress.”

18 September 1913. Manufacturers Record.

“Johnson City Industries. – Influence in the Upbuilding of its Manufacturing Interests. (Special Cor. of Manufacturers Record.) Johnson City, Tenn., September 15... the United States leather Co. has a big tannery here, and annually reduces many thousands of raw hides to leather, which finds a wide market. This concern uses a great deal of tanbark, this furnishing a market for a forest product that would otherwise be wasted.”²⁸

2.4. Union Tanning Company, Watauga Tannery and Johnson City Extract Company,
By 1915 to at least 1922 and perhaps 1925

10 June 1915. Johnson City Comet.

“Extract Plant Now Building – the Union Tanning Company has a large force at work making preparations to build an extract plant with a capacity of 100 barrels of acid a day. The buildings necessary will be erected on the site of the present tannery and buildings are being razed and ground graded for their erection. A new boiler house will be built and a new leach house fifty by two hundred feet. It will be one of the largest extract plants in East Tennessee and modernly equipped through out, using about 70 cords of wood a day. It is the intention of the company to have the plant in operation by the first of January, and it will require a large force of workmen to accomplish this result. It is also the intention of the company to begin making leather in the tannery that has recently been put in order, as soon as the market will permit.”

26 June 1915. Hide and Leather.

“The Union Tanning Company is razing a number of buildings and grading grounds for the erection of a large extract plant on their site in this city. Already about 100 men are at work at the plant and it is expected by the company to have the extract plant in operation here by early fall. Several thousand dollars are being expended. The plant will have a 100-barrel per day output and will consume seventy cords of wood daily. A large number of men will be employed when the plant is completed.”²⁹

1 July 1915. The Iron Age.

“The Union Tanning Company, Johnson City, Tenn., will build a plant for the manufacture of tanning extracts. The capacity will be 100 bbl. a day. Boilers and special apparatus will be required.”³⁰

2 September 1915. Johnson City Comet.

“Great Activity Shown at the Site of Johnson City’s Tanning Plant – Johnson City. – Great activity is being shown at the Union Tanning company’s plant, where a large extract plant is being erected. This is one of the largest extract plants in the South and no more ideal location could have been found for such an industry as this will be when it is completed. Already full train loads of wood, chestnut being the only kind used, is arriving and the yards and side tracks are being filled with loaded cars.

Eight large buildings will be erected to hold the machinery and the products from the plant. A large well, 250 feet deep, is being dug and many other things are being put in order at the plant. When the plant is completed a large force of men will be given work and their pay roll will add materially to the welfare of Johnson City...”

4 September 1915. Hide and Leather.

“Johnson City, Tenn. – The Johnson City extract plant will begin operations about October 1. This industry requires eight large buildings, the largest of these being 200 feet by 160 feet, and

three stories high. This is the leash [leach] house in which are twenty-eight tanks, eighteen feet high and eighteen feet in diameter. In the operation of this plant about 125 men will be used, and from seventy-five to ninety cords of chestnut wood will be consumed each day. Around Johnson City much chestnut timber is found. The Union Tanning Company's plant is located adjoining the extract plant. The tannery began operation in 1883, but has been closed for four years. When the extract plants begin operations it is expected that the tannery will also resume work."³¹

13 November 1915. Hide and Leather.

"Will Enlarge Tanneries. The plant of the Union Tanning Company, Johnson City, Tenn., is to be enlarged, according to announcement recently made by W.H. Gildersleeve, superintendent of the concern. The tannery was closed for three years, but resumed operations about a year ago, and business has been good enough to justify enlargements. An extract plant which is being built to run in connection with the tannery, is about ready to begin operation."³²

17 February 1916. Johnson City Comet.

"Work Resumed – After Standing Idle for the Past Four Years – Large Force of Men. – The Union Tanning Co., which has been idle for the past four years, will resume operation within a short time, as a large force of men are at work making all necessary repairs on the buildings for this purpose. This tannery was opened in 1880 and ran without ceasing until about four years ago, when it closed down on account of the low prices received for its products.

A few months ago an acid plant was built there and has been operated for some time with paying results. A large force of men are employed. When the tannery is started about 150 men will be employed at both plants. Mr. H. Gildersleeve is manager of the company."

30 June 1916. Sixth Annual Report – Carolina, Clinchfield and Ohio Railway...³³

Table 1. Sidings, Spurs and Additional Yard tracks Built During Year Ended June 30, 1916.

Mile Post		Miles
119.0	Commercial siding for Carnegie spur for Johnson City Extract Works	.097

September 1919. Shoe and Leather Facts.

"Johnson City, Tenn. – The Union Tanning Co., of this city, it is understood, is planning to increase the capacity of its plant."³⁴

25 May 1922. Shoe and Leather Reporter.

"Union Tanning Co. to Resume – The Union Tanning Co., Johnson City, Tenn., a subsidiary of the Central Leather Co., is to resume operations after a year of idleness. It is expected that the extract plant operated in conjunction with the tannery will also resume operations early in the summer. Officials of the Central Leather Company have advised that better market conditions warrant the immediate opening of the plant."³⁵

1926. Moody's Manual of Investments and Security Rating Service.

“Central Leather Company. History: Incorporated under the laws of New Jersey, April 12, 1905; successor to properties and assets of the United States Leather Co. Properties owned by the Central Leather Co. and its constituent companies include the following: Tanneries at... Johnson City, Tenn... Extract factories at... Johnson City...”³⁶

2.5. Watauga Tannery, The United States Leather Company, 1927 to at least 1931?

25 May 1928. Railway and Industrial Compendium, Railway Number.

“United States Leather Co. (The) – Organization. – Incorp. in N.J. on June 23 1927 as a reorganization... of Central Leather Co. Central Leather Co. was incorp. in N.J. on April 12, 1905 as a reorganization... of U.S. Leather Co...”

Nature of Business. – The U.S. Leather Co., directly or through subsidiaries, is the largest company engaged in the manufacture and sale of leather and all its related products (including glue)... The properties owned or controlled by the company include the following: Tanneries at... Johnson City, Tenn... Extract factories at... Johnson City...”³⁷

July 1931. Industrial Employment Information Bulletin.

“Johnson City. – The foundries and machine shops worked on somewhat curtailed schedules, while part time and curtailed forces obtained in the hardwood flooring plants. A clay-products plant remained closed. The reopening of a tannery, with a force of 150 employees, is anticipated. Building included the erection of a tobacco warehouse.”³⁸

1930s. “The Tannery at Johnson City”.

“During the 1920s and 1930s, many of the old tanneries, including the Watauga Tannery, began closing their doors.”³⁹

2.6. Paty Lumber Company, 1932 to at least 1998

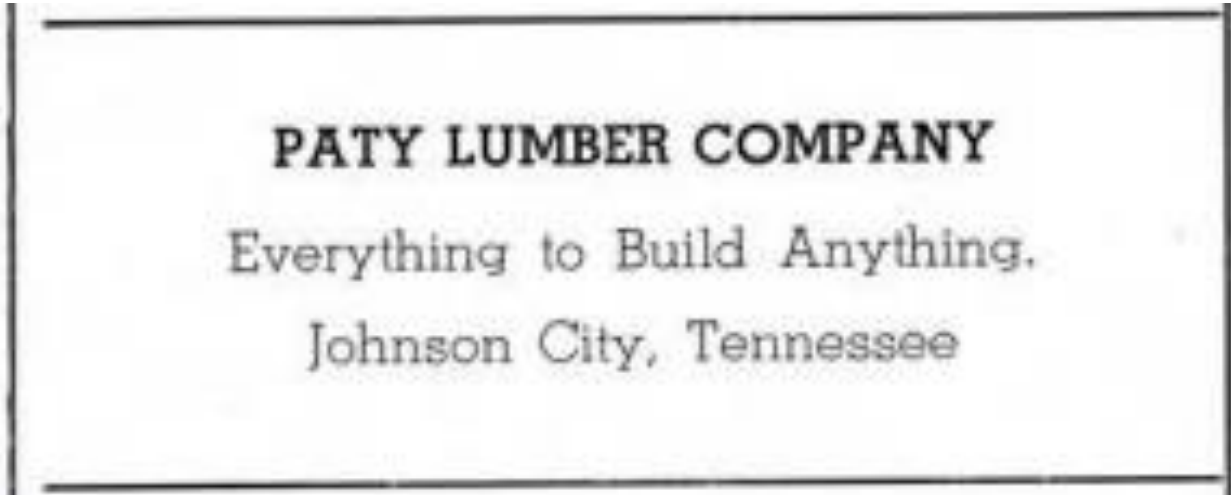


Figure 8. The Buccaneer, 1941.⁴⁰ Added 24 September 2024.

Paty Lumber Co.

ELIZABETHTON

JOHNSON CITY

Phone 5115

Phone 5119

Figure 9. The Buccaneer, 1946.⁴¹ Added 24 September 2024.

The Price
is RIGHT at
PATY'S



Wholesale
To
Everyone
CASH and CARRY

PATY LUMBER COMPANY

Main & Division Sts.

926-7144

Cash & Carry — DELIVERY CAN BE ARRANGED

Figure 10. The Buccaneer, 1964.⁴² Added 24 September 2024.

PATY LUMBER COMPANY

ELIZABETHTON, TENNESSEE
JOHNSON CITY, TENNESSEE
GREENEVILLE, TENNESSEE
BRISTOL, VIRGINIA

32 Years of Service
To East Tennessee

SEE OUR NATIONAL
BRAND QUALITY MATERIALS

Crane Plumbing
Frigidaire Appliances
U. S. Gypsum Products
Celotex Products
Dexter Hardware
Pen Paint Products
Weyerhaeuser Products



WHOLESALE DISTRIBUTING COMPANY

Johnson City, Tennessee

Figure 11. The Buccaneer, 1965.⁴³ Added 24 September 2024.

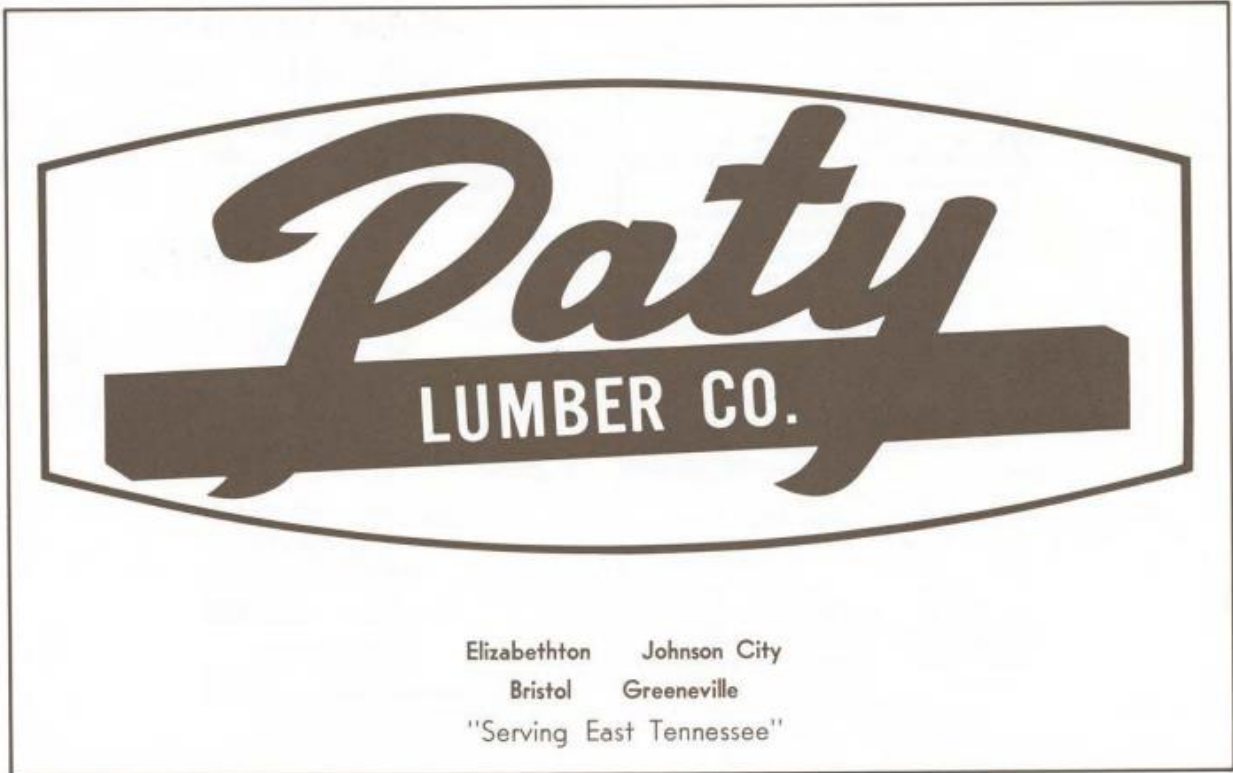


Figure 12. The Buccaneer, 1969.⁴⁴ Added 24 September 2024.

Table 2. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year Ending 1 December 1916.⁴⁵

Firm Name	Character of Business	No. Employes at time of inspection				No. Hours Labor Males		No. Hours Female and Min. Under 16		General Condition
		Over 16		Under 16		Over 16		Min. Under 16		
		M.	F.	M.	F.	Da.	Wk.	Da.	Wk.	
Union Tanning Co.	Tannery	25				10	60			Good
Union Tanning Co.	Extract Plant	56				12	72			Good

Table 3. "Directory of Industries Inspected, with No. Employes, December 1, 1916, to December 31st, 1917... Washington County – Johnson City".⁴⁶

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Extract Co.	Extract Mfrs.	70			
Union Tannery	Tannery	87			

Table 4. "Directory of Industries Inspected, with No. Employes... Washington Co. – Johnson City", for the Year Ending 31 December 1918.⁴⁷

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Extract Works	Tanning Extracts	70			
Union Tanning Co.	Tanners	97			

Table 5. "Directory of Industries Inspected, with No. Employes... Washington County – Johnson City", for the Year Ending 31 December 1919.⁴⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Extract Co.	Mfrs. Tannery Extracts	81			
Watauga Tanning Co.	Tannery	76			

Table 6. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1920.⁴⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Union Tanning Co.	Leather and Extract	38			

Table 7. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1921.⁵⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Union Tanning Co.	Tannery & Extracts	47			

Table 8. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.⁵¹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Union Tanning Co.	Tannery	60			

Table 9. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1925.⁵²

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Union Tannery	Tannery	61			

Table 10. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1926.⁵³

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Union Tannery	Tannery	61			

3. Enterprises near the Junction of First Avenue and New Street, in the Carnegie Addition,
1889 to at least 1950

The Area Occupied by the James A. Martin lumber yard and Related Enterprises,
1901 to at least 1920

James A. Martin lumber yard, at this location 1901 – 1908?
Dickey Lumber Company, 1908? to at least 1920

The Area Occupied by the Brown & Biddle Flour Mill and Related Enterprises,
1889 to at least 1922

Brown & Biddle Flouring Mill, 1889 – 1893
Biddle & Ellsworth, 1893 – 1905
A.B. Biddle & Company, 1905
The Standard Automatic Mine Door Company, by 1920 to at least 1922

Johnson City Mills (Hosiery), 1916 – 1960s

The Area Occupied by the Tennessee Box Company and Related Enterprises,
1915 to at least 1950

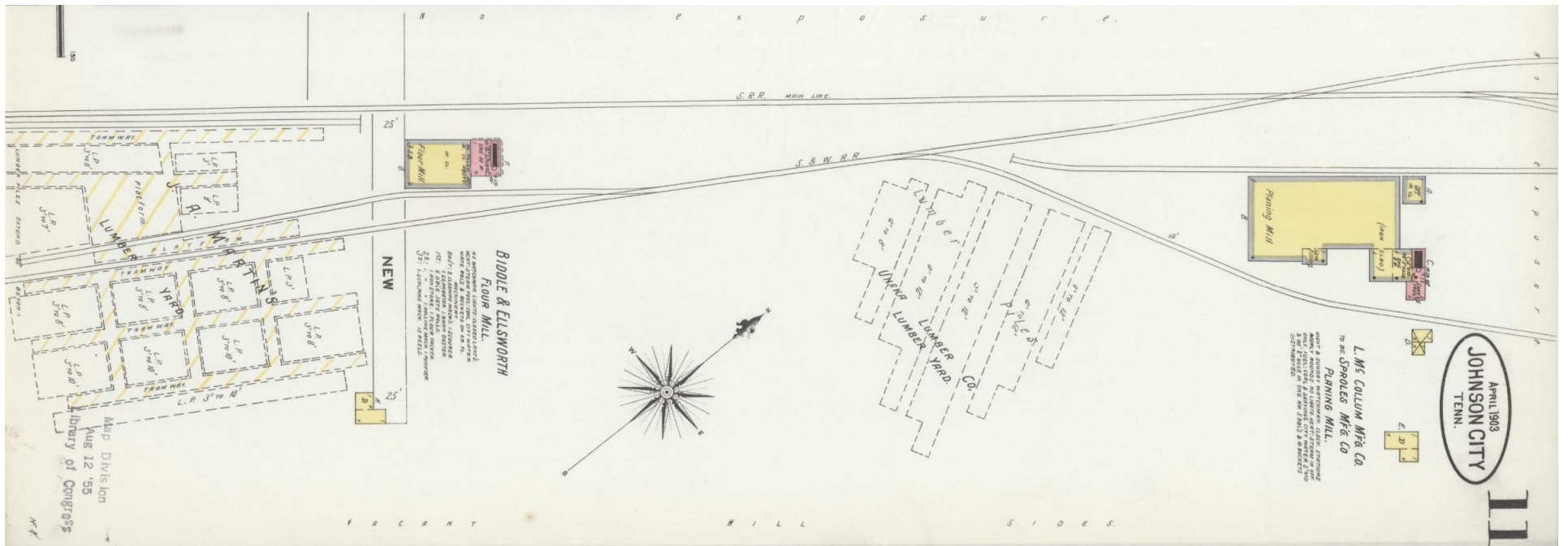
Tennessee Box Company, 1915 – 1926
Tennessee Box & Lumber Company, 1926 to at least 1950

The Area Occupied by the L. McCollum Manufacturing Company and Related Enterprises,
1900 – 1911

L. McCollum Manufacturing Company, 1900 – 1902
Sproles Manufacturing Company, 1902 – 1905?
Unaka Lumber Company, at this location, 1905 – 1911
(Unaka Lumber Company's nearby lumber yard, by 1903 to at least 1908)

This area was served by the E.T. & V./E.T.V. & G./Southern since its construction. The 3C's/O.R.
& C./South & Western/Clinchfield had trackage to this area by 1891.

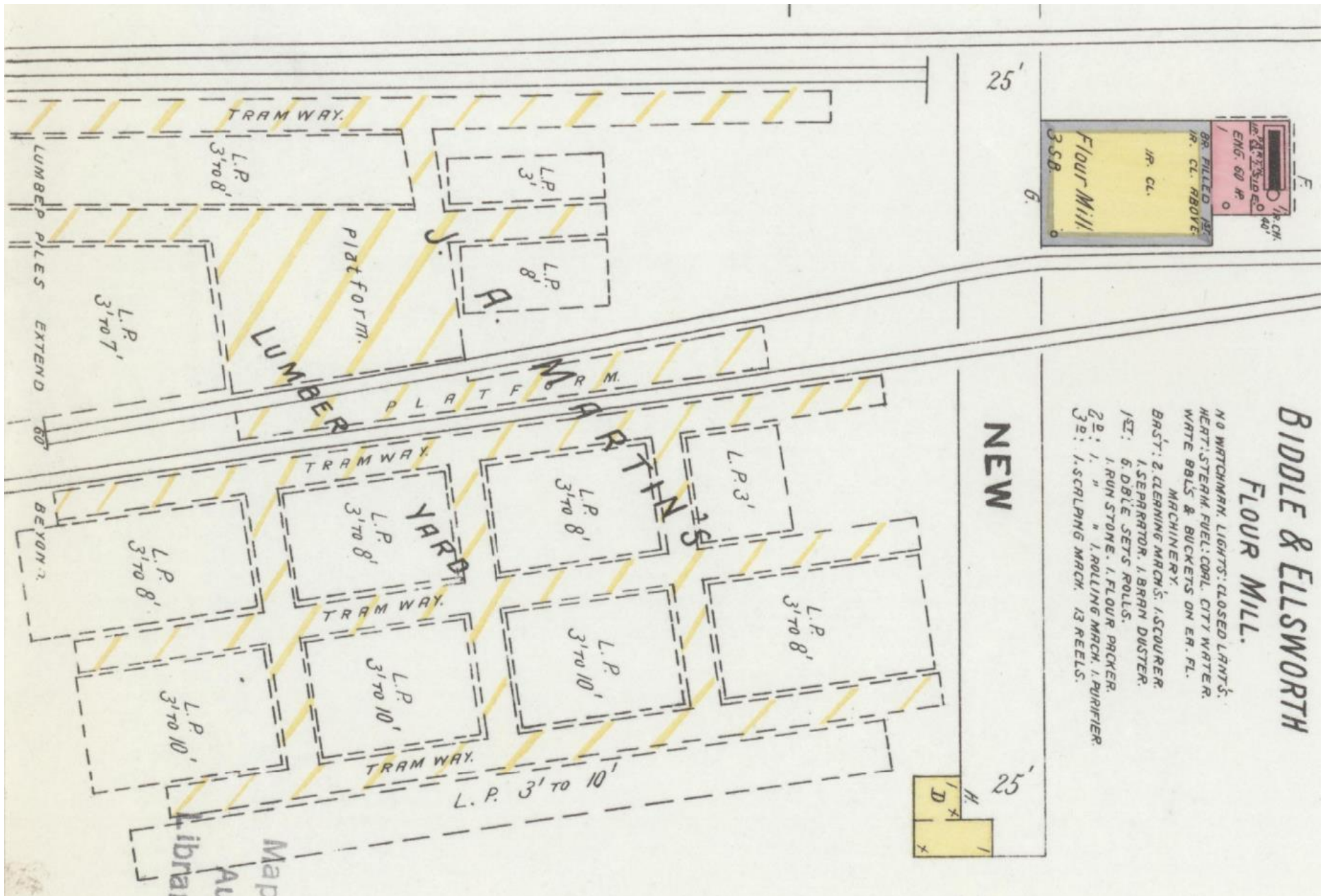
Google maps indicates that this area is now occupied by a series of warehouse type structures.



Map 9. This map depicts “J.A. Martin’s Lumber Yard”, the “Biddle & Ellsworth Flour Mill”, the “Unaka Lumber Co. Lumber Yard”, and the “L. McCollum M’F’G. Co. to be Sproles M’F’G. Co. Planing Mill” (from left to right), in April 1903. These enterprises were located near the junction of New Street and the Southern Railway.⁵⁴

3.1. The Area Occupied by the James A. Martin lumber yard and Related Enterprises,
1901 to at least 1920

James A. Martin lumber yard, at this location 1901 – 1908?
Dickey Lumber Company, 1908? to at least 1920



Map 9A. A detail of "J.A. Martin's Lumber Yard" and the "Biddle & Ellsworth Flour Mill", in April 1903.



Figure 13. "Workmen in Johnson City, Tenn., c. 1920," with a skidder, which was probably similar to the 20-h.p. one sought by James A. Martin in 1907.⁵⁵



Figure 14. “Steam Donkey. A steam donkey is a steam-powered winch invented in 1881 by John Dolbeer that revolutionized 19th-century logging in areas with difficult or steep terrain and large logs beyond the capability of a team of oxen or horses. The operator was referred to as a donkey puncher. This postcard was postmarked 1908 in Johnson City and mailed to Polly Dunn in Vaughtsville, Johnson County, Tennessee. At that time, there were 8 lumber wholesalers and 12 lumber manufacturers listed in the city directory.”⁵⁶

James A. Martin lumber yard, at this location 1901 – 1908?
Dickey Lumber Company, 1908? to at least 1920

3.1.1. James A. Martin lumber yard, at this location 1901 – 1908?

11 April 1901. Johnson City Comet.

“Jas. A. Martin is moving his lumber yard from near the Uptegrove plant to the lot just south of the Biddle & Ellsworth mill.”

22 August 1901. Johnson City Comet.

“Putting in Siding. The Southern Railway is putting in a siding for Biddle & Ellsworth’s flouring mill and J.A. Martin’s lumber yard. The sidetrack will cut out just below the bridge at the tannery and run down the east side of the main line below the mill. The road bed is being graded and the ties and iron is on the ground.”

25 Dec. 1902. Johnson City Comet.

“James A. Martin, a prominent lumberman of Johnson City, is in the city [of Knoxville] today. He has a number of mills in Western North Carolina, and has an extensive yard at Johnson City. He says that all kinds of lumber is still bringing the very best of prices. – Knoxville Sentinel.”

16 July 1903. Johnson City Comet.

“A peculiar accident occurred at Jas. A. Martin’s lumber yard near Biddle & Ellsworth’s mill Monday night. The scaffolding beneath several stacks of lumber gave way and the lumber fell toward the sidetrack carrying the platform and an empty car standing on the siding with it. The mass of wreckage so blocked the main line that the track had to be moved out in order to let trains pass until the lumber could be moved.”

24 September 1903. Manufacturers Record.

“Burned... Johnson City, Tenn. – James A. Martin’s saw-mill and lumber-yard; estimated loss \$12,500.”⁵⁷

February 1907. Packages.

“Jas. A. Martin, Johnson City, Tenn., wants a 20-hp skidder to drag logs on the ground for about half a mile in woods.”⁵⁸ See figures 13 and 14.

23 February 1907. American Lumberman.

“J.A. Martin, of Johnson City, Tenn., has bought 5,000 acres of hardwood timber in Haywood and Swain counties, North Carolina. The timber is mostly poplar, oak and chestnut. Mr. Martin has been in the lumber business at Johnson City for twenty-three years.”⁵⁹

9 May 1907. Johnson City Comet.

“Runaway Car. Friday morning some workman allowed a car loaded with lumber at J.A. Martin’s yard to get away, and it ran out on the main line just as passenger train No. 4 was passing, striking the first day coach and damaging it slightly. The box car was overturned and ditched. Engineer Bustor Dunn tried to avoid the accident by speeding by, but could not, and prevented a greater calamity by bringing his train almost to a full stop before the impact.”

20 August 1908. Johnson City Comet.

“Lumber Yard Burns. – Monday Night’s Blaze Destroys Piles of Lumber for J.A. Martin. About midnight Monday night fire was discovered in the lumber yards of J.A. Martin on the Southern Railway near New street and an alarm was turned in, but the department was unable to be of value because there was no water plug close enough to reach the scene and there was nothing to do but watch the flames do the work. There is scarcely a doubt that the fire was of incendiary origin, as it seems to have been kindled in two places widely apart. The lumber destroyed amounts to \$22,000, with \$17,000 insurance.”

27 August 1908. The Insurance Field.

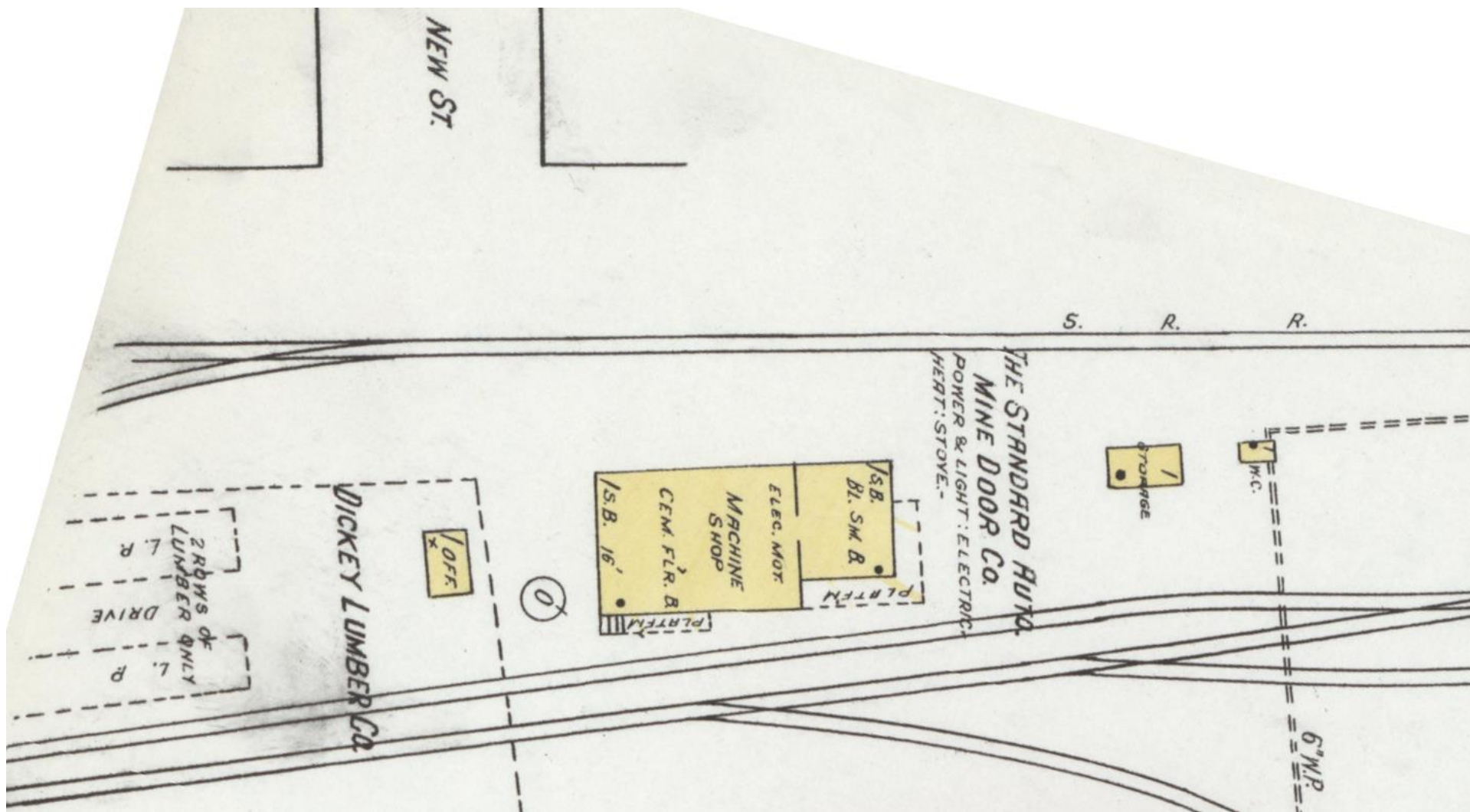
“Looking for Incendiaries. Detectives are trying to locate the incendiaries who fired the lumber yard of J.A. Martin, of Johnson City, Tenn., and caused a loss of \$30,000. Evidence has been unearthed that the fire was of incendiary origin, having been started at three different places in the lumber yard.”⁶⁰

1 September 1908. The Lumber Trade Journal.

“Johnson City, Tenn., August 19. – Fire in the lumber yard of Jas. D. Martin in Carnegie did heavy damage. On this yard was stacked 1,000,000 feet of oak, poplar and pine, all of which was consumed yesterday. The estimated value of this lumber was \$22,000, with \$14,000 insurance. The Dickey Lumber Company also had several carloads of lumber in this yard, which was destroyed. No insurance was carried.

The fire is thought to be the work of an incendiary, for when discovered, it was burning in four different places. The city fire company could render no assistance, as there are no fire plugs near the yard.”⁶¹

Note that the author has not found any later information on this enterprise, which perhaps indicates that James A. Martin did not re-establish this particular lumber yard.



Map 12A. The “Dickey Lumber Co.” and “The Standard Auto. Mine Door Co.” (from left to right) in February 1920, near the junction of the Southern Railway and New Street. Note that “The Standard Auto. Mine Door Co.” appears to have the same footprint as the Biddle & Ellsworth flour mill. Also note that the “Dickey Lumber Co.” appears to have expanded into the same area that was occupied by the “J.A. Martin’s Lumber Yard”.

3.1.2. Dickey Lumber Company, 1908? to at least 1920

1 September 1908. The Lumber Trade Journal.

“Johnson City, Tenn., August 19. – Fire in the lumber yard of Jas. D. Martin in Carnegie did heavy damage. On this yard was stacked 1,000,000 feet of oak, poplar and pine, all of which was consumed yesterday. The estimated value of this lumber was \$22,000, with \$14,000 insurance. The Dickey Lumber Company also had several carloads of lumber in this yard, which was destroyed. No insurance was carried.

The fire is thought to be the work of an incendiary, for when discovered, it was burning in four different places. The city fire company could render no assistance, as there are no fire plugs near the yard.”⁶²

30 September 1909. Johnson City Comet.

“Dickey Lumber Company. Among the enterprising and progressive men who are making Johnson City one of the cities of Tennessee, known to be in line as ‘live wire’ town, is Dickey Lumber Company.

They control over 38,000 acres of timbered land and market the output of five mills, and their shipments of hardwood lumber to Eastern and New England cities will approximate \$90,000 annually. They are admirably located in the eastern section of this city on the C.C. & O. and Southern railway tracks, where they keep and are prepared at all times to fill small or large orders for building material, framing and dimension stock, dressed flooring and siding, hardwood, oak and maple a specialty, interior finish, laths, shingles, etc., etc.

This firm first saw the light of business competition twelve years ago, and in that time they have built up a most enviable wholesale and retail business among our critical home people and the Eastern and Western markets. Their business is continually increasing, and they state that the present price for lumber equals those that existed before the panic of 1907. The members of this firm are well known in many business walks of life and have always held a high position wherever they have been known for their sterling business principles and upright manner in which they have treated all who have dealings with them.”

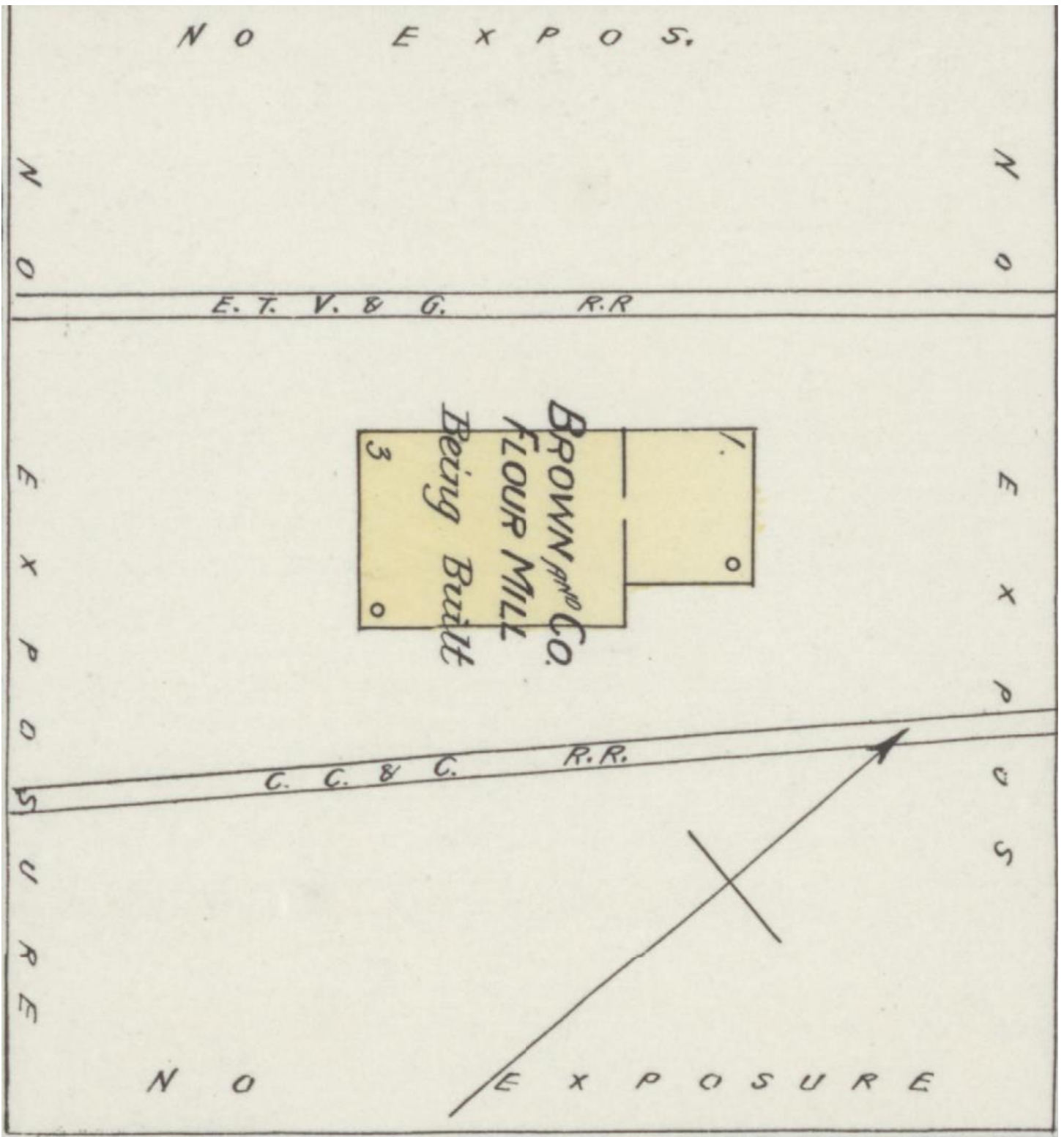
3.2. The Area Occupied by the Brown & Biddle Flour Mill and Related Enterprises,
1889 to at least 1922

Brown & Biddle Flouring Mill, 1889 – 1893

Biddle & Ellsworth, 1893 – 1905

A.B. Biddle & Company, 1905

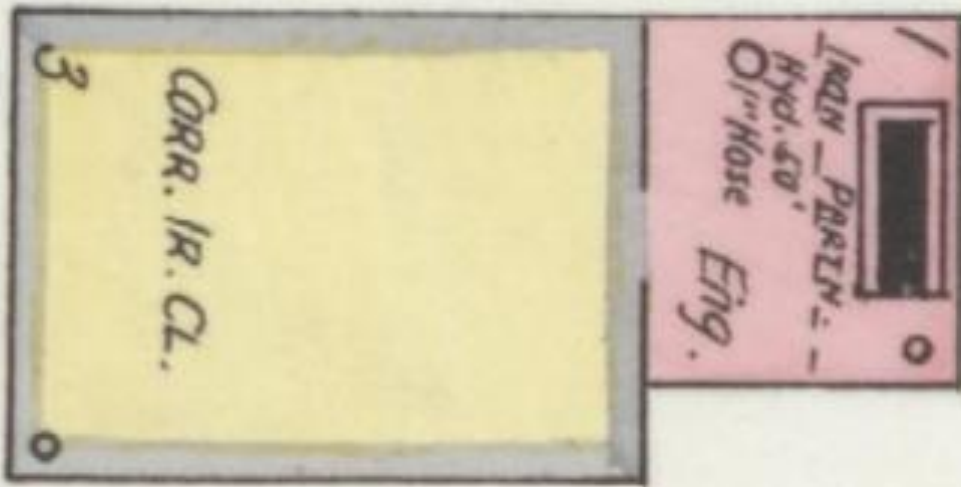
The Standard Automatic Mine Door Company, by 1920 to at least 1922



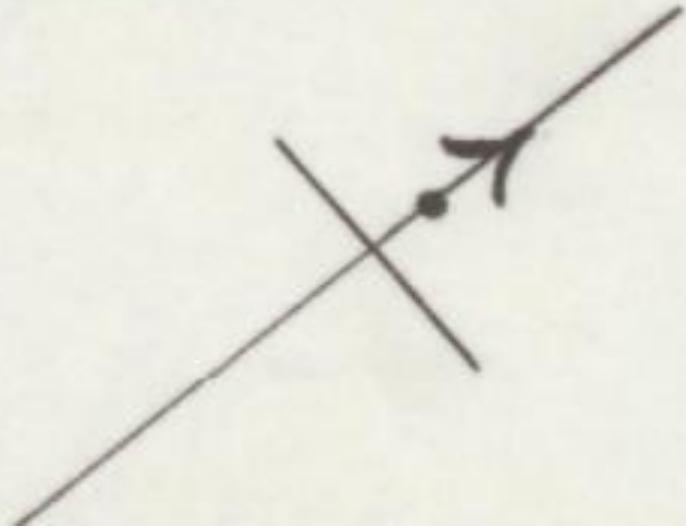
Map 10. "Brown & Co. Flour Mill Being Built" in January 1891. This building was on the southeast corner of 1st Avenue and New Street, in the Carnegie Addition.⁶³

E.T.V. & G. R. R.

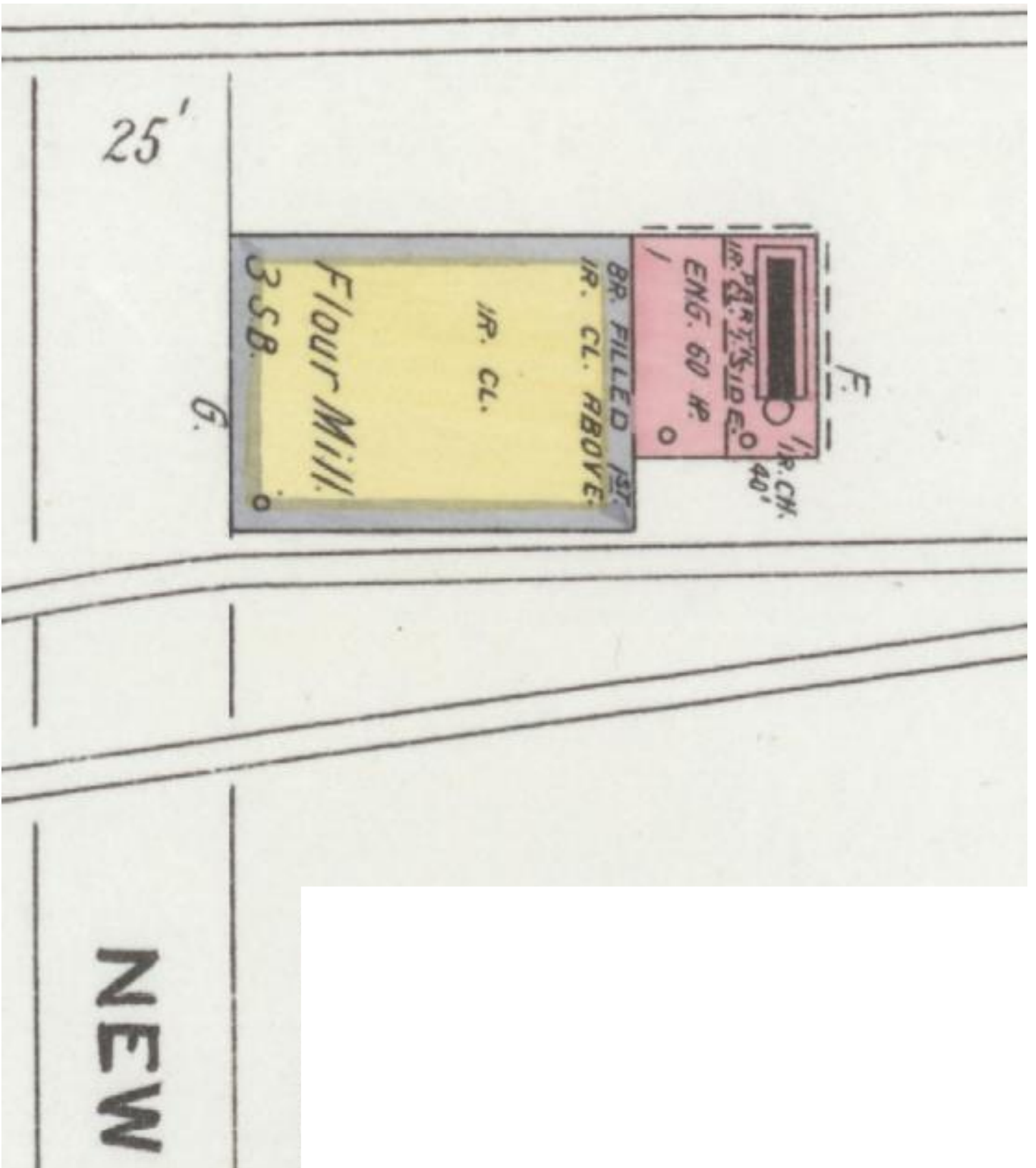
BIDDLE AND ELLSWORTH
FLOUR MILL.



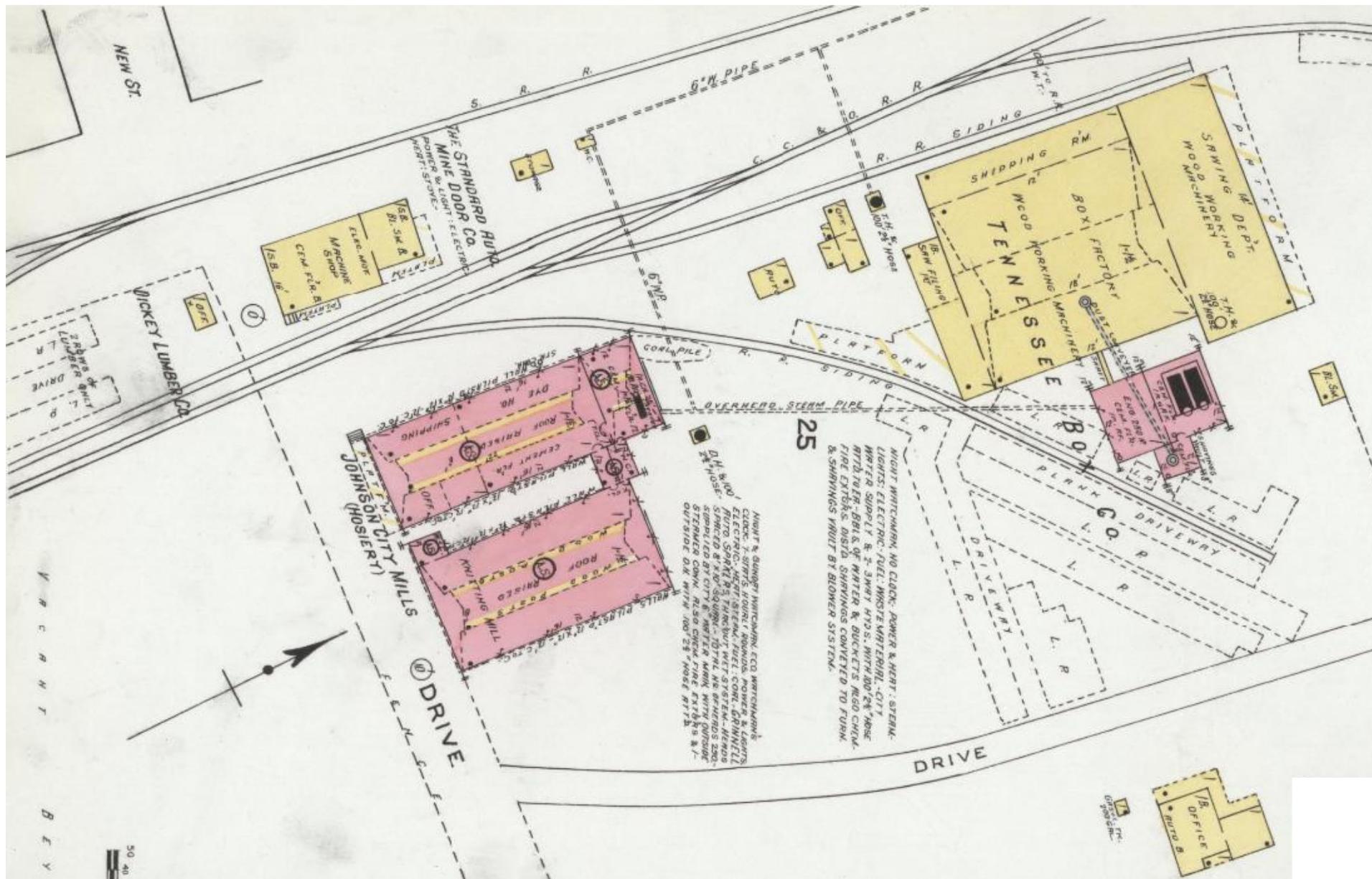
C.C. & C. R. R.



Map 11. The Biddle and Ellsworth Flour Mill in May 1897.⁶⁴



Map 9B. The Biddle and Ellsworth Flour Mill in April 1903.



Map 12. The Dickey Lumber Company, the Standard Automatic Mine Door Company, the Johnson City Mills, and the Tennessee Box Company (from left to right) at the junction of New Street and the Southern Railway mainline, February 1920.⁶⁵ Regrettably, the area covered by this map was not included in the December 1908 and July 1913 editions of the Sanborn maps.

Brown & Biddle Flouring Mill, 1889 – 1893
Biddle & Ellsworth, 1893 – 1905
A.B. Biddle & Company, 1905
The Standard Automatic Mine Door Company, by 1920 to at least 1922

3.2.1. Brown & Biddle Flouring Mill, 1889 – 1893

5 December 1889. Johnson City Comet.

“We can not give particulars just now, but we know that a roller process flouring mill is a certainty for Johnson City. The party will be here this week to look out a location and will let the building to contract at once. The plant will be built for 250 barrels a day, but only 125 barrels will be made to start on, which is a very nice output, and there are only two mills in east Tennessee that beat it. In another week we hope to be able to announce that the work has been contracts.”

24 April 1890. Johnson City Comet.

“Johnson City – The Great Mineral Center... Editor of the Chattanooga Republican: Johnson City, Tenn., April 12, 1890... among other new enterprises to be undertaken is a 200-barrel flouring mill with full roller system...”

24 April 1890. Johnson City Comet.

“Roller Flouring Mill. – Site Located and Contract Signed for its Erection. Mr. [A.B.] Biddle and Mr. [J.H.] Brown, of Pennsylvania, have been in the city looking out a location for a roller-process flouring mill and were immediately impressed with the fact that Johnson City was the right town. They were friends of our townsman, W.B. Whiteside, and he took them to General Wilder and told him what they wanted. The General drove them down to the Carnegie addition Monday and they selected a location and went into an agreement in writing to build a 100 barrel roller mill. They will commence work on the building in 90 days. The site is all that could be desired. It lies just below the tannery and between the Three C’s and E.T.V. & G. tracks south of the junction of the two roads and where the E.T. & W.N.C. road can run a track to the mill. The ground was donated by the company and any other bona fide enterprise can get the same inducement.”

26 June 1890. Johnson City Comet.

“The Flouring Mill. – Plans now in the Hands of Contractors and Work to Commence at an Early Day. – Messrs. Brown and Biddle, of Pennsylvania, have sent plans and specifications for their roller-process flouring mill to Mr. W.W. Whiteside and requested him to receive bids for its construction. The Comet had the pleasure of seeing the plans Tuesday. The main building is to be 50 x 75 feet and four stories high, first story to be of stone. The engine house will be separate from the main building and will be a good-sized building. The plant is to be erected at once and must be commenced not later than September 1st by terms of contract with Carnegie Land Company for the site. The mill will have a capacity of 125 barrels per day and give employment to a large number of hands. The site is near the junction of the Three C’s and East Tennessee, Virginia and Georgia roads and has every railroad facility possible.”

4 September 1890. Johnson City Comet.

“The Flouring Mill. The Comet has direct information that work will commence on the new roller process flouring mill within the next ten days. Messrs. Brown & Biddle, the proprietors, have contracted for the erection of the plant, and the contractors will arrive about the 8th of this month. The mill will be equipped with the latest and most improved machinery, and will have a capacity of 135 barrels per hour. The mill will be ready for business as soon as money and men can erect it. Another evidence of the advantages of Johnson City.”

18 September 1890. Johnson City Comet.

“It was announced in The Comet some weeks ago, that Brown & Biddle had contracted for the erection of their Roller-Process Mill. The contractors are Pennsylvania gentlemen and arrived here last Friday to commence work. A force was put to work Monday excavating for the foundation, and as soon as the stone work is completed the building will be rapidly put up. It will require several months to get the mill ready for operation, on account of the machinery.”

Note: “Josiah F. Ellsworth was remarkably successful as a contractor and mill builder, and his work was so excellent in durability and quality that his services were in great demand beyond his own county. He erected... the flouring mills of Brown & Biddle at Johnson City, Tennessee...”⁶⁶ Perhaps Josiah F. Ellsworth was related to “E.E. Ellsworth, of Minnesota”, who joined this enterprise in 1893, which resulted in it being renamed “Biddle & Ellsworth”.

27 November 1890. Johnson City Comet.

“The Flouring Mill. The foundation of the roller process flouring mill is completed. The timbers are all cut for the frame and it will be finished at once.”

15 January 1891. Johnson City Comet.

“The Brown & Biddle flouring mill is nearing completion. The main building is receiving the finishing touches and the stone foundation for the engine and boiler house is being laid. The contractors have not allowed the weather to interfere with their work in the least. The mill will have a capacity of 125 barrels of full roller process flour per day and it is needless to say it will make Johnson City a first class wheat market.”

29 January 1891. Johnson City Comet.

“The Flouring Mill. Mr. Biddle, of Loysburg, Pa., was in the city several days this past week. He is a member of the firm Brown & Biddle, owners of the flouring mill now being erected in Johnson City, and was here on business connected with the enterprise.

In conversation with The Comet Tuesday, he stated that the machinery for the mill would begin to arrive in a few days, and the millrights would be here about the first of February to put it in position. It is the intention to have the mill ready by the next wheat crop.

The engine and boiler house will be completed this week and six more days will complete work on the outside of the mill proper,

The mill will be equipped with the latest and most improved machinery, and will have full roller process and when completed will have a capacity 125 barrels of flour every 24 hours.

Mr. Biddle was pleased with the progress of the work and the prospects for the future in general. He will return in a few weeks.”

12 March 1891. Johnson City Comet.

“The Flouring Mill is Rapidly Nearing Completion – Machinery Arriving – The Johnson City flouring mill is moving on steadily toward completion, notwithstanding the unfavorable weather. The house is about done, some of the machinery is already in position and the remainder on the road. It will be in full blast with a capacity of 200 barrels per day by the middle of May.”

30 April 1891. Johnson City Comet.

“Like a Mighty Giant – Johnson City Stalks on to the Goal in Spite of Hard Times... Not far from the great tannery, which has been in successful operation for several years is the immense flouring mills of Brown & Biddle. These mills have a capacity of 125 barrels per day and will be completed as soon as the machinery has all arrived and most of it is placed. Messrs. Brown & Biddle came here from Pennsylvania, and say that the iron workers of Pittsburg see the hand writing on the wall, and are about to fold their iron tents and steal away to this wonderful steel producing land...”

7 May 1891. Johnson City Comet.

“Will Start the Roller Mills. Brown & Biddle, partners in the new roller flouring mill, have moved their families from Loysburg, Pa., to the city, and are living on Holston avenue.

Work on the mill has advanced rapidly, and it is now most ready to be operated. The engine will be fired tomorrow for the first time to test the boilers, etc.

There are now on the side tracks two carloads of grain and 325 bushels of corn, waiting for the mill to be opened up. They will be ready to start in about ten days.

This is one of the best and most substantial mills in East Tennessee, and it will prove a valuable acquisition to Johnson City.

Being the first flouring mill for the city, it adds to the industrial variation and goes to show that we will have diversified plants outside of the iron industry.”

28 May 1891. Johnson City Comet.

“Their First Flour. – Brown & Biddle Set Their Great Mill Agoing to be in Motion Soon. Johnson City’s first and only flouring mill has been completed and is now in operation. They made their first flour Monday. The mill, which is estimated to make 125 barrels of flour per day, is the latest improved roller process and runs with perfect grace and ease. It is one of the most substantial plants in the city. The base is built from heavy limestone. The three upper stories are constructed from timber, the frame work being heavy and staunch. The outer surface of the structure is overlaid with corrugated iron which has the appearance of brick. There is no doubt that the machinery is of the very best brand; and it has, moreover, been well put in place, so that it insures successful operation, comparatively without friction or jar. The machinery was put in place by Mr. Ferguson, of Buffalo, N.Y., a thorough mechanic.

Here now is another of the varied industries which are seeking this magnetic place, and its whistle will be heard each morning along with the foundry, furnace, lumber shops and other industries. They seek this wonderful country whose beds of iron and coal are inexhaustible and stand as security to capital in whatever branch of industry it may be invested. This is a good point for a flouring mill and the move which these gentlemen have taken is all that has been necessary; for otherwise the success of the industry was already assured. A hundred and twenty five barrels per day is a good beginning for the flour industry of Johnson City.”

4 February 1892. Johnson City Comet.

“\$1,288,051. – These are the Startling figures which Tell of the Growth and Activity of the Plucky, Magnetic City – In 1891 – Complete List of Improvements during the Year... Brown & Biddle Flouring Mill. [\$]25,000”.

11 August 1892. Johnson City Comet.

“New Wheat. The steam flouring mills of Messrs. Brown & Biddle at this place are now running on full time, and are doing a good business. Much new wheat is also coming in and finds a ready sale to them at from 70 to 77 cents per bushel.”

3.2.2. Biddle & Ellsworth, 1893 – 1905

20 April 1893. Johnson City Comet.

“New Firm. Mr. Brown retiring from the milling firm of Brown & Biddle, this city, has been succeeded by Mr. E.E. Ellsworth, of Minnesota, the change taking place a few days since. The new firm will in every way maintain the reputation of the old one and continue to turn out, at the old stand, the whitest of white flour.”

24 August 1893. Johnson City Comet.

“Amputation Necessary. Jacob Bayless, the young man who had his hand caught in the machinery at Biddle & Ellsworth’s flouring mill some days ago, and badly mashed, is doing well. The thumb and index finger of his right hand were so badly mashed they had to be amputated. This was done by Dr. W.J. Miller, and the young man is resting easily and will soon be at work again.”

20 May 1897. Johnson City Comet.

“Stand Together. Just as soon as our business concerns learn to patronize home industries they may expect to do a little more themselves. No town on earth can prosper as long as the leading firms in it boycott home industries. One may travel the country over and not find a place so niggardly to her industries as Johnson City. There is only one way to make a good town and that is for our citizens to stand together and patronize home industries. The recent boycott on Biddle & Ellsworth’s flouring mills is a sample of what ruins a town, and is one reason why Johnson City is shakily and unsteady. Such proceedings is a disgrace to any town.”

27 May 1897. Johnson City Comet.

“The Storm. This city was visited last Friday by a severe hail storm accompanied by a destructive wind... The smokestack of Biddle & Ellsworth’s flouring mill was blown down and had to be taken to the foundry where it was straightened out and repaired...”

17 June 1897. Johnson City Comet.

“Mill Improvements. The progressive flouring mills of Biddle & Ellsworth are soon to have the ‘new universal’ bolter added to their already modern and up to date machinery. This bolter is the very latest patent and only a few of them have been sold. It will do the work of four

or five ordinary bolters of the same size, and has the advantage of extra quality of work added to wonderful capacity. It will make still better the celebrated brands of this popular mill.

The bolter is manufactured by the Edward P. Allis Co., at Milwaukee, Wis. The car containing the bolter is now on the yards and as soon as possible the machinery will be put in the mill.” See Figure 15.

22 July 1897. Johnson City Comet.

“The flouring mills of this city are so far behind with orders that they are running on double time – day and night.”

17 December 1897. The Weekly Northwestern Miller.

“The Prinz & Rau Mfg. Co. reports the following sales and orders this week for milling machinery... Biddle & Ellsworth, Johnson City, Tenn., 1 No. 2½ cockle machine...”⁶⁷

19 March 1898. The Modern Miller.

“Among the Mill Furnishers. – Nordyke & Marmon Co., Indianapolis. – We are pleased to advise you that our shops have been and are still running steadily full time and with full force... We have a contract... to remodel the mill of... Biddle & Ellsworth, Johnson City, Tenn...”⁶⁸

10 November 1898. Johnson City Comet.

“Fire at the Flouring Mills. Last Friday evening just before six o’clock a fire alarm was turned in from the roller mills of Biddle & Elsworth. The fire company responded promptly. The fire originated at the journals of the new bolting machine recently put in and was soon under control, but not until this and other machinery was badly damaged from fire and water. The damage is about \$1,500, fully insured.”

25 November 1898. The Weekly Northwestern Miller.

“The damage loss of Biddle & Ellsworth, Johnson City, Tenn., has been adjusted at \$778, and was promptly paid by the millers’ companies interested. The fire originated in a universal bolter, but by means of buckets of water was put out before spreading very much.”⁶⁹

December 1898. The Southern Architect.

“Johnson City, Tenn. – The flouring mills of Biddle & Ellsworth burned. Loss, \$1,500. Insured.”⁷⁰

12 December 1898. The Weekly Northwestern Miller.

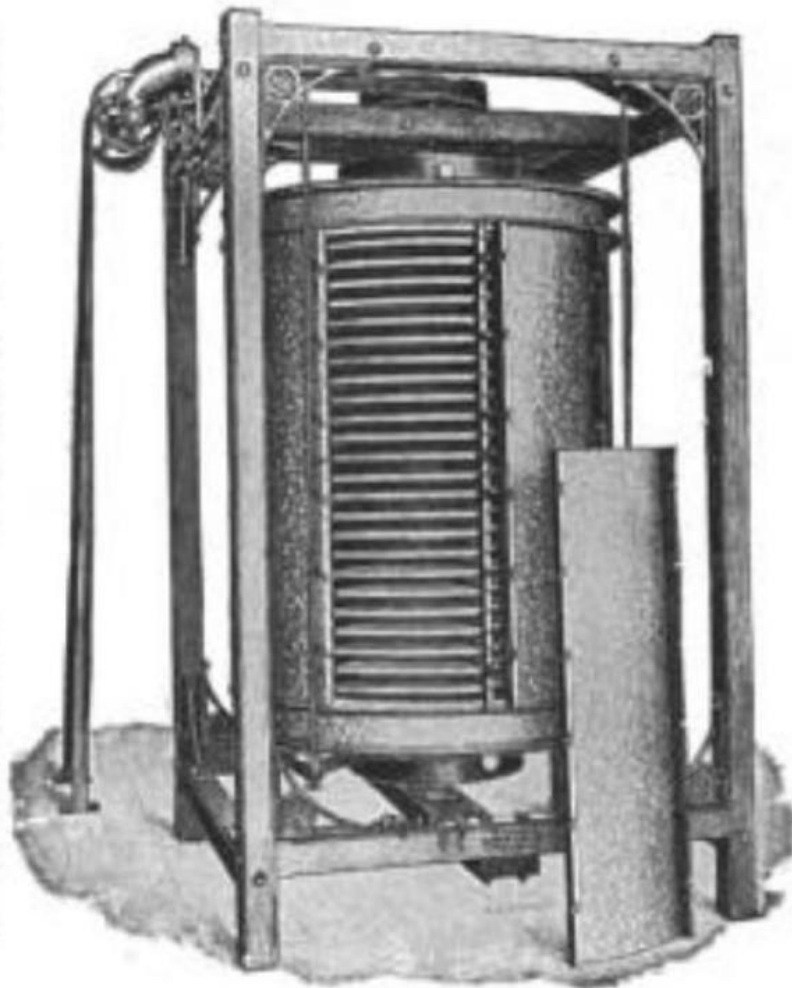
“Nordyke & Marmon... Additional orders are from... one swing sifter and one sieve purifier from Biddle & Ellsworth, Johnson City, Tenn...”⁷¹

UNIVERSAL BOLTER.

THIS list indicates quite an active youngster. It is only a yearling.

This machine has more and stronger friends than most machines have, after years of labor have been expended to perfect and introduce them.

Our new Circular gives a few of the letters of endorsement received. If you do not receive one of these beautiful green-covered Circulars within a few days, write for it.

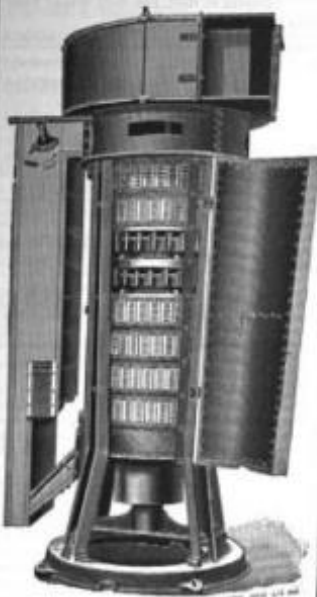


If You Want a Beauty, Write for Our New Catalogue.



Figure 15. *The Weekly Northwestern Miller*, 8 October 1897.
“In the border we give a partial list of the users of the Universal Bolt...
Biddle & Ellsworth, Johnson City, Tenn.”⁷²

THE
IRON PRINCE
SCOURER



IS GUARANTEED
TO CLEAN WHEAT BETTER
IN ONE OPERATION THAN
ANY TWO STANDARD MA-
CHINES ON THE MARKET.
IT WILL TAKE LESS POWER
AND DOES NOT BREAK THE
WHEAT.

More Letters

FROM LEADING MILLERS SPEAKING OF
THE RESULTS OBTAINED BY USING THE

Iron Prince Scourer

BIDDLE & ELLSWORTH,
Limited,
Merchant Millers.

JOHNSON CITY, TENN., Dec. 31, 1898.

The Prinz & Rau Mfg. Co.,
Milwaukee, Wis.

Gentlemen: Answering your favor of the 28th, we are pleased to say that the Iron Prince Scourer that we put in last summer meets every expectation, so far as cleaning the wheat goes. In fact, it surpasses what we really expected, especially in cleaning smutty wheat, and is the only machine that we have ever seen that really does clean this class of wheat so that it is in grinding condition. We have used nearly every cleaning machine on the market, and are free to say that it is as far ahead of any of them as they are ahead of the old style smutters in use thirty years ago. There is no other machine in the same class and there is no machine that we have ever used in a mill that we can recommend as cheerfully as we do this one.

The machine is well made, and, from appearances, should be a durable one. It is a very light running machine, considering the quality of work done; in fact, we think it is lighter running than the average close scourer. It does not break wheat, and it is only in this respect that we have been disappointed, and, of course, agreeably so. We had been told so many times that it broke so much wheat that we had serious doubts of it in this respect.

We don't know how we could do any miller any greater favor than to recommend him to put in an Iron Prince Scourer. With best wishes and the compliments of the season, we remain,

Yours truly,

[Signed] BIDDLE & ELLSWORTH, LTD.

THE STURGIS MILLING CO.
Flour, Feed and Grain.

STURGIS, MICH., Jan. 3, 1899.

Prinz & Rau Mfg. Co.,
Milwaukee, Wis.

Gentlemen: Your favor received. Would say in response that thus far your Iron Prince Scourer has been working very satisfactorily, and we have no reason to believe that it will not continue to do so.

Wishing you a good year's business.

Yours truly,

[Signed] STURGIS MILLING CO.

The Prinz & Rau Manufacturing Co.
Milwaukee, Wis.

Figure 16. The Northwestern Miller, 8 February 1899.⁷³

22 August 1901. Johnson City Comet.

“Putting in Siding. The Southern Railway is putting in a siding for Biddle & Ellsworth’s flouring mill and J.A. Martin’s lumber yard. The sidetrack will cut out just below the bridge at the tannery and run down the east side of the main line below the mill. The road bed is being graded and the ties and iron is on the ground.”

For Sale--CORD WOOD

**At mill or delivered
anywhere in the city**

FOR CASH

'Phone 5 BIDDLE & ELLSWORTH

Figure 17. Johnson City Comet, 11 February 1904.
This advertisement ran until at least 17 March 1904.

15 October 1903. Johnson City Comet.

“Engine Runs Away. The stationary engine at the Biddle & Ellsworth roller mills got tired of taking things easy Friday and, breaking a bolt, tried to see how fast it could run. It never stopped until a cylinder head was blown out and other parts badly broken. The wreck was complete and the engineer’s life was saved perhaps by being called out just before the accident occurred.”

1 December 1904. American Miller.

“Biddle & Ellsworth, proprietors of the Johnson City Mills at Johnson City, Tenn., have put in a complete roller process cornmeal outfit.”⁷⁴

21 September 1905. Johnson City Comet.

“Flouring Mill Burned. Tuesday night about 1 o’clock the Biddle & Ellsworth roller mill was destroyed by fire. The fire was discovered in the engine room and the alarm was sounded by locomotive whistles in the Carnegie yards. After the fire department arrived it did not have hose enough to reach the plant from the plug on Watauga Avenue and before additional hose could be secured the fire had made so much headway that it was impossible to extinguish it. The building, fixtures and about 3000 bushels of grain were consumed with only \$12,000 insurance.”

28 September 1905. Manufacturers Record.

“Burned... Johnson City, Tenn. – Biddle & Ellsworth’s flour mill; loss \$25,000.”⁷⁵

5 October 1905. Manufacturers Record.

“Johnson City – Flour Mill. – Biddle & Ellsworth, Ltd., will erect a brick building, 42 x 55 feet, of ordinary construction, equipping as flour mill, replacing plant reported burned last week.”⁷⁶

October 1905. Flour & Feed.

“The mill of Biddle & Ellsworth, at Johnson City, Tenn., burned a day or two ago. Loss, \$20,000; insurance, \$11,000. The mill did a large local business. It is understood that it will be replaced.”⁷⁷

October 1905. Engineering World.

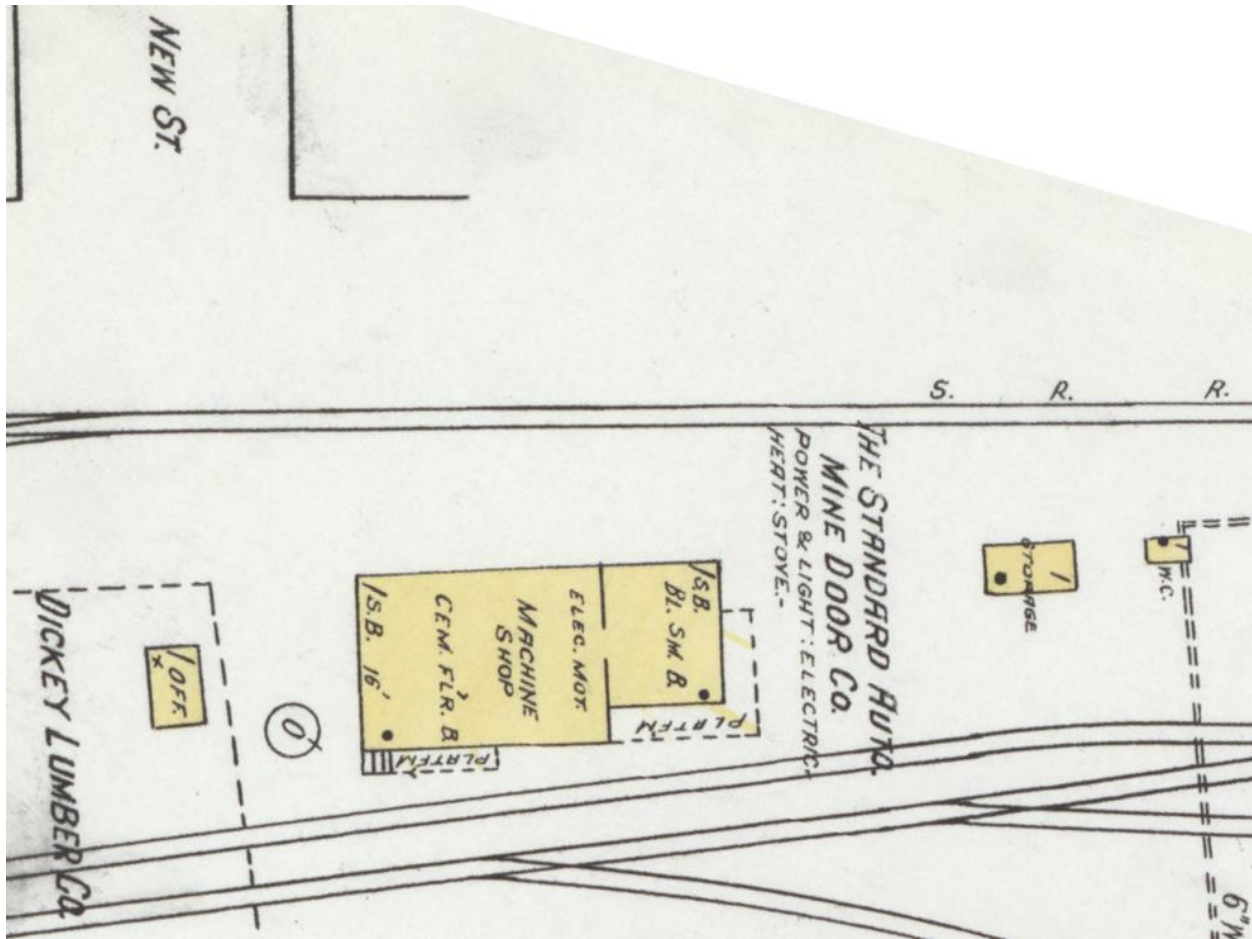
“Johnson City, Tenn. – The large flouring mill of Biddle & Ellsworth was destroyed by fire. Loss, \$20,000. It will probably be rebuilt.”⁷⁸

3.2.3. A.B. Biddle & Company, 1905

2 November 1905. Johnson City Comet.

“Will Build Flouring Mill. When the Biddle & Ellsworth mill was destroyed by fire several weeks ago it was not known whether it would be rebuilt. Since then a company has been formed to be styled A.B. Biddle & Company and will erect a modern mill. The plant will be of brick with the latest improved equipment. Mr. Biddle has associated with him several experienced mills. The capacity of the new plant will be 125 barrels a day.” This is the last mention of this mill that the author has found in the Johnson City Comet; apparently, this flour mill was never rebuilt.

3.2.4. The Standard Automatic Mine Door Company, by 1920 to at least 1922

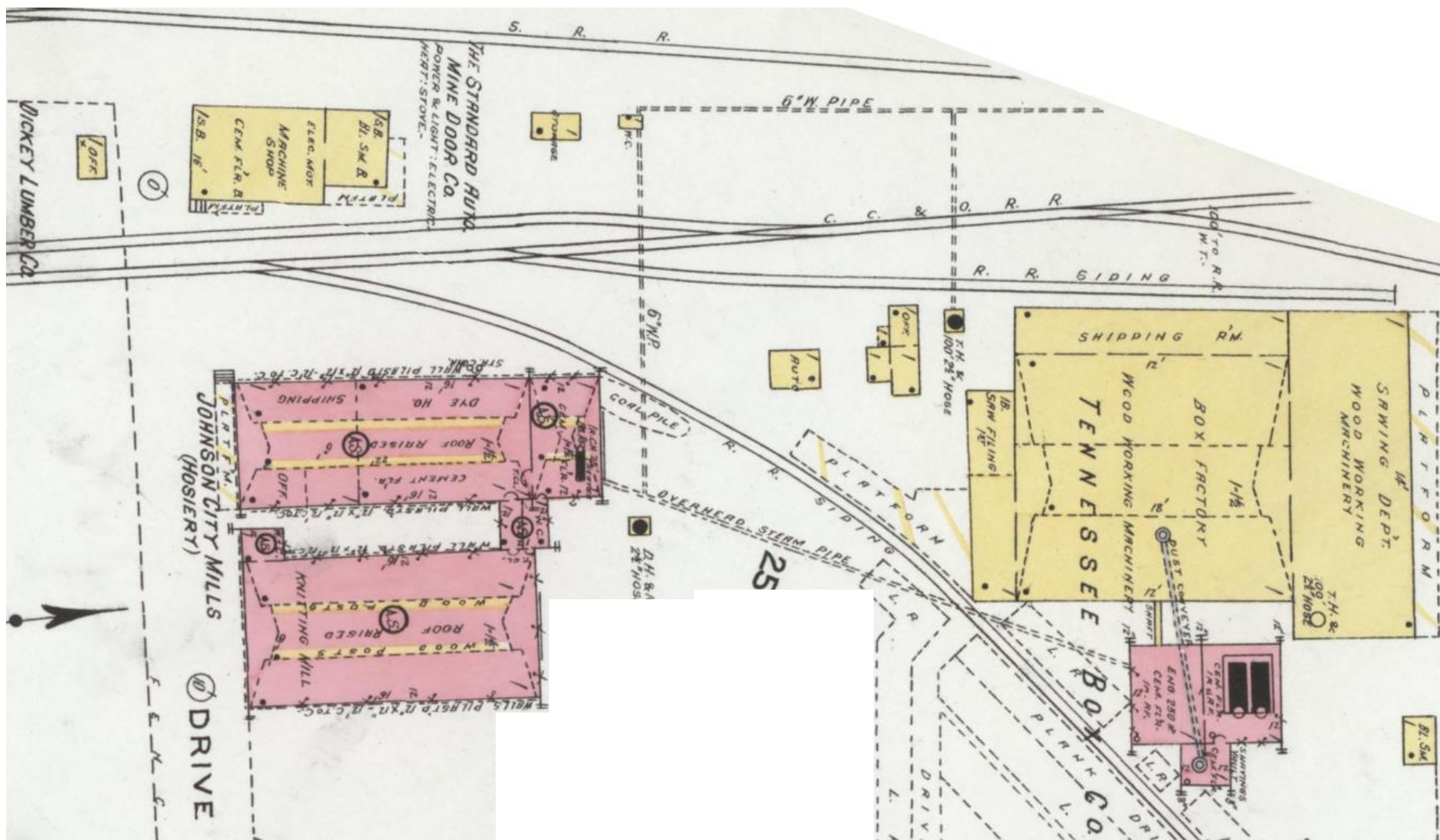


Map 12B. The “Dickey Lumber Co.” and “The Standard Auto. Mine Door Co.”. Note that “The Standard Auto. Mine Door Co.” appears to have the same footprint as the Biddle & Ellsworth flour mill.

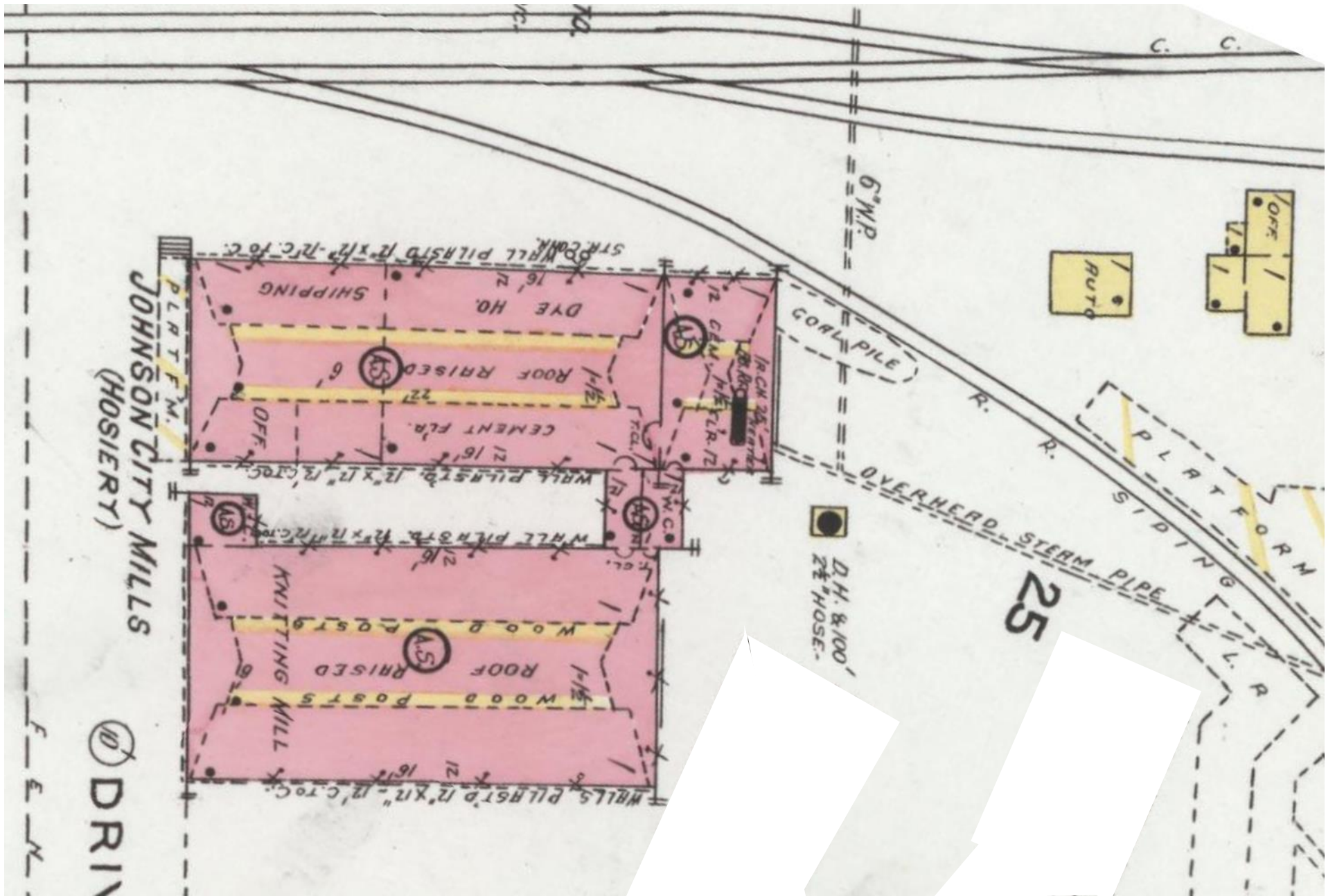
January 1922. Industrial Employment Survey Bulletin.

“Johnson City – Although building operations continue active, the shortage of dwellings is not yet relieved. Building operations and sewerage extensions employing about 500 men. Woodworking plants operating full time. A large chemical plant and automatic mine door plant remain closed. A pig-iron furnace, silk throwsters mill, and a lumber and veneer plant are temporarily closed. A dye plant has been reopened. The greatest amount of unemployment exists among common labor.”⁷⁹

3.3. Johnson City Mills (Hosiery), 1916 – 1960s



Map 12C. The Dickey Lumber Company, the Standard Automatic Mine Door Company, the Johnson City Mills, and the Tennessee Box Company (from left to right) at the junction of New Street and the Southern Railway mainline, February 1920.⁸⁰ The area covered by this map was not included in the July 1913 edition of the Sanborn maps.



Map 12D. A detail of the Johnson City Hosiery Mills in February 1920, see figures 18 and 19.⁸¹

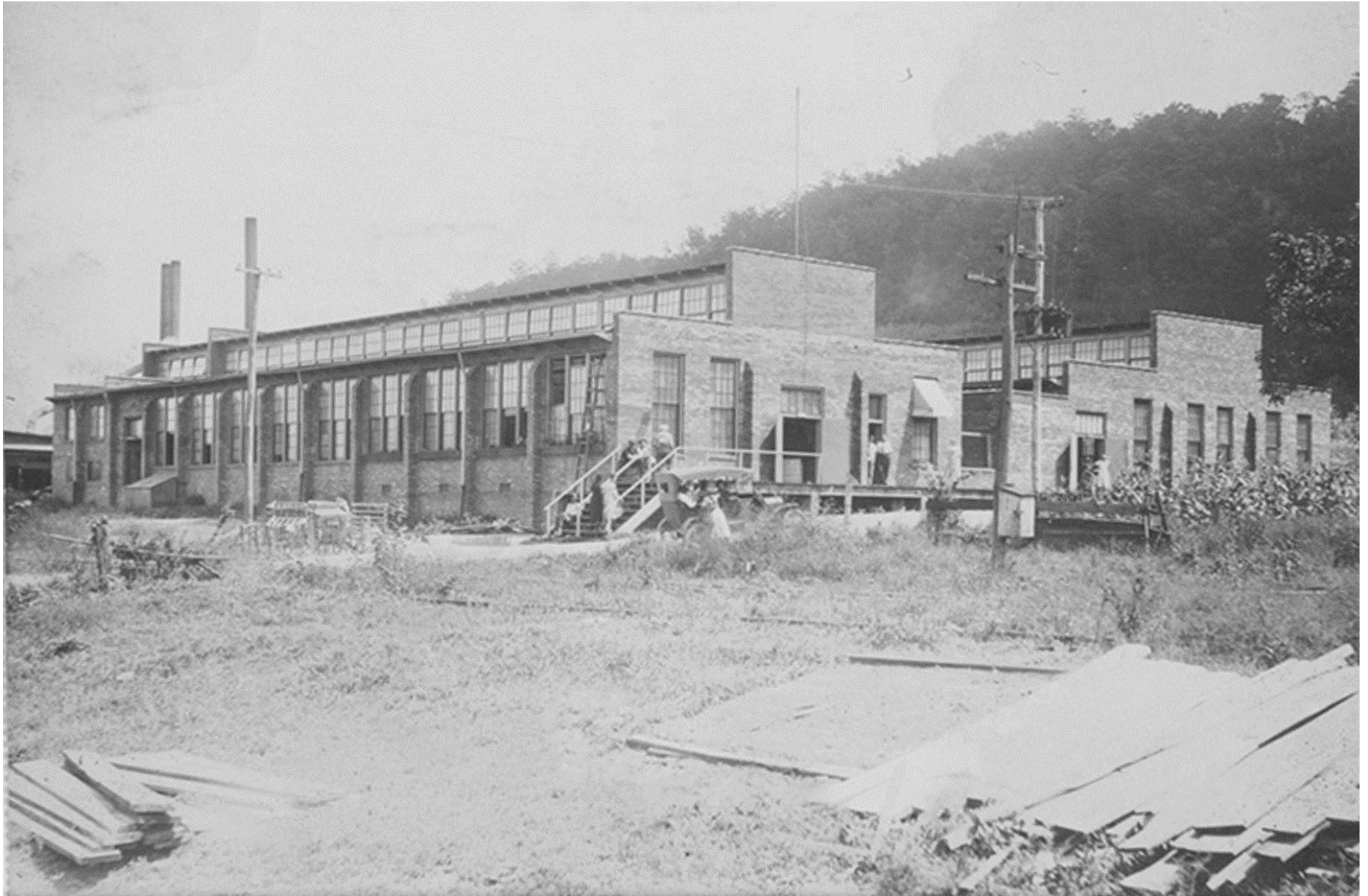


Figure 18. "Hosiery Mills" looking northeast.⁸² Note the grass-covered standard gauge tracks running across this photograph. Also note what appears to be the Tennessee Box Company plant in the left background. The "Dye Ho." is in the foreground and the "Knitting Mill" is in the right background.



Figure 19. "New Street Plant, Johnson City Mills" looking northwest.⁸³ The "Knitting Mill" is on the left and the "Dye Ho." is on the right. Note the overhead steam pipe, attached to the "Dye Ho." on the right. This pipe was connected to the boiler house of the Tennessee Box Company. The small, light-colored, building housed a "D.H." (Dry Hydrant? Double Hydrant?), see Map 12D.

3.3. Johnson City Mills (Hosiery), 1916 – 1960s

19 October 1916. Johnson City Comet.

“\$50,000 Knitting Mill a Certainty for the City – A charter has been granted to the Johnson City Mills, a corporation with a capital stock of \$50,000.

The incorporators are Ben. A. Morton, C.L. Dooley, J.P. Ready, G.H. Gallagher and T.F. Dooley.

The incorporators are business men of experience, and it has been stated that work on the building has begun. We hope to be able to make further announcements in our next issue.”

1916. American Wool and Cotton Reporter.

“Johnson City Mills, Incorporated 1916. Capital \$75,000. Ben. A. Morton, president; T.F. Dooley, manager, superintendent and buyer. Production and equipment: Misses’ cotton ribbed seamless hosiery, 100 knitting, 100 ribbing, 25 looping machines, dye, bleach, dye, finish, 1 boiler, electric. Employ 125. Buy 24, 30 carded, 24, 30 combed peeler, 36 50-2 combed peeler lisle. Sell jobbers. New st. and C.C.&O. industrial track.”⁸⁴

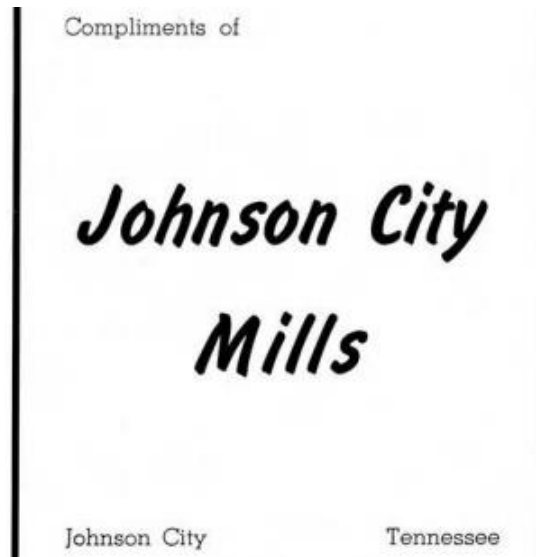


Figure 20. The Buccaneer, 1942.⁸⁵ Added 24 September 2024.

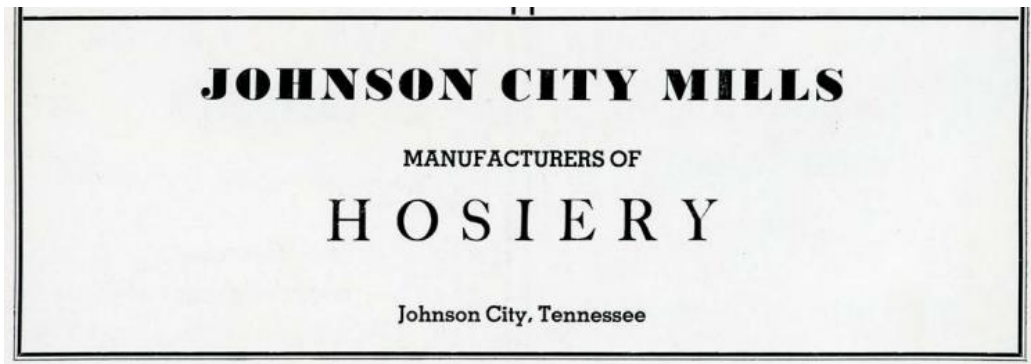


Figure 21. The Buccaneer, 1948.⁸⁶ Added 24 September 2024.

Table 11. “Directory of Industries Inspected, with No. Employes, December 1, 1916, to December 31st, 1917... Washington County – Johnson City”.⁸⁷

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Hosiery Mfrs.	6	50	1	3

Table 12. “Directory of Industries Inspected, with No. Employes... Washington Co. – Johnson City”, for the Year Ending 31 December 1918.⁸⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Hosiery Mfrs.	15	89		11

Table 13. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1919.⁸⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfrs. Hosiery	16	87		

Table 14. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1921.⁹⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Hosiery Mfg.	6	51		

Table 15. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.⁹¹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mill	Hosiery Mfg.	14	89		12

Table 16. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1925.⁹²

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	21	82		

Table 17. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1928.⁹³

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	24	87		

Table 18. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1929.⁹⁴

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	28	142		

Table 19. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1930.⁹⁵

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	22	151		

Table 20. “Location and List of Industries Inspected 1933... Washington County – Johnson City”, for the Year Ending 31 December 1933.⁹⁶

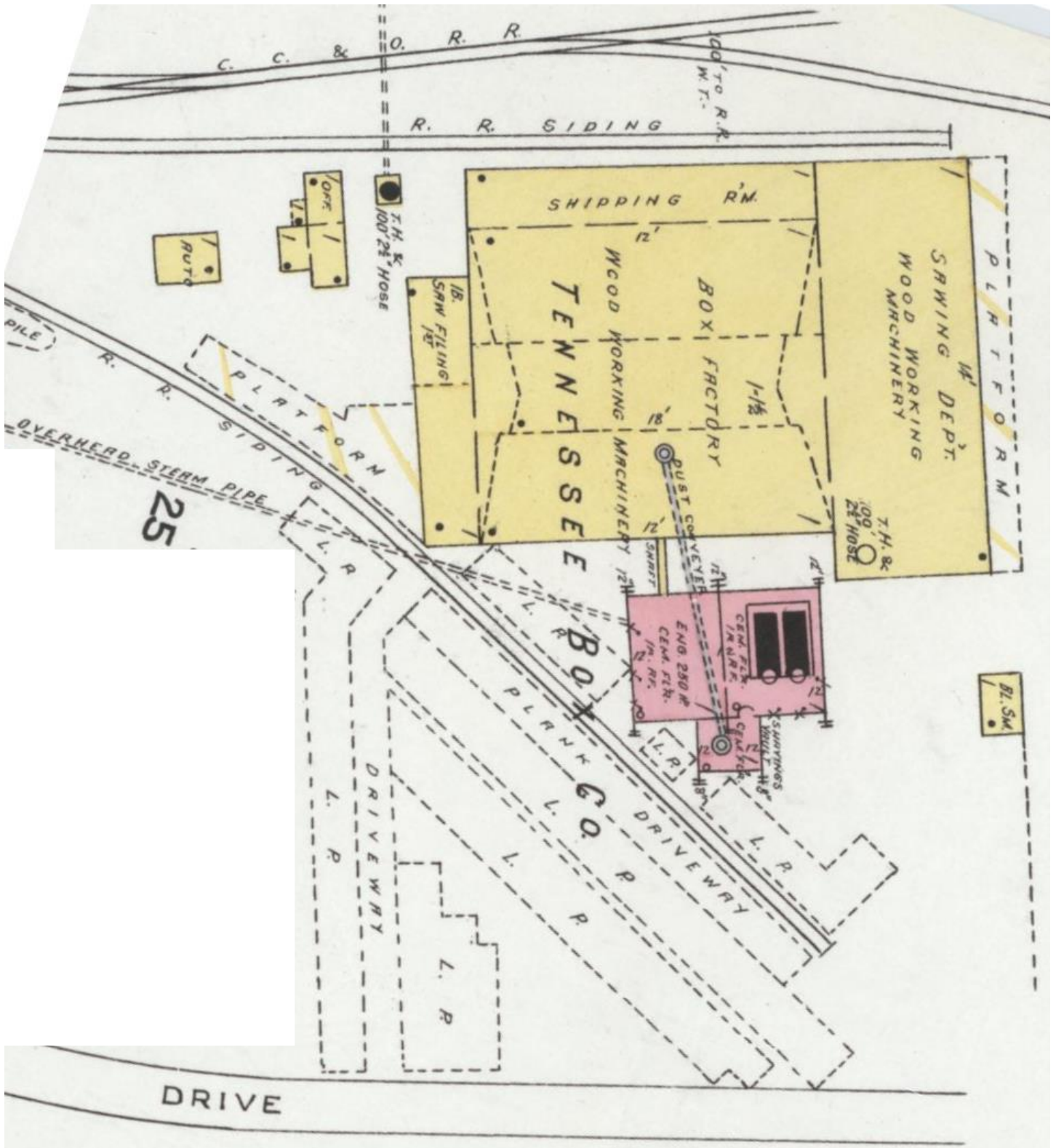
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	17	65		

Table 21. “Location and List of Industries Inspected 1935... Washington County – Johnson City”, for the Year Ending 31 December 1935.⁹⁷

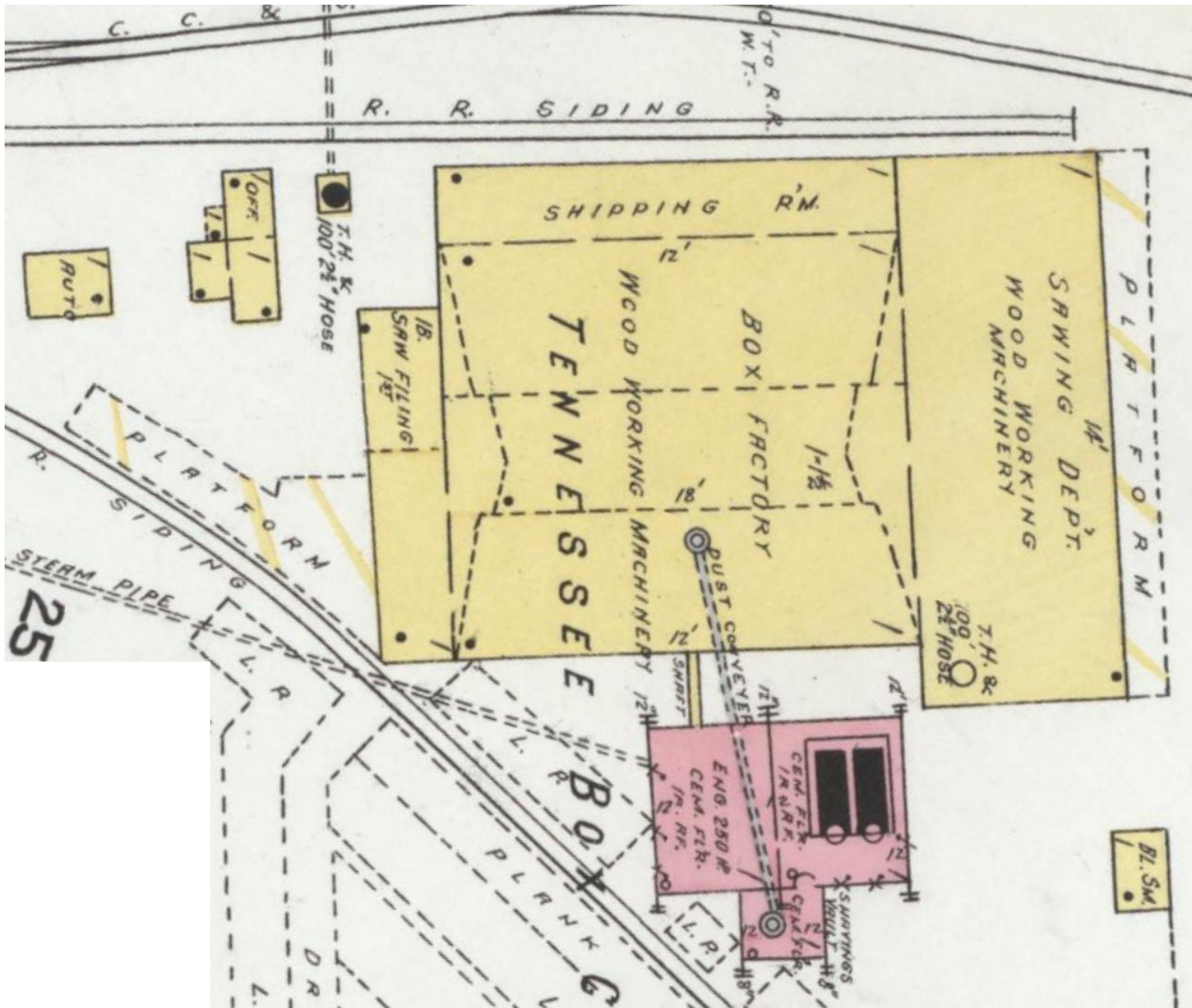
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Mills	Mfg. Hosiery	14	104		

3.4. The Area Occupied by the Tennessee Box Company and Related Enterprises,
1915 to at least 1950

Tennessee Box Company, 1915 – 1926
Tennessee Box & Lumber Company, 1926 to at least 1950



Map 12E. A detail of the Tennessee Box Company, in February 1920.⁹⁸ Note the marking "100' to R.R. W.T." (railroad water tank) at the top of this map.



Map 12F. Another detail of the Tennessee Box Company, in February 1920.



Figure 22. It appears that the enterprise in this photograph was owned by the Tennessee Box Company, “c. 1920,”⁹⁹ Note the railroad water tank, 100 feet behind the first building on the left, as well as the two smokestacks and the cyclone. See Map 12F.



Figure 22A. A detail of the Tennessee Box Company. The first building on the left appears to stand on the ground previously occupied by the office and is not depicted on the Sanborn map of February 1920; and thus, it has not been identified. The middle building appears to be the “Box Factory”, and the building to the right appears to be a boiler house.



Figure 23. Tennessee Box Company “New Street and Southern Railway”.¹⁰⁰ Note the Wilder Block and the old Clinchfield Freight Depot, on the far right. Also note the railroad water tank behind the first building on the left, the two smokestacks and the cyclone (which appears to have been repositioned). This photograph appears to be of the same facility shown in Figure 22, just at a later date, perhaps after the 1926 fire.

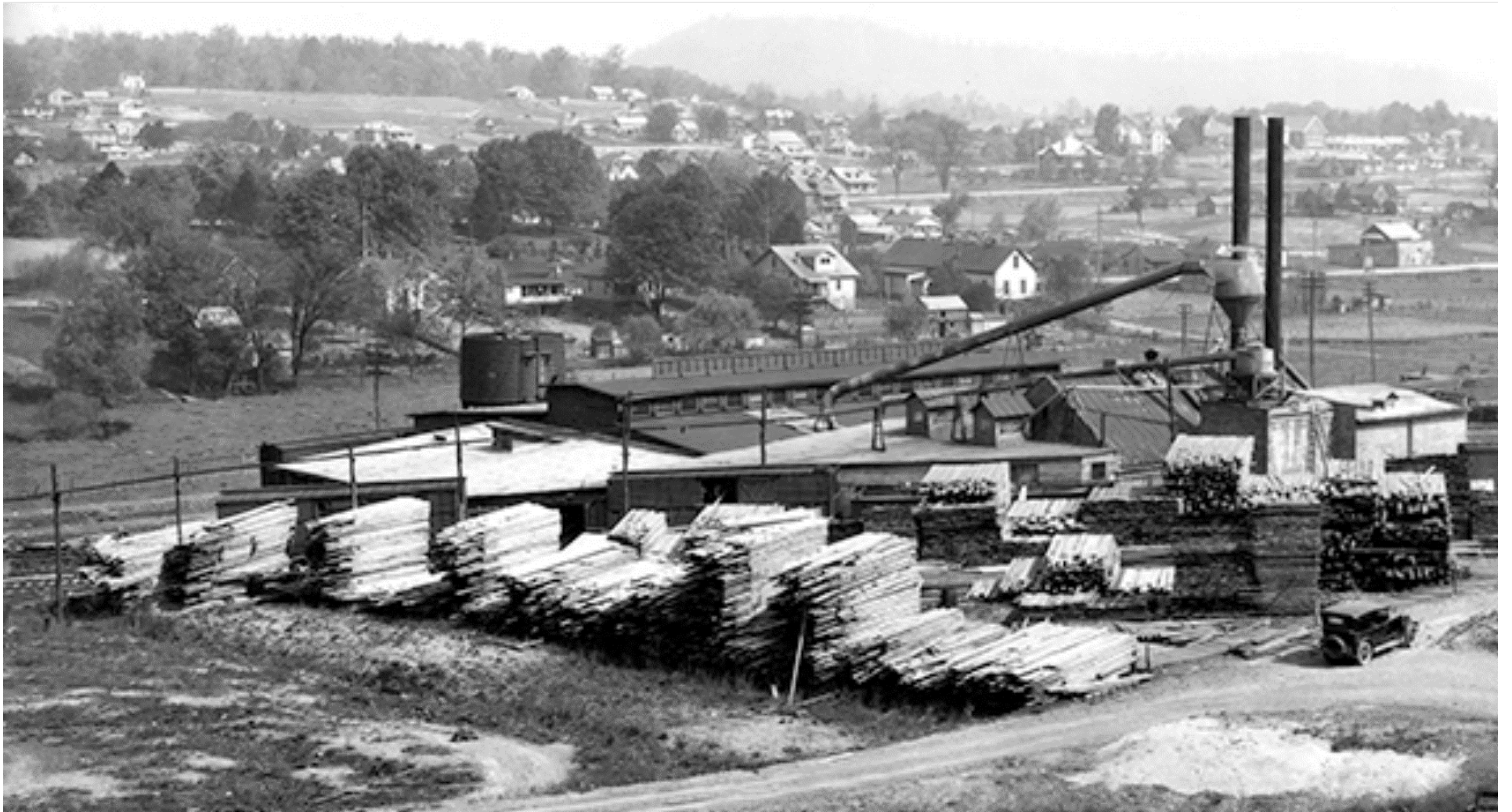


Figure 23A. A detail of the Tennessee Box Company.



Figure 23B. Another detail of the Tennessee Box Company. Note the railroad water tank on the left. The building with the clerestory roof appears to still be the “Box Factory” and the building with the two smokestacks and cyclone still appears to be a boiler house. Also note the railroad cars on the spur in front of the “Box Factory”.



Figure 23C. Another detail of the Tennessee Box Company, which appears to include the office. The Wilder block, the “old C.C. & O. Depot”, the office of the Southern Bending Company/Unaka Flooring Company, and, perhaps, the iron-clad building of the Tennessee Carving & Wood Working Company that was “later used as a tenement” are visible on the right side of this image. Note the boxcars on the spur behind and to the left of the office.



Figure 24. Captioned: “Carnegie Yard – ET interchange – March 1943”. However, it appears that this photograph was taken looking west, toward the area that was occupied by the Tennessee Box Company, which was adjacent to a railroad water tank (like the one on the left of this image, see figures 22A and 23B). If so, this image was taken near the Clinchfield and Southern crossing, rather than that of the Southern and the E.T. & W.N.C., which was further east. Note the apparent lack of narrow-gauge freight cars in this yard which is at least four tracks wide.¹⁰¹

Tennessee Box Company, 1915 – 1926
Tennessee Box & Lumber Company, 1926 to at least 1950

3.4.1. Tennessee Box Company, 1915 – 1926

19 August 1915. Johnson City Comet.

“Box Factory New Industry in this City – Officers: Wm. H. Lauff, Ford Jones, F. M’Neil – Factory Run Full Time – This Adds One More New Industry to Johnson City’s New Enterprises. It will be Constructed in Carnegie. – It is said that in ninety days the box factory will be running full time, in its new home in Carnegie, which will be one hundred feet square and filled with the best machinery obtainable.

It will require skilled labor to run this new industry, which means the coming of at least about 30 new citizens, besides giving work to a great number of home people.

Mr. Ford Jones said Johnson City was an ideal place and held out many advantages over any place he had seen. The credit for this new industry coming here is due the officials of the C.C. & O. railroad and Mr. A.B. Crouch of the Unaka National Bank.

The officers of the Box Company, who are from Chicago, are Mr. Wm. H. Lauff, Ford Jones and Frank McNeil. These gentlemen, in speaking of Johnson City and its people, said in all their travels they had never had the pleasure of meeting more perfect gentlemen than Messrs. A.B. Crouch, C.L. Marshall, J.A. Summers... Brewer and Johnson.

It was after investigating claims of these gentlemen and finding them to be all they had represented to be that they located here.

We have the location and advantages and if we will only let industries seeking locations know our advantages they will all say as these gentlemen are saying.”

2 September 1915. Johnson City Comet.

“A representative of the box factory is in the city making preparations for the buildings which are to be erected at once. The plans for the buildings have been drawn and the company hopes that everything will be in readiness to operate within ninety days. Two large buildings will be erected and others added as the business grows. The representatives state that it will be one of the leading industries in this city within a short time.”

25 September 1915. Hardwood Record.

“Chicago capitalists will equip a box factory at a cost of \$10,000 at Johnson City, Tenn. It is reported that the factory will make only tobacco boxes at the start, but probably will later turn out boxes of other kinds.”¹⁰²

September 1915. Barrel and Box.

“Tennessee Box Co., Johnson City, Tenn., has been organized by Ford Jones, Frank McNeill and Wm. F. Lauff, of Chicago. The company will make a specialty of tobacco and knitting mill boxes, and contemplates operating about Jan. 1. Arrangements are now being made for the erection of a plant.”¹⁰³

March 1916. Barrel and Box.

“Wanted – One or more No. 4½ Dolg Cleaters. Address Tennessee Box Company, Johnson City, Tenn.”¹⁰⁴

30 June 1916. Sixth Annual Report – Carolina, Clinchfield and Ohio Railway...

Table 22. “List of New Industries Established during Year Ended June 30, 1916”¹⁰⁵

Shipping Point	Name	Character
Johnson City	Tennessee Box Company	Box and box shooks

Table 23. “Sidings, Spurs and Additional Yard tracks Built During Year Ended June 30, 1916.”¹⁰⁶

Mile Post		Miles
119.0	Commercial siding for Tennessee Box Company	.090

25 July 1918. Hardwood Record.

“The Tennessee Box Company, Johnson City, reports an active box situation in upper east Tennessee.”¹⁰⁷

31 January 1920. American Lumberman.

“Wanted – 40 Inch Upright Discharge Exhaust Fan. Not less than 18-inch intake. Tennessee Box. Co., Johnson City, Tenn.”¹⁰⁸

22 April 1922. Southern Lumberman.

“We Want to Move Two cars of 4 – 4 No. 2 Basswood, one car of 4 – 4 2-A Poplar, and one car 4 – 4 Log Run Ash. Tennessee Box Co., Johnson City, Tenn.”¹⁰⁹

1922. Southern Lumberman’s Directory of American Lumber Consuming Factories.

“Johnson City... Pop 8,500...”

The Tennessee Box Co., box shooks. Basswood, beech, birch, buckeye, chestnut, cottonwood, black gum, hemlock, longleaf, shortleaf, N.C. and white pine, poplar, tupelo...”¹¹⁰

June 1926. Packages.

“The Tennessee Box Co.’s plant at Johnson City, East Tenn., was destroyed by fire. Loss estimated at \$150,000. Only the boiler room was saved. The plant will be restored... Wm. H. Loft, president, will immediately begin rebuilding...”¹¹¹

July 1926. Packages.

“Rebuild Box Plant. Johnson City, Tenn. – Reconstruction of the Tennessee Box Factory, destroyed by fire recently, is to begin at once, according to announcement just made by officials. The new building is to occupy the site of the former plant, the debris from which has been cleared away, after rapid work and adjustments. Construction of the building is expected to be rapid, and completed within less than 60 days. New machinery has been ordered, and it is stated that this will be here, ready for installation, by the time the building is completed.

The factory is ready to continue its work on full time as soon as building and machinery are ready, as orders are on hand awaiting filling. During the idle period, important orders were taken care of by other manufacturers.”¹¹²

August 1926. Packages.

“New Plant About Ready – Tennessee Box Co. Expects to Move into New Mill at Johnson City Tenn., Sept. 1. Sept. 1 is the date set by Tennessee Box Co. officials for the resumption of operations in the new plant at Johnson City, Tenn., active work on which was begun recently.

The new main building when completed will be two stories in height in the part measuring 58 by 155 feet, ground floor dimensions and one story in the building 155 by 156.

The manufacturing plant will be rebuilt with heavy framing timbers. The boiler room of brick construction has already been repaired and re-roofed.

The plant equipped and ready to manufacture boxes will cost in the neighborhood of \$100,000, according to the estimate of T.T. Zimmerman, contractor in charge of the work.”¹¹³

3.4.2. Tennessee Box & Lumber Company, 1926 to at least 1950

December 1926. Packages.

“Johnson City, Tenn. – The Tennessee Box Co. has changed name to Tennessee Box & Lumber Co.”¹¹⁴

February 1928. The Packages.

“Acquire Plant – The lumber plant formerly occupied by the Von Cannon Lumber Co. at West Walnut and Tennessee streets, Johnson City, Tenn., has been acquired by the Tennessee Box Co. of which Frank McNeill is president. Machinery will be installed which will double the present capacity of the plant.”¹¹⁵

1932. Manufacturers Record.

“Tenn., Johnson City – Pat Reilly, Tennessee Box Co., reported, rebuild burned plant.”¹¹⁶

Table 24. “Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County”, for the Year Ending 1 December 1916.¹¹⁷

Firm Name	Character of Business	No. Employes at time of inspection				No. Hours Labor Males		No. Hours Female and Min. Under 16		General Condition
		Over 16		Under 16		Over 16		Min. Under 16		
		M.	F.	M.	F.	Da.	Wk.	Da.	Wk.	
Tenn. Box Co., The	Box Mfrs.	68		55		10¾	60	10¾	58	Good

Table 25. “Directory of Industries Inspected, with No. Employes, December 1, 1916, to December 31st, 1917... Washington County – Johnson City”.¹¹⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Packing Boxes, Mfrs.	83			

Table 26. “Directory of Industries Inspected, with No. Employes... Washington Co. – Johnson City”, for the Year Ending 31 December 1918.¹¹⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Box Mfrs.	106		8	

Table 27. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1919.¹²⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Mfrs. Wood Boxes	91			

Table 28. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1921.¹²¹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Box Mfg.	105	1		

Table 29. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.¹²²

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Box and Box Shooks	85			

Table 30. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1928. Note: beginning in 1928, the Tennessee Box Company leased the Von Cannon plant on West Walnut Street in Johnson City. Consequently, 72 “employes” may reflect the combined work force at both plants. The Von Cannon suffered a fire in 1931 and may not have been rebuilt.¹²³

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co.	Lumber	72			

Table 31. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1929.¹²⁴ See note with Table 30.

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box & Lumber Co.	Mfg. Wooden Boxes	87	15		

Table 32. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1930.¹²⁵ See note with Table 30.

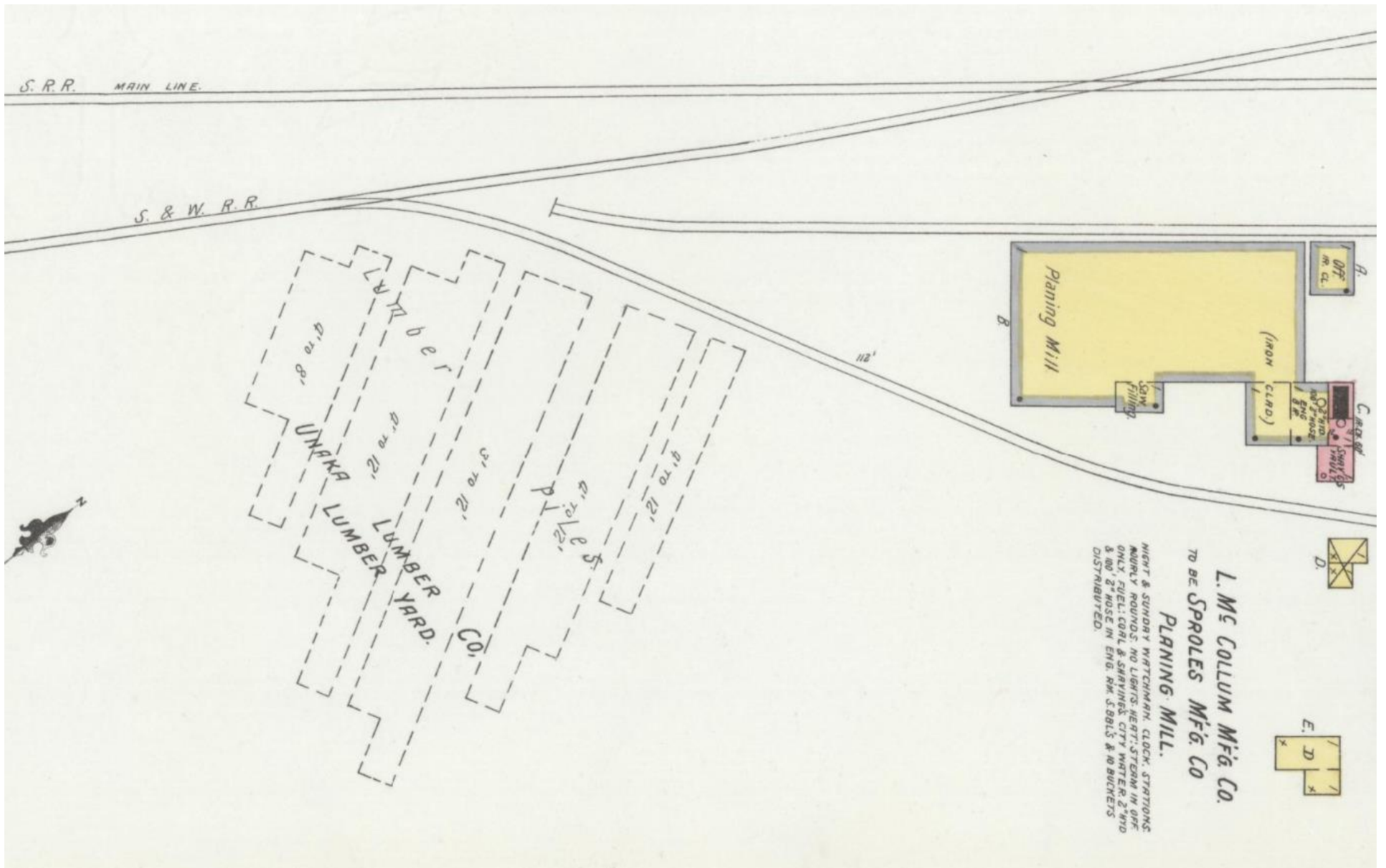
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box & Lumber Co.	Mfg. Wooden Boxes	56	8		

Table 33. “Location and List of Industries Inspected 1935... Washington County – Johnson City”, for the Year Ending 31 December 1935.¹²⁶

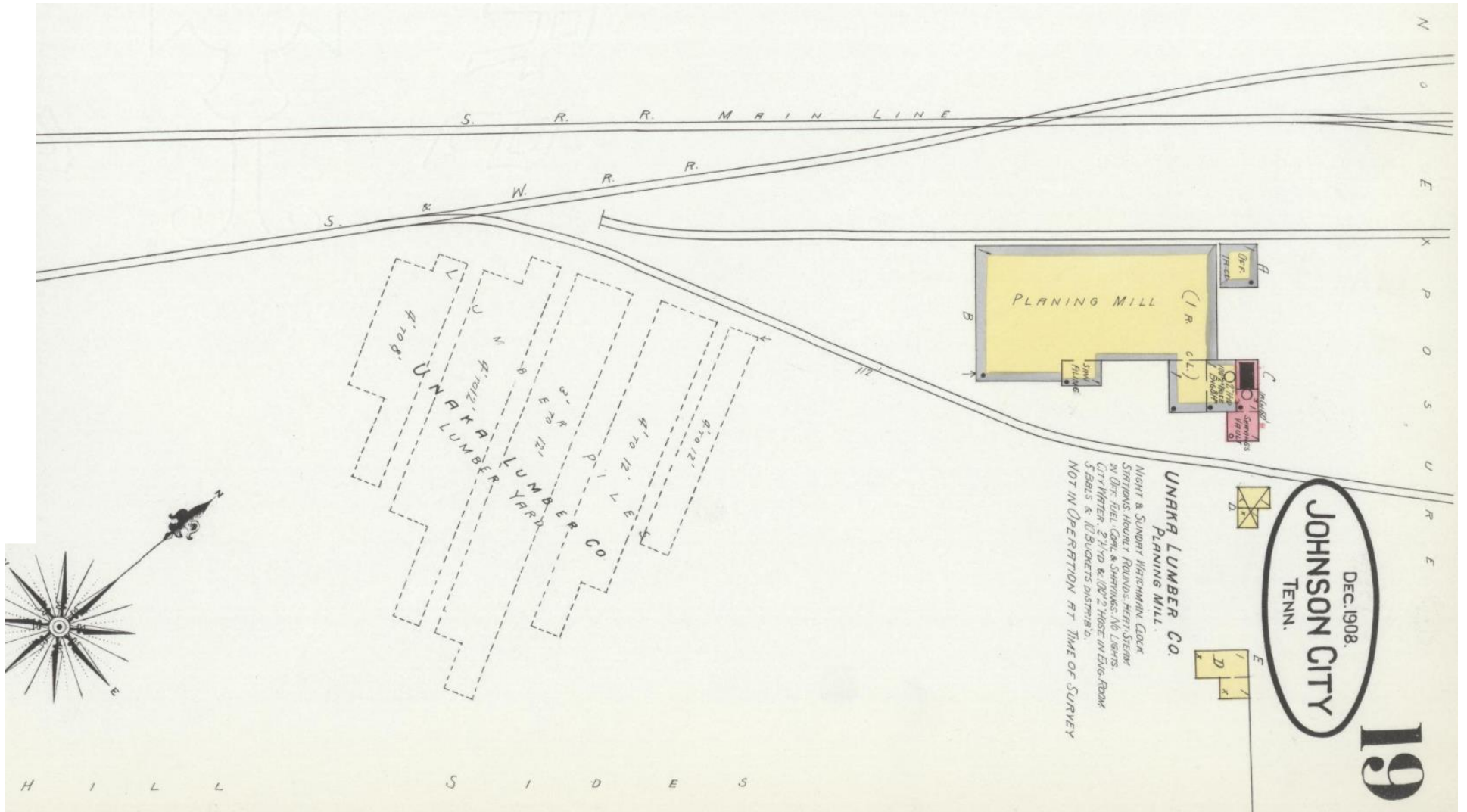
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Tennessee Box Co., Inc.	Mfg. Wooden Boxes	38			

3.5. The Area Occupied by the L. McCollum Manufacturing Company and Related Enterprises,
1900 – 1911

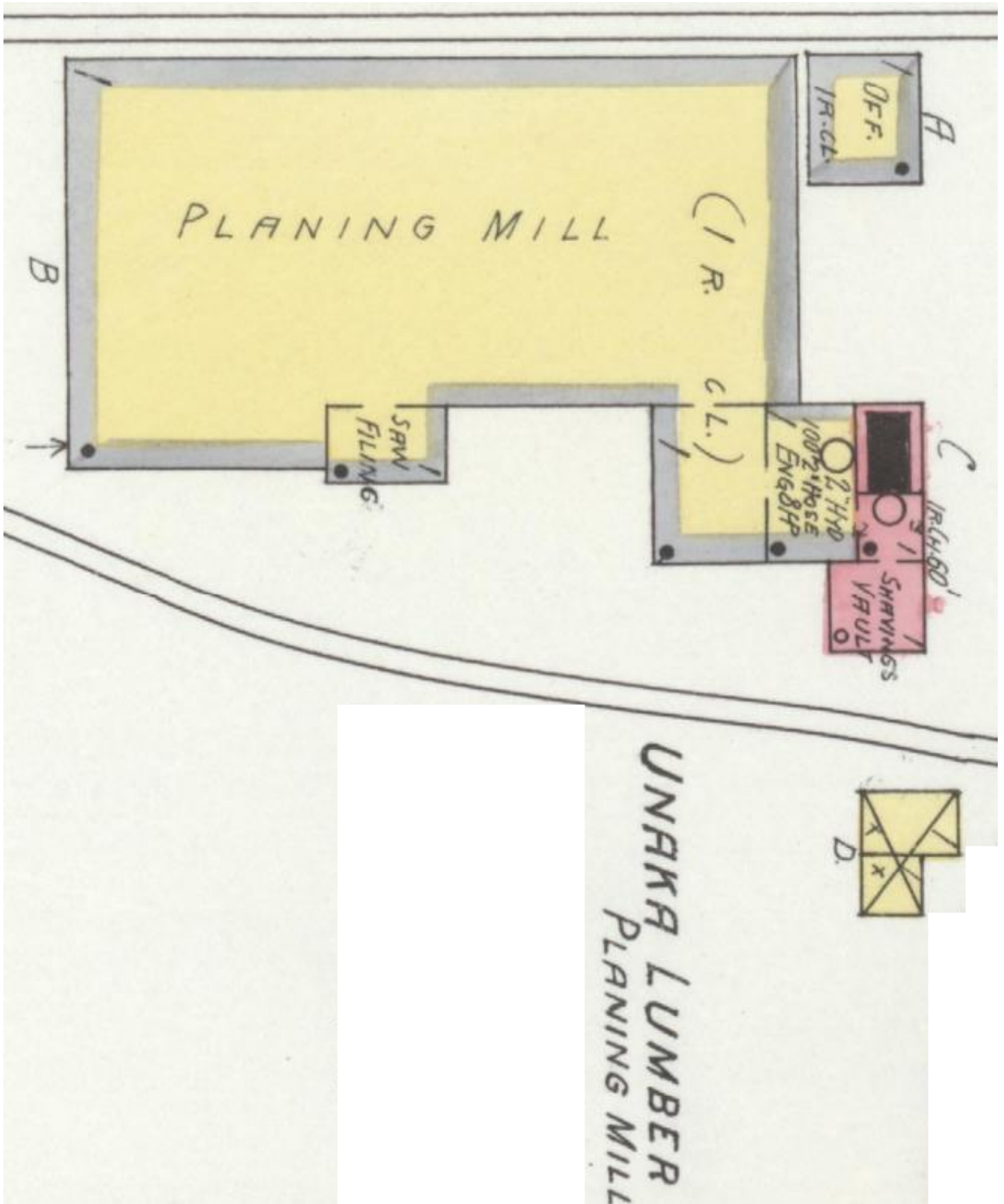
L. McCollum Manufacturing Company, 1900 – 1902
Sproles Manufacturing Company, 1902 – 1905?
Unaka Lumber Company, at this location, 1905 – 1911
(And the Company's nearby lumber yard, by 1903 to at least 1908)



Map 9C. A detail of the “Unaka Lumber Co. Lumber Yard” and the “L. McCollum M’F’G. Co. to be Sproles M’F’G Co. Planing Mill”, from left to right, in April 1903.



Map 13. A map of the “Unaka Lumber Co. Planing Mill” and lumber yard at the crossing of the Southern and South & Western railroads in Carnegie, in December 1908. Note that this planing mill was “not in operation at time of survey”.¹²⁷



Map 13A. A detail of the “Unaka Lumber Co. Planing Mill”, in December 1908. Note that there does not appear to have been any changes in comparison to the April 1903 map of this facility, see Map 9C.

L. McCollum Manufacturing Company, 1900 – 1902
Sproles Manufacturing Company, 1902 – 1905?
Unaka Lumber Company, at this location, 1905 – 1911
(And the Company's nearby lumber yard, by 1903 to at least 1908)

3.5.1. L. McCollum Manufacturing Company, 1900 – 1902

3 November 1900. American Lumberman.

“Southern News Notes. Dr. L. McCollum, of Johnson City, Tenn., and C.V. Burnett, of Ann Arbor, Mich., have purchased some tracts of timber land on the Ohio River & Charleston railway new extension, in North Carolina. A saw mill for the manufacture of small dimension furniture and chair stock, etc. will probably be built at Johnson City.”¹²⁸

15 November 1900. Johnson City Comet.

“Will Locate Here. Dr. L. McCollum and C.V. Burnett have decided to locate their plant in Johnson City for cutting dimension stock, spoken of in The Comet a few weeks ago. Two acres of ground have been purchased from the Carnegie Land Co., opposite the O.R. & C. depot and has been surveyed. The buildings will be erected at once. These gentlemen have a large body of timber in North Carolina on the O.R. & C. railway and will locate sawmills up there to cut it. The class of timber that cannot be marketed at a profit will be shipped to Johnson City and worked up in the plant here into box shooks and other dimension stock.”

22 November 1900. Johnson City Comet.

“To Purchase Machinery. Dr. L. McCollum left Tuesday for Ohio to purchase the necessary machinery to equip the manufacturing plant he is erecting in this city. He is anxious to get his plant in operation and will push it as rapidly as possible to completion.”

13 December 1900. Johnson City Comet.

“Work Begun. Dr. L. McCollum has begun the erection of his factory buildings at the intersection of the O.R. & C. and Southern roads. The main building will be 60 x 125 feet with a boiler and engine room in addition. The foundation is to be of stone and brick. The buildings are to be modern and substantial and will be hurried to completion.”

28 March 1901. Johnson City Comet.

“The engines were started at the L. McCollum Manufacturing Company's plant this week and the machinery is being trained up. The plant will be in full blast next week.”

3.5.2. Sproles Manufacturing Company, 1902 – 1905?

12 June 1902. Johnson City Comet.

“Dr. W.L. McCollum has sold his stock in the L. McCollum Mfg. Co. to Dr. W.J. [William J.] Sproles. The plant has been shut down for about two weeks.”

3.5.3. Unaka Lumber Company, at this location, 1905 – 1911
(And the Company’s nearby lumber yard, by 1903 to at least 1908)

12 January 1905. Johnson City Comet.

“Factory Purchased. The Unaka Lumber Company has purchased the L. McCollum Manufacturing Company’s plant at the junction of the S. & W. and Southern railways. As soon as the title papers are arranged the plant will be greatly enlarged and put in operation. The new owners are able to run it upon a large scale and will use it in connection with their band mills to manufacture dimension stock, etc.”

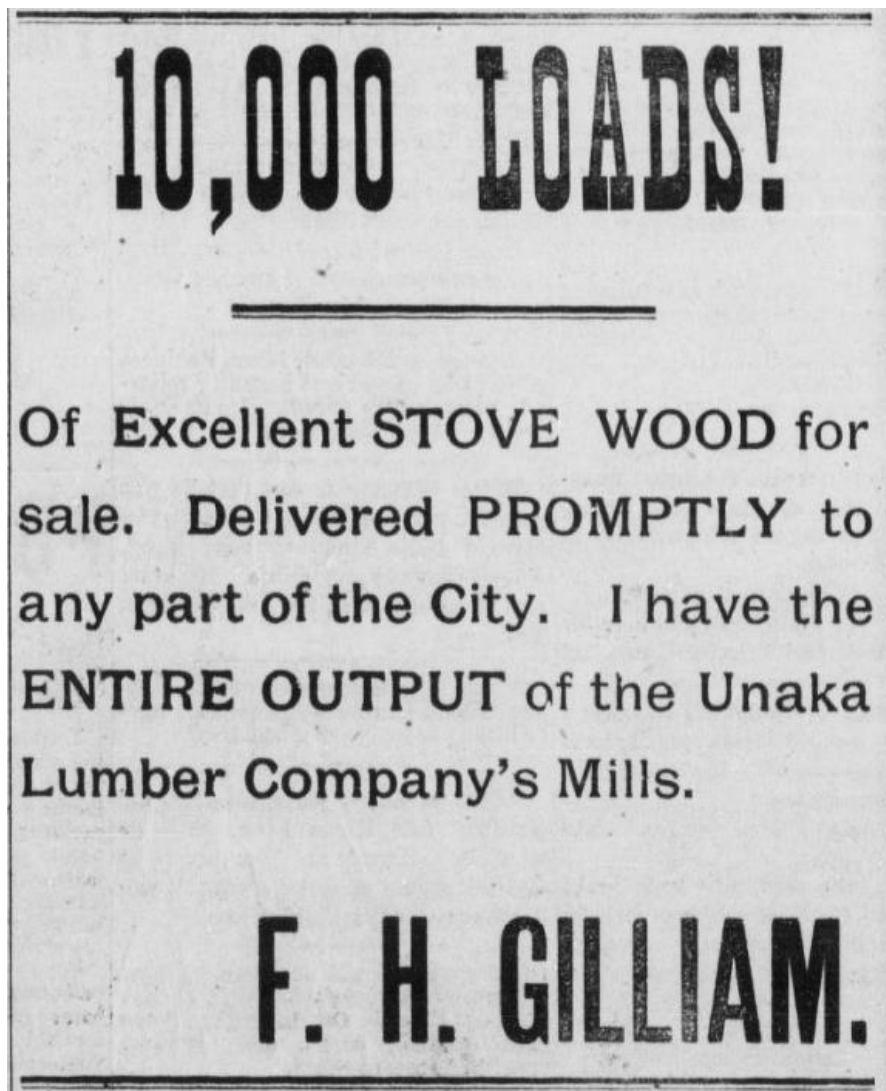


Figure 25. Johnson City Comet, 30 March 1905.
This advertisement ran until at least 10 August 1905.

— SEE —

J. D. COLLETTE


o—FOR—o

COAL and WOOD.

I can furnish you wood any length from

\$1.25 to \$2.00 Per Load---Cash on Delivery.

KINDLING A SPECIALTY.

Satisfaction Guaranteed.  Prompt Delivery.

I SOLICIT YOUR PATRONAGE.

J. D. COLLETTE

Office— On Unaka Lumber Co.'s Yard. 'Phone 134.

Figure 26. Johnson City Comet, 24 August 1905.

28 September 1905. Johnson City Comet.

“U.S. Archer, president of the Unaka Lumber Company, is now driving his new motor car, a 80-horse-power Winston, through the streets. It is a beauty and looks to be worth the \$2,800 it cost.”

Note: U.S. Archer was dead by 27 February 1908.

14 March 1907. Johnson City Comet.

“Unaka Lumber Company is again operating its plant in the Carnegie addition, and in addition to cutting quartered oak, is making wagon dimension stock, S.J. Taylor, of Indiana, has charge of the wagon cutting department.”

25 July 1907. Johnson City Comet.

“A fire hydrant will be placed at the Unaka Lumber Co.’s plant.”

31 August 1911. Johnson City Comet.

“Notice of sale of Real Estate. – In the District Court of the United States of the Eastern District of Tennessee, at Greenville, in the matter of Unaka Lumber Co. Bankrupt. In Bankruptcy No. 103.

By virtue of the power vested in me by decree of the court in the above styled cause, filed the 29th day of May, 1911, I will on the 2nd day of September, 1911, at the front door of the court house on Spring Street in Johnson City, Tennessee proceed to sell, to the highest and best bidder for cash in hand and in bar of the equity of redemption, the following described tracts of lying and being in the 9th Civil District of Washington county, Tennessee:

1. That parcel of land, containing about two acres, situated in Carnegie, fronting on the rights of way of the Southern and C.C. & O. Railways known as the McCollum and Unaka Lumber Company factory site, and being the property conveyed to the McCollum Manufacturing Company by Carnegie Development Company.

2. The three parcels of land, situated on the line the C.C. & C. railway, north of Johnson City, composing what is known as the ‘Klondike Site’ of Unaka Lumber Company, being the same tracts of land conveyed to Unaka Lumber Company by Carnegie Development Company by deed registered in Deed Book 85, page 369 in the Register’s office at Jonesboro, Tennessee, to which reference is made for more complete description.

3. Also a certain parcel of land lying in the Carnegie Addition to Johnson City, Tennessee, known as the Handle Factory site, being the property conveyed by deed of Cardwell to Unaka Lumber Company by deed dated January 26th, 1903, Registered in Deed Book 84, page 47, to which reference is here made.

For a more definite description of the property here offered for sale reference is here made to deed of trust dated the 25th day of May, 1908, executed by Unaka Lumber Company to S.C. Williams, Trustee, Registered in Deed of Trust book Book No. 22, pages 321 to 227 in the Registry for Washington County at Jonesboro, Tennessee, which deed of trust was filed in this cause on the 23rd day of February, 1911.

Said sale will be made subject to certain exceptions as to rights of way, etc., referred to in said deed of trust.

This 29th day of August, 1911. J.W. Cass, Trustee in Bankruptcy of Unaka Lumber Company.”

4. Enterprises South of the Southern Railway Yard in Carnegie,
Between the Southern/Clinchfield Crossover and Broadway, 1911 to at least 1929

Tennessee Carving & Wood Working Company, 1912 to at least 1915

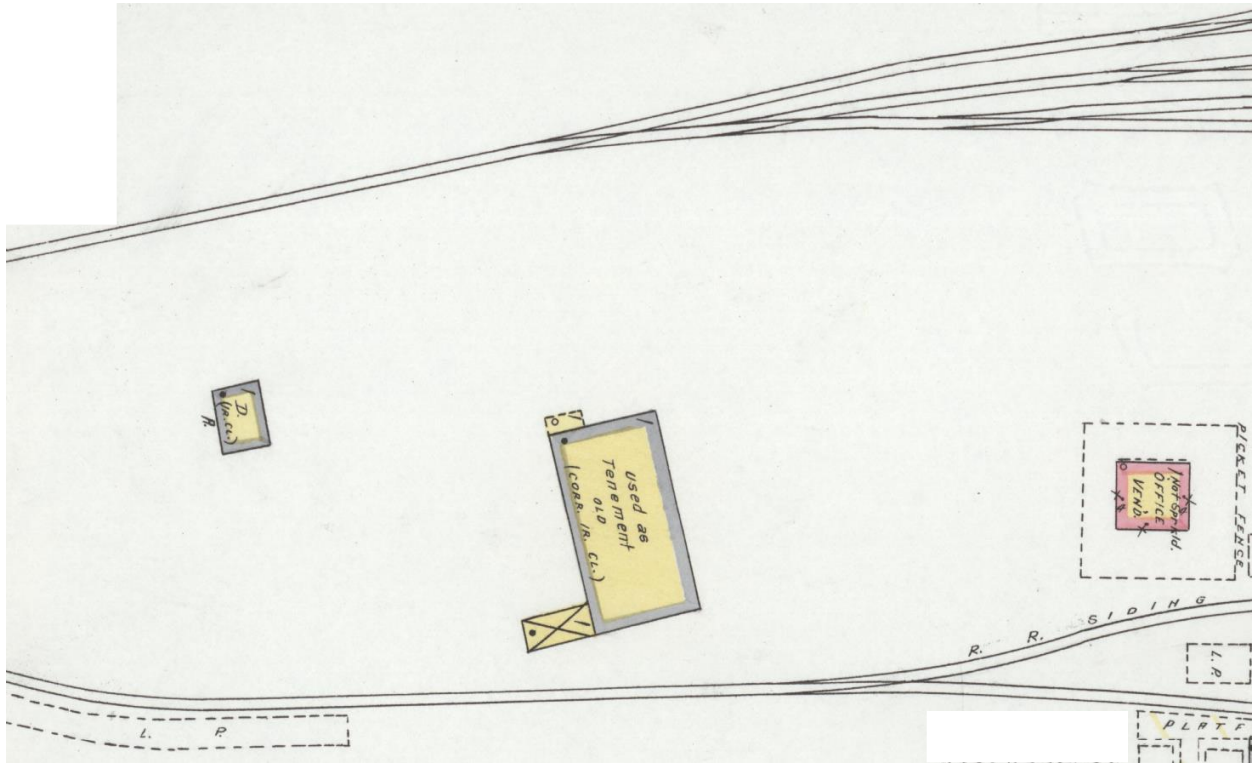
The Area Occupied by the Hice Manufacturing Company and Related Enterprises,
1911 to at least 1929

Hice Manufacturing Company, 1911 – 1916
Wood Products Corporation, 1916 – 1918
Southern Bending Company, 1918 – 1924 (1925?)
Unaka Oak Flooring Company, 1924 to at least 1929

The enterprises in this area were served by the Clinchfield from about 1911.

It does not appear that any of the buildings, covered by this documentary history in this area, survived to the present time.

4.1. Tennessee Carving & Wood Working Company, 1912 to at least 1915



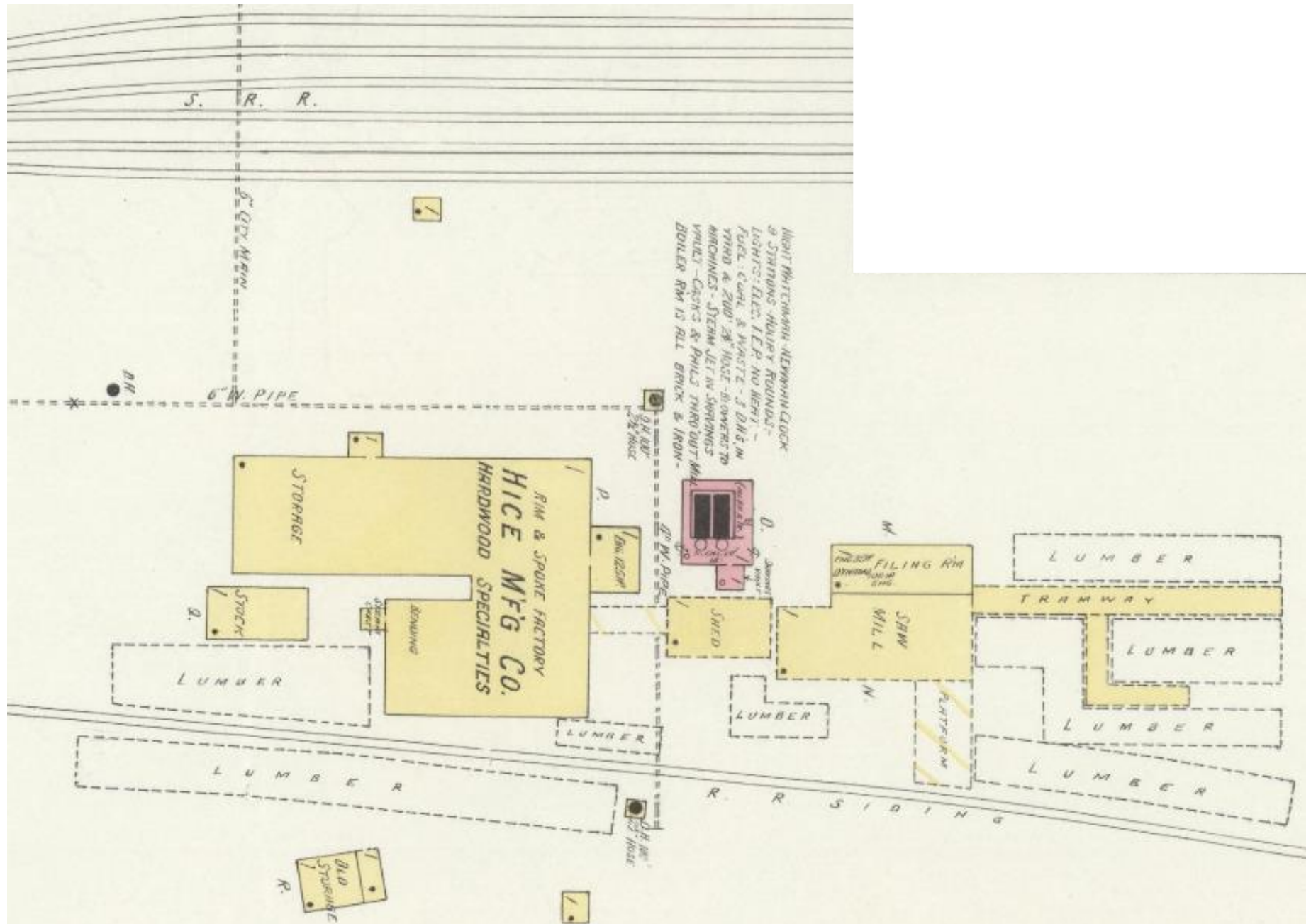
Map 15A. A detail of the building occupied by the Tennessee Carving & Wood Working Company (1912 to at least 1915), labeled “Used as Tenement”, in February 1920, see Map 2 and Figure 23D. The brick building on the right side of this map was the office of the Southern Bending Company. The spur along the bottom of this image belonged to the Clinchfield, while the trackage at the top of this image belonged to the Southern Railway.

11 April 1912. The Tradesman.

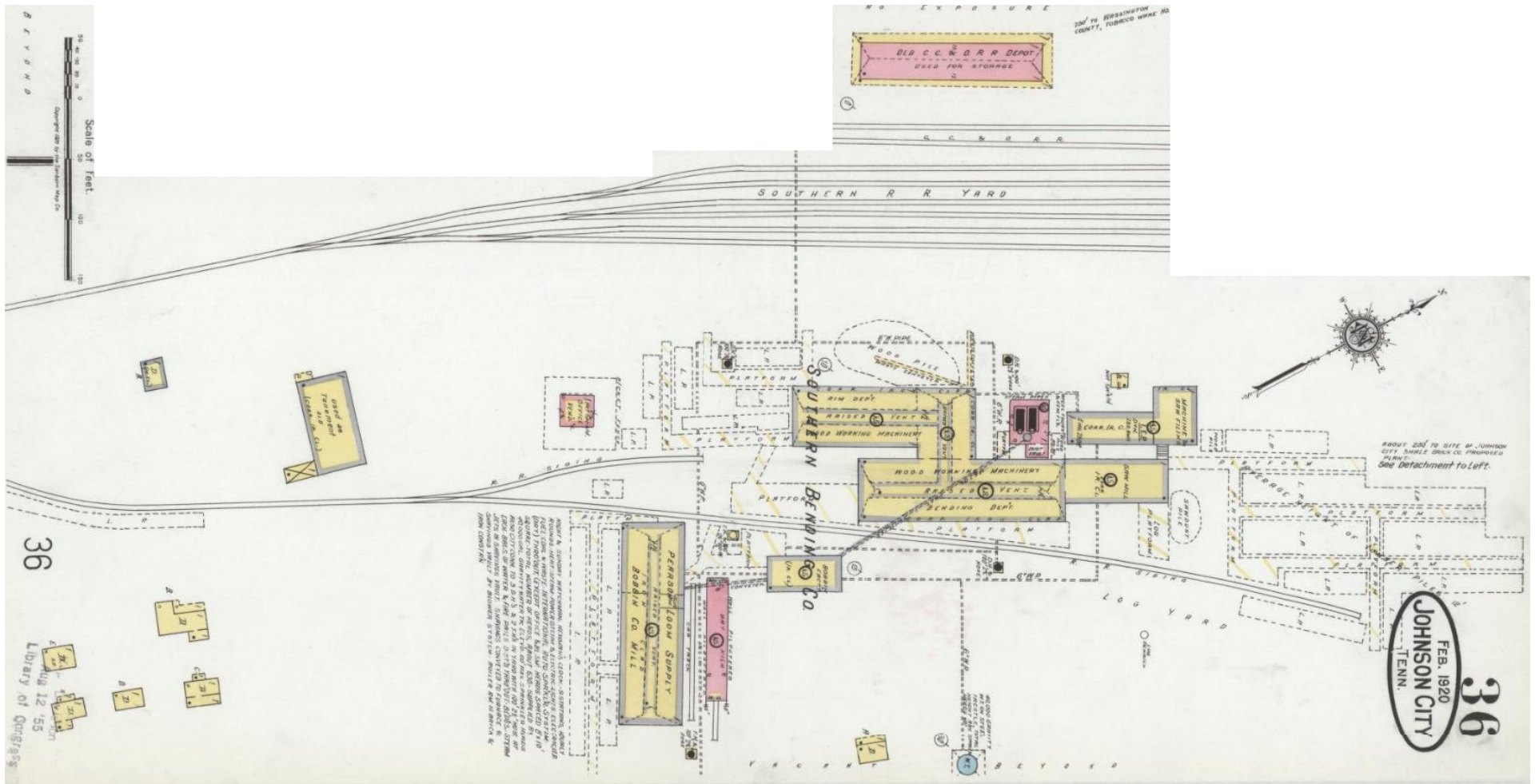
“Johnson City, Tenn. – The Tennessee Carving and Woodworking Co. C.E. Burchfield, president; Henry Rogerson, vice-president, and Julius Mennish, general manager.”¹²⁹

4.2. The Area Occupied by the Hice Manufacturing Company and Related Enterprises,
1911 to at least 1929

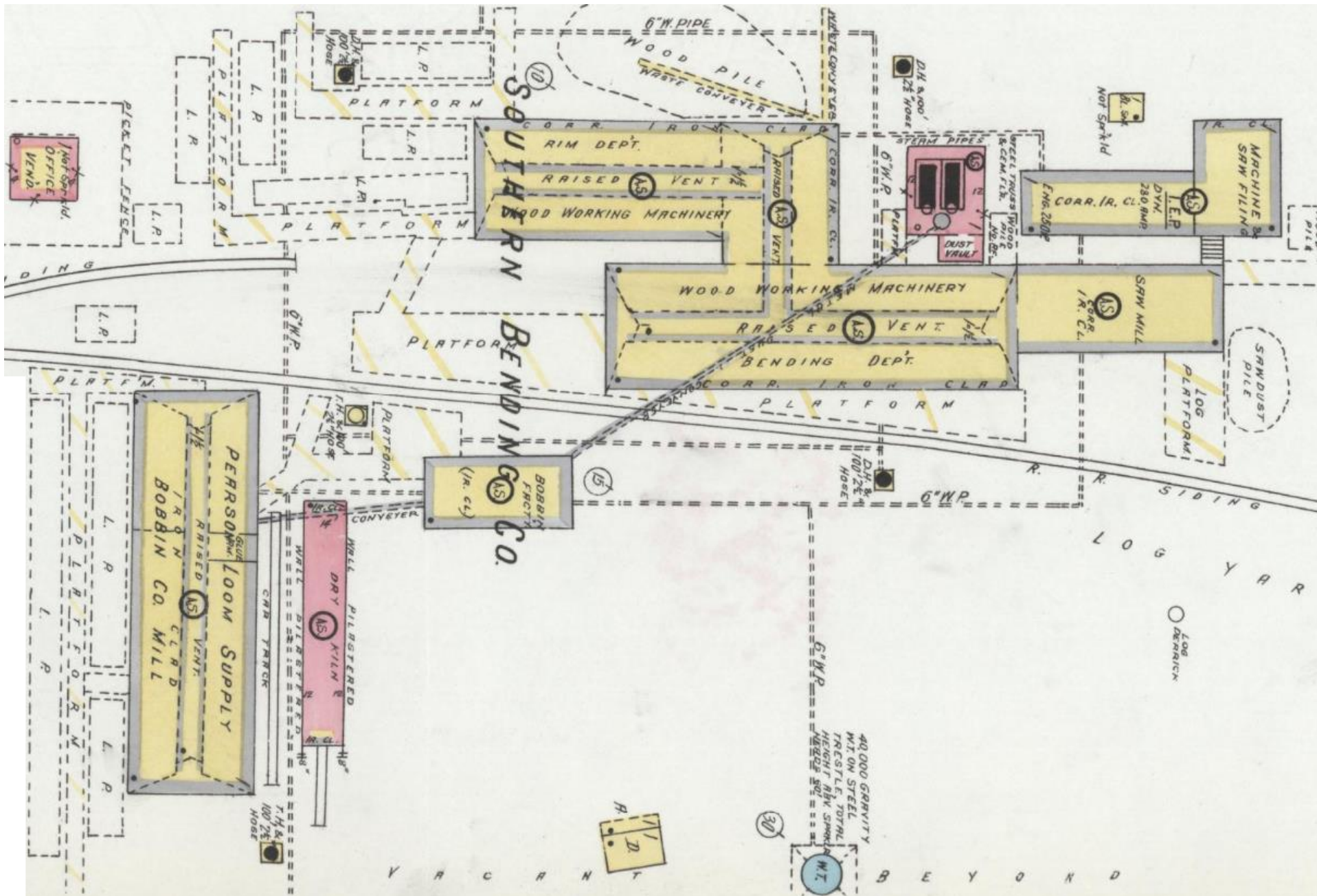
Hice Manufacturing Company, 1911 – 1916
Wood Products Corporation, 1916 – 1918
Southern Bending Company, 1918 – 1924 (1925?)
Unaka Oak Flooring Company, 1924 to at least 1929



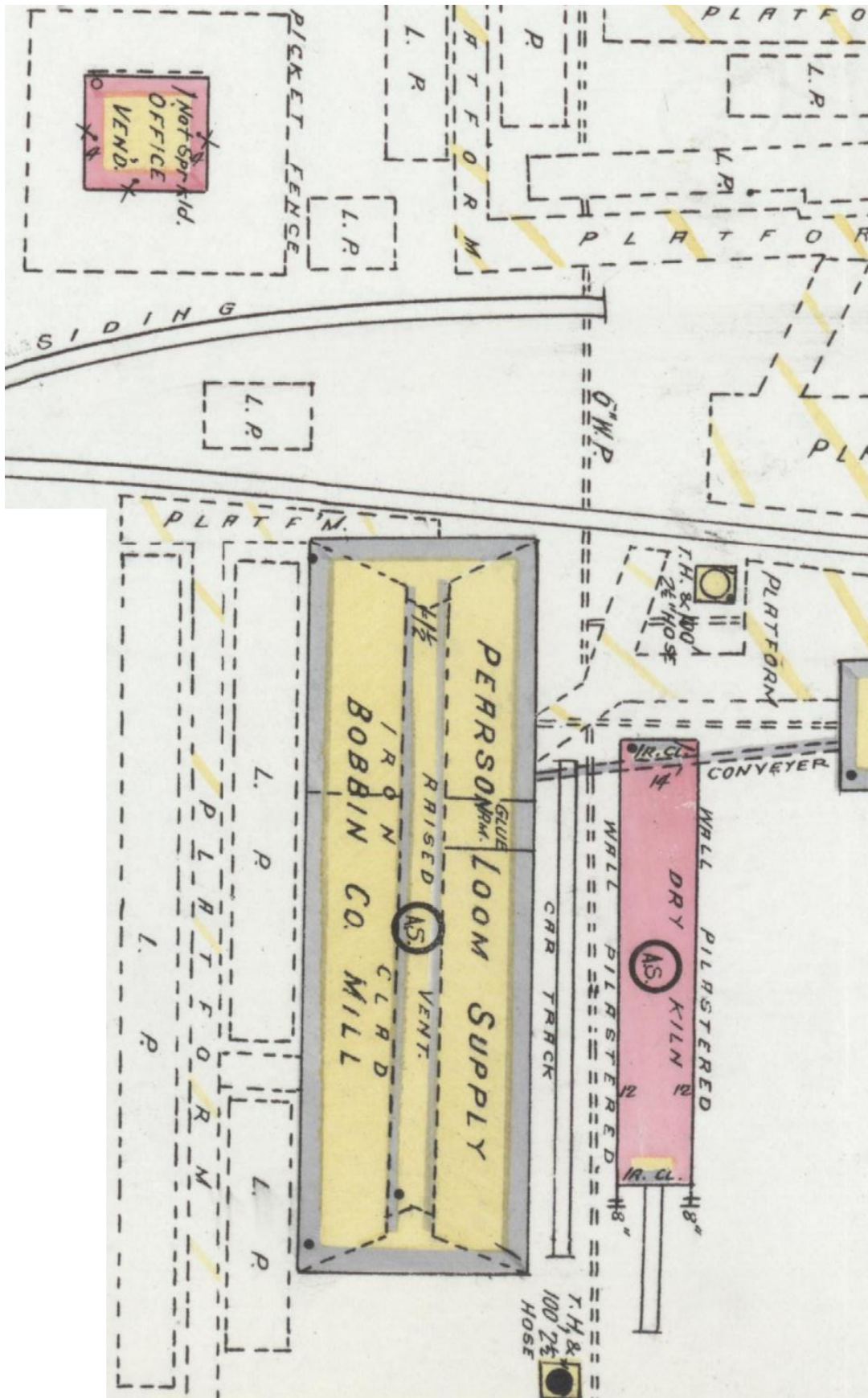
Map 14. Hice Manufacturing Company, July 1913.¹³⁰



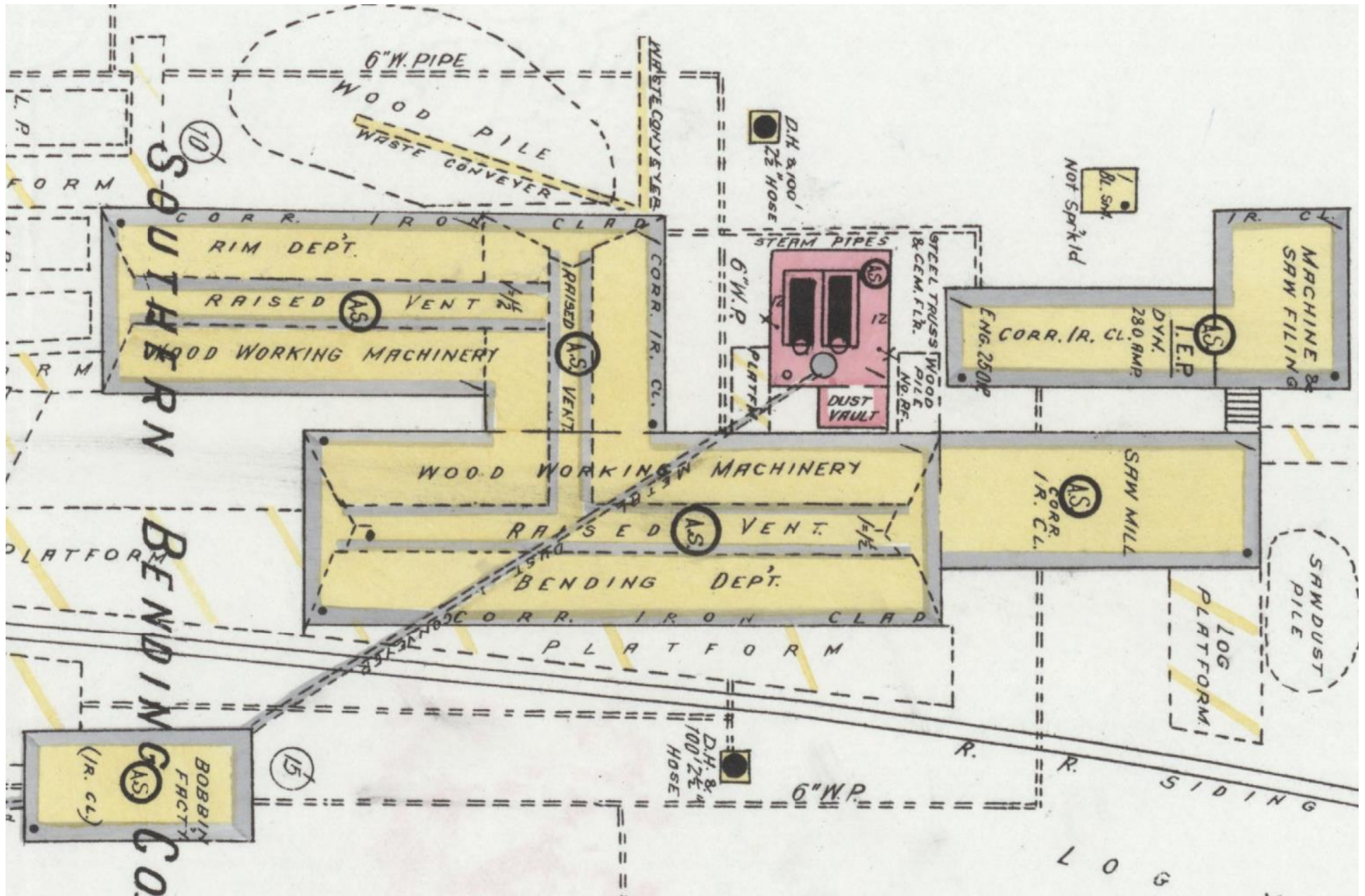
Map 15. The “Southern Bending Company”, February 1920. Note the “Old C.C. & O. R.R. Depot” “used for storage” at the top of this image. Also note that the relatively large iron clad building on the left, labeled “Used as Tenement”, is marked as the “Tennessee Carving & Wood Working Co.” on Map 2.”¹³¹



Map 15A. A detail of the Southern Bending Company, in February 1920.



Map 15B. A detail of the Southern Bending Company, depicting the Office (see Figure 23D), the “Pearson Loom Supply”/“Bobbin Co. Mill” and the “Dry Kiln”, in February 1920, see Figure 30.



Map 15C. A detail of the Southern Bending Company, depicting the “Rim Dep’t.”, “Bending Dep’t.”, boiler room, “Saw Mill”, and “Bobbin Fact’y”, in February 1920, see figures 27, 28, 29, and 30.

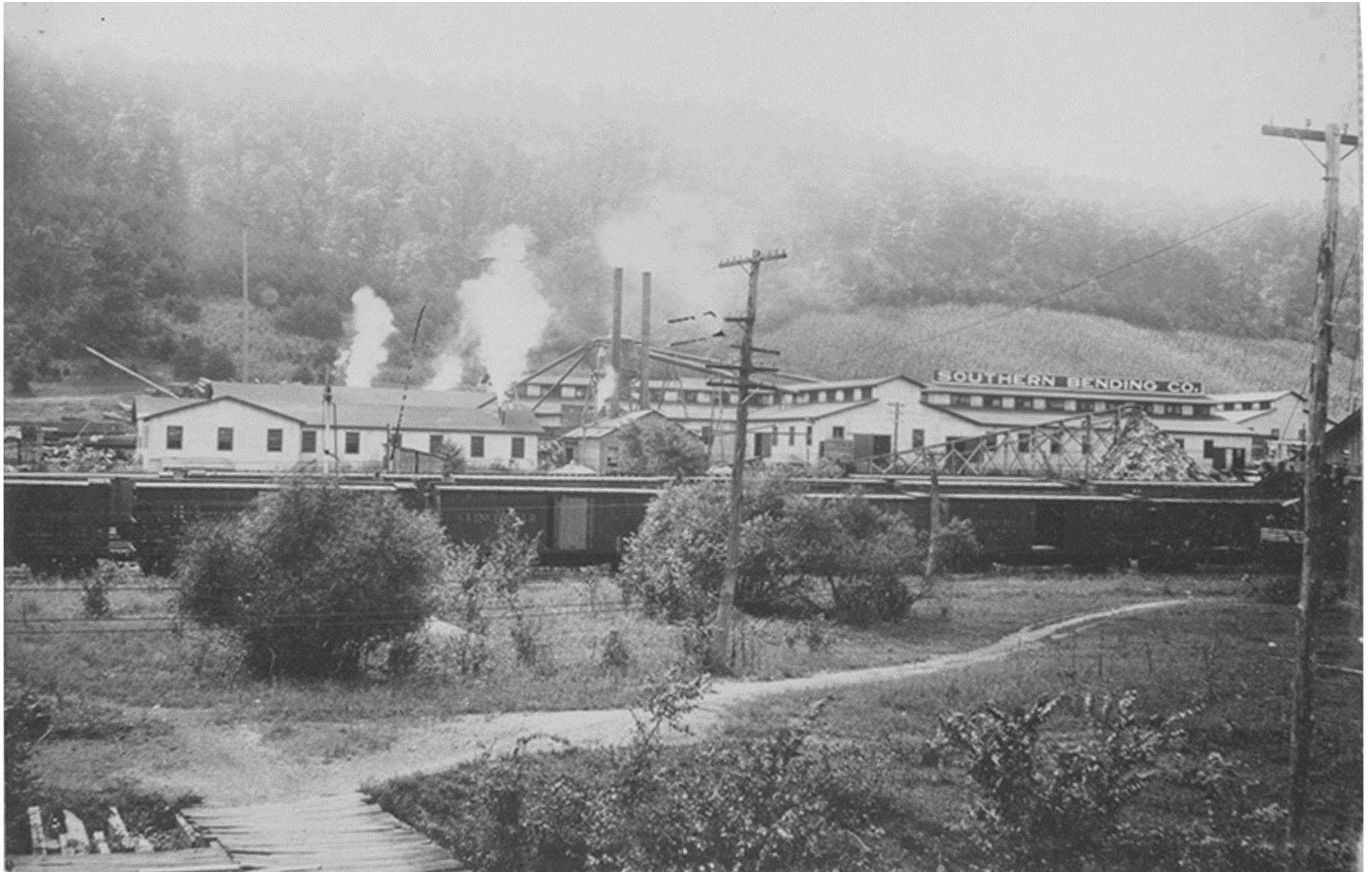


Figure 27. “Southern Bending Company; Southern and Carolina, Clinchfield and Ohio Railways; opposite Main Street, Carnegie,” “c. 1920” looking south across the Southern Railway yard. See Map 15A, note that this map image is “flipped” in comparison with this image. This photograph appears to date to between 1920 and 1924.¹³²



Figure 27A. The iron clad building on the left and in the center of this photograph contains the “Rim Dep’t.” and “Wood Working Machinery”. The iron clad building on the right contains the “Pearson Loom Supply” and “Bobbin Co. Mill.” Note that the structure partially visible on the right appears to be the east end of the old Clinchfield Freight Depot. Also note the “Waste Conveyer” in front of the “Rim Dep’t.” in the center of this photograph. See Map 15C. This photograph appears to date to between 1920 and 1924.



Figure 27B. A detail of the boiler house and the cyclone of the Southern Bending Company, see Map 15C.



Figure 27C. A detail of the Southern Bending Company, depicting the iron clad building that housed “Machine & Saw Filing”, as well as a “280 amp dynamo” on the left. The right side of this image includes the boiler house (with cyclone) and the “Bending Dep’t.”. Note Clinchfield boxcar, in the central foreground, and the four dust conveyors leading into the cyclone on the boiler house. Note the pole behind the building on the left, this pole was a “Log Derrick” and was used for moving logs around the yard. Also note the shack in front of this building, which was used for black smithing. The Sanborn map of February 1920 indicates only one dust conveyor leading to this cyclone, which indicates that this photograph was probably taken after the map from February 1920 was made. The Unaka Flooring Company had acquired this facility by 1924, consequently, this photograph appears to date to between 1920 and 1924.



Figure 28. Looking south, across the Clinchfield in Carnegie at the “Southern Bending Company”. Note the Norfolk & Western and Southern Railway boxcars in the foreground.¹³³



Figure 28A. A detail of the Southern Bending Company, which depicts the boiler house, the “Rim Dep’t.” and the “Waste Conveyor”, from left to right. Note that only two dust conveyors enter the cyclone, indicating that this photograph was taken after the Sanborn map of February 1920 was produced, but before the images in figures 27 and 30 were taken, consequently, this photograph appears to date to between 1920 and 1924. Also note the early model car on the right.



Figure 28B. A detail of the boiler house. The shack in the foreground appears to have been used for black smithing. The building in the right background contained the "Rim Dep't."



Figure 28C. A detail of the Southern Bending Company, depicting the iron clad building that housed “Machine & Saw Filing”, as well as a “280 amp dynamo” on the left. Note the platform to the left of this building, with the “Log Derrick” behind it. Also note the water tower “40,000 [gallon] gravity W.T. on steel trestle, total height abv. Sprklr heads 60” and the building to the left of it, which was not depicted on Map 15A. The tall building in the right background contained the “Bending Dep’t.”. The boiler house and black smith shack are also visible on the right. This photograph appears to date to between 1920 and 1924.



Figure 29. The Southern Bending Company, note the old Clinchfield Freight Depot in the right background.¹³⁴ This photograph appears to date to between 1920 and 1924.



Figure 29A. A detail of the Southern Bending Company. This one depicts the lumber yard and platforms on the right, “Average height of lumber & Piles 12”, note the gondola in the lumber yard. On the left is the “Saw Mill” and to its right can be seen the iron clad building that housed “Machine & Saw Filing”, as well as a “280 amp dynamo”. Also note the “Old C.C. & O. R.R. Depot” in the background, as well as what appears to be a Santa Fe boxcar. This photograph appears to date to between 1920 and 1924.



Figure 53A. A detail of what appears to be the lumber yard and platforms on the east side of the Unaka Flooring Company (previously the Southern Bending Company). Note the early model automobile, as well as the N&W, Southern and Clinchfield freight cars in the background. This photograph appears to date to the time between 1925 and 1928.

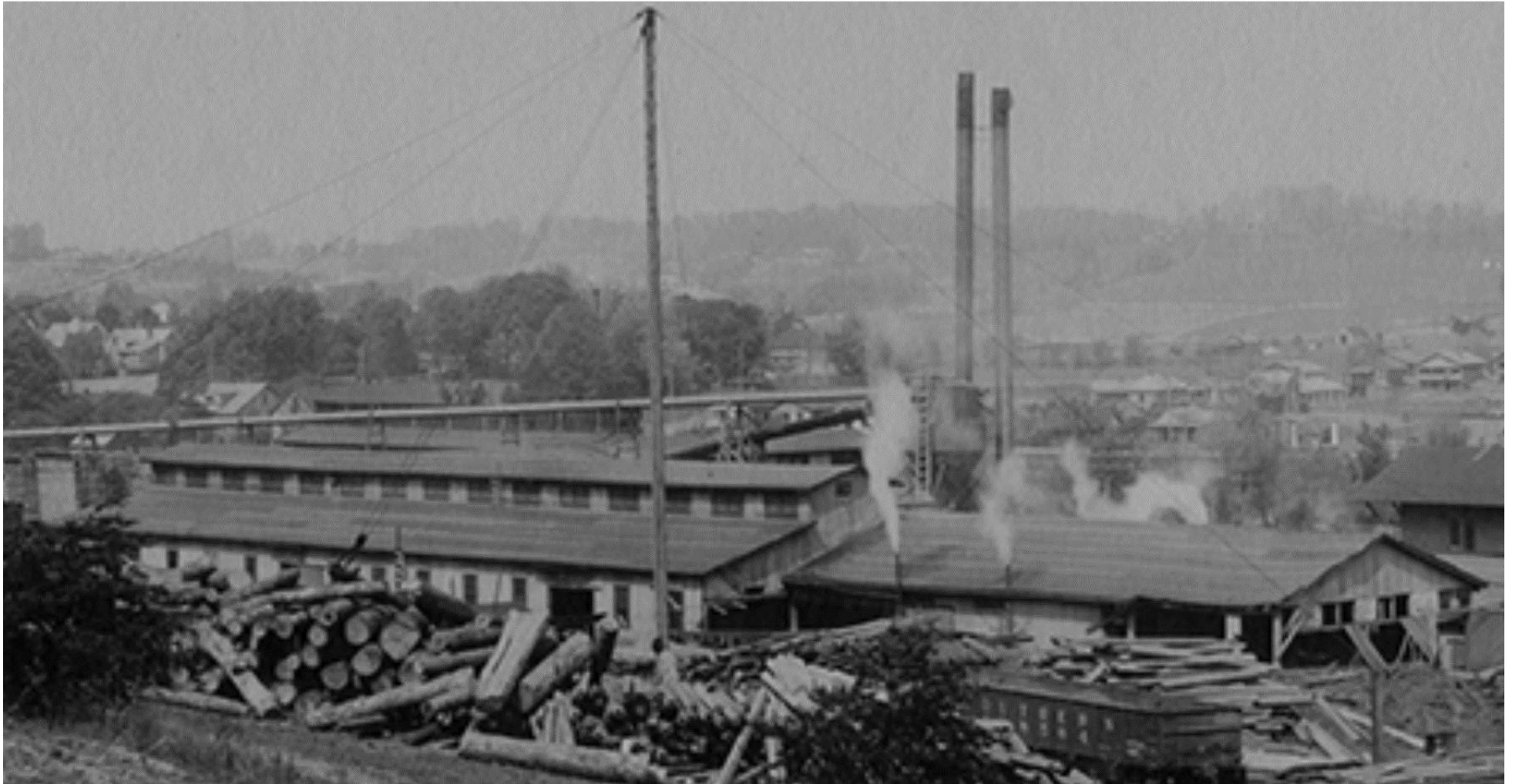


Figure 29B. A detail of the Southern Bending Company, depicting (from left to right) the “Bending Dep’t.” the “Log Derrick”, and the “Saw Mill”. All of these buildings were “Corr. Iron Clad”. Note the Southern Railway gondola in the foreground and the “Old C.C. & O. R.R. Depot” in the right background. Also note the boiler house and that only two dust conveyors enter the cyclone on top of it, indicating that this photograph was taken after the Sanborn map of February 1920 was produced, but before the images in figures 27 and 30 were taken, consequently, this photograph appears to date to between 1920 and 1924.



Figure 29C. Another detail of the Southern Bending Company. This one depicts the “Saw Mill”.



Figure 29D. Another detail of the Southern Bending Company. This one depicts the “Bending Dep’t.” Note that the “Rim Dep’t.” is visible behind it. This photograph appears to date to between 1920 and 1924.



Figure 29E. A detail of the Southern Bending Company. The building on the left appears to be the one occupied by the “Pearson Loom Supply” and “Bobbin Co. Mill”. Note the interesting structure associated with what appear to be the “Metal Dust Conveyors” on right of this image. This photograph appears to date to between 1920 and 1924.

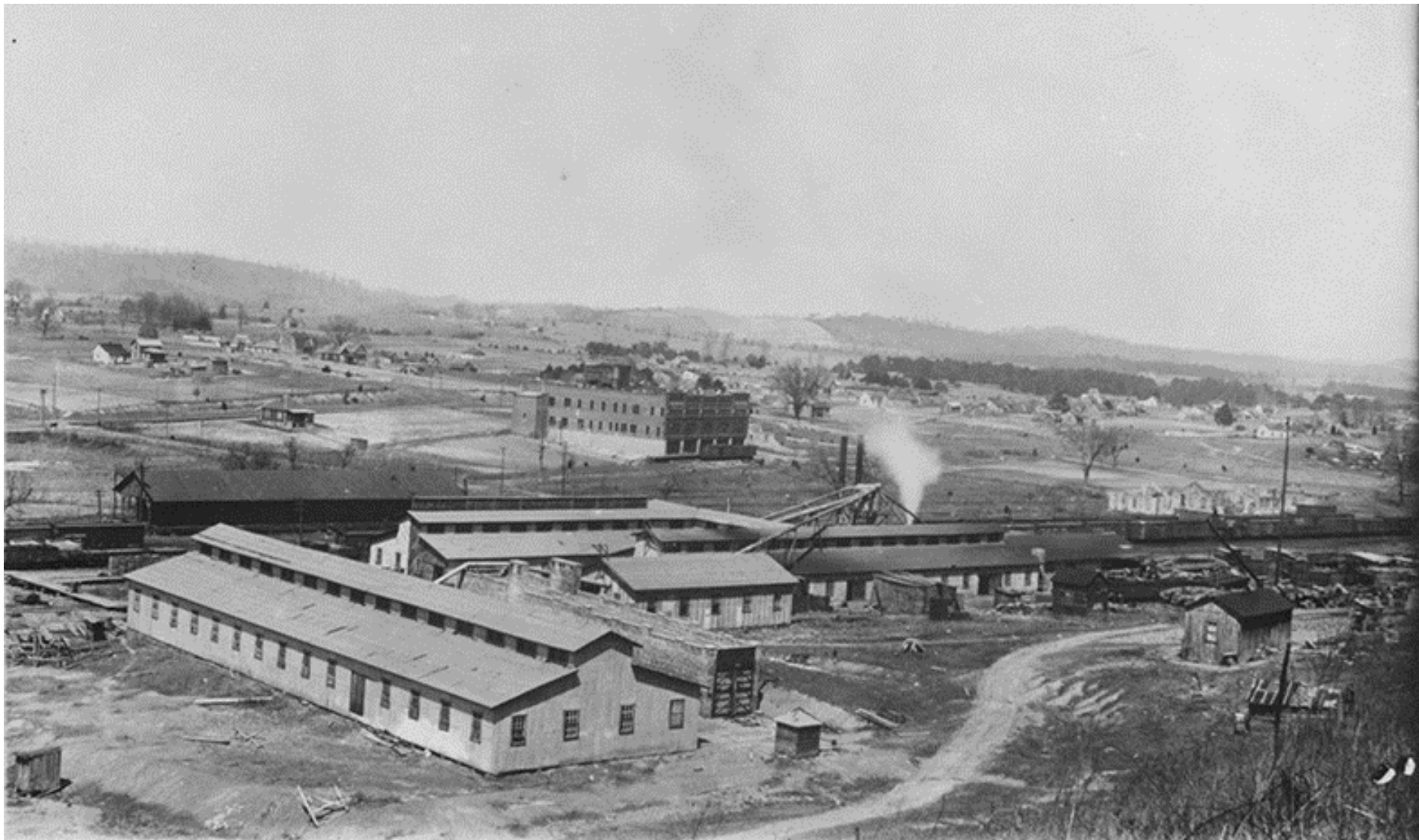


Figure 30. The Southern Bending Company (or perhaps the Unaka Oak Flooring Company, which occupied this facility from 1924 to at least 1929), with four “Dust Conveyors” leading to the cyclone on the boiler house. Note the old Clinchfield Freight Depot in the left background, the Wilder block (now occupied by the Empire Chair Company, with a railroad spur holding a couple of boxcars) in the middle background, and the unfinished buildings of the 3C’s/O.R. & C./S. & W. railroads on the right. Also note that the McCorkle Lumber Company facility (see Figure 53), which was built in 1925 on the site occupied by the Carnegie Hotel (to the right of the Wilder block) from 1891 to 1910, is not present.¹³⁵ Consequently, this photograph appears to have been taken after those in figures 28 and 29, and to date to between 1920 and 1925.



Figure 30A. A detail. In 1920, the “iron clad” building in the left foreground contained the “Bobbin Co. Mill” and “Pearson Loom Supply”. The structure to its right was built of brick and contained the “Dry Kiln”. The small building in the lower right corner appears to have contained 100’ of 2½” fire hose, see Map 15B. The iron clad building with the clerestory roof, closest to the “Old C.C. & O. R.R. Depot”, contained the “Rim Dep’t.” and was attached to the iron clad building that contained the “Bending Dep’t.” The smaller detached building, between the “Dry Kiln” and the buildings with the clerestory roofs, contained the iron clad “Bobbin Fact’y.”, see Map 15C. This photograph appears to date to between 1920 and 1925.



Figure 30B. A detail. The iron clad building, with the clerestory roof on the left, contained the “Rim Dep’t” and “Wood Working Machinery”. The relatively small, detached building was also iron clad and contained the “Bobbin Fact’y” and was connected to the cyclone with a “metal dust conveyor.” The building with the clerestory roof just behind and to the right of the small detached building contained the “Bending Dep’t” and “Wood Working Machinery” under the portion with the “Raised Vent”, with the “Saw Mill” in the portion, to the right, without the “Raised Vent”. Note that the brick boiler house is behind the “Bending Dep’t.” This photograph appears to date to between 1920 and 1925.



Figure 30C. A detail showing the “Log Derrick”, with the “Saw Mill” on the left and the unfinished buildings of the 3C’s/O.R. & C./S. & W. railroads in the background. The small building in the foreground appears to be the dwelling depicted on Map 15A, near the water tower (which would have been out of the frame of this photograph, to the right). This photograph appears to date to between 1920 and 1925.



Figure 23D. A detail that includes the Wilder block and the “Old C.C. & O. R.R. Depot” on the right. Note the square building, with the pyramid shaped roof, in front of the “Old C.C. & O. R.R. Depot”, which appears to be the office building of the Southern Bending Company/Unaka Oak Flooring Company. Also note the freight cars on the left. This photograph appears to date to 1926 or later, which would indicate that the Unaka Oak Flooring Company had acquired the facility previously occupied by the Hice Manufacturing Company (1911 – 1916), the Wood Products Corporation (1916 – 1918), and the Southern Bending Company (1918 – 1924 or 1925).

Hice Manufacturing Company, 1911 – 1916
Wood Products Corporation, 1916 – 1918
Southern Bending Company, 1918 – 1924 (1925?)
Unaka Oak Flooring Company, 1924 to at least 1929

4.2.1. Hice Manufacturing Company, 1911 – 1916

26 January 1911. Johnson City Comet.

“Textile Novelty Plant Soon to be Established near Carnegie Crossing – New Company organized with \$25,000 Capital – Headed by a Hickory, N.C. Capitalist – Hickory, N.C., Jan. 19. – J.H. Hice has sold his interest in the firm of Ivey & Hice company, incorporated, to L. Flagler. This company was organized two years ago for the manufacture of textile novelties... Mr. Hice will erect a similar plant at Johnson City, Tenn., having formed a company of Johnson City capitalists for that purpose. This new company will be capitalized at \$25,000 and will manufacture picker sticks, rug straps, loom supplies and wheel and table rims. –

This is one of the new enterprises Johnson City will secure this year. A site has been secured almost opposite the crossing of the Southern and C., C. & O. railways in the Carnegie yards, on the south side of the Southern, and it is expected that the work of erecting the plant will begin within two weeks. The plant will employ 20 men to start with and another feature will be soon added that will require an additional 20 men, making 40 skilled mechanics, who will be given regular employment by the new enterprise.”

WANTED
Hickory and White Oak
LOGS
We also buy hicky and oak lumber
Highest Market Price
paid for quartered oak. Write or
Apply to
Hice Mfg Co.
Johnson City, - Tennessee

Figure 31. Johnson City Comet, 16 March 1911.

9 February 1911. Johnson City Comet.

“All the Money has been Subscribed – The Hice Manufacturing Company is now at work in Johnson City constructing the place. The full amount of money necessary to locate the industry in Johnson City has been subscribed and more than half of the amount has been paid to the secretary of the Commercial Club.”

26 October 1911. Johnson City Comet.

“Manufacturing Plant will be Enlarged. – The Hice Manufacturing Company, one of the most prospering industries of this city, are now making preparations for the addition of a new building 50 x 100 feet a modern up-to-date boiler house of brick and steel to be absolutely fireproof, as well as install one of the latest improved shavings exhaust system. They hope to have this work completed within the next thirty days. It will be remembered that this firm has been located here less than a year, but with the efficient management of their Mr. Hice, the business has grown until it is impossible to take care of same with their present facilities. They also contemplate the installment of a modern band sawmill in the near future.”

26 October 1911. Johnson City Comet.

“Stove Wood for Sale. Good oak and hickory stove wood, properly cut and promptly delivered. Call 215 new ‘phone. Hice M’F’G’ Co.”

2 May 1912. Johnson City Comet.

“Wilbur Looper Accidently Killed – Thursday afternoon Wilbur Looper met a terrible fate at the Hice Mfg. Co., in Carnegie. While attempting to put a belt on a revolving pulley, a pile of rims he was standing on slipped and he fell onto the shaft, his clothing catching on a set screw and in less than a minute he was whirled round and round the shaft until every stitch of clothing was torn from his body and his nude form from which the left arm had been torn out at the shoulder, fell to the floor and the arm was thrown twenty feet away. The boy never lost consciousness and was hurried to the Memorial Hospital...”

9 May 1912. Manufacturers Record.

“Woodworking Machinery. – Hice Manufacturing Co., Johnson City, Tenn., wants L spoke tenoning machine, facing, throating and sand-belt machines; equalizing saw; all second-hand.”¹³⁶

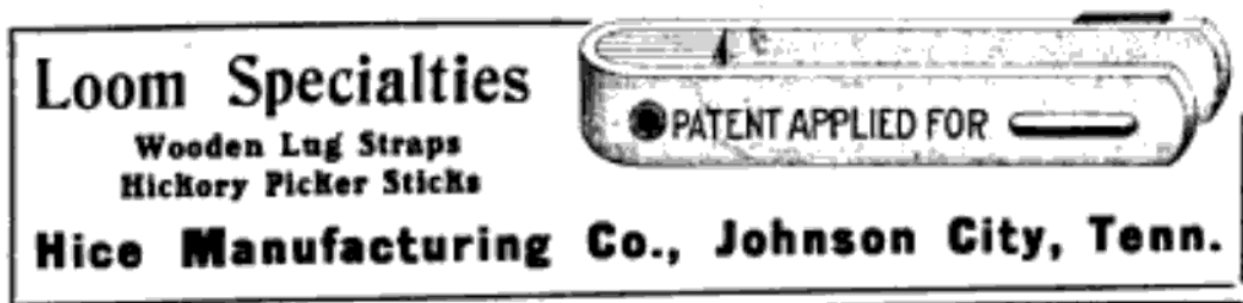


Figure 32. Fibre and Fabric, 6 July 1912.¹³⁷

30 May 1912. American Machinist.

“Woodworking machinery will be installed in the spoke factory of the Hice Mfg. Co., Johnson City, Tenn. Sanders equalizers, etc., will be needed.”¹³⁸

24 December 1914. Johnson City Comet.

“Johnson City Lumber Plant to be Rebuilt. – At a meeting of the Hice Manufacturing company, it was decided to rebuild their plant here which was destroyed by fire Dec. 11.

The work of clearing away the debris has begun.

One large building which was being constructed by the company was saved. This will be completed in sixty days, and then the plant will resume operations.”

10 January 1915. Hardwood Record.

“The Hice Manufacturing Company has about completed its plant at Johnson City, Tenn., and is purchasing logs with a view of resuming operation at once. The company’s plant was destroyed by fire some weeks ago.”¹³⁹

15 January 1915. Wooden and Willow-ware Trade Review.

“Hice MFG. Co.’s Wood Working Plant Burned. The Hice Manufacturing Company’s Wood Working plant in Johnson City, Tenn., was seriously damaged by fire recently, the main building with its contents being destroyed, a loss of about \$35,000, of which only \$12,000 was covered by insurance, resulting, states the Johnson City Comet. The office building, boiler room and a new addition under construction were saved [?] from the flames. Started in 1911 by John [H. Hice?] of Hickory, N.C., the business has grown [rapidly?] until at present it employs seventy-five [men?].”¹⁴⁰

23 January 1915. American Lumberman.

“The Hice Manufacturing Company has nearly completed its new plant at Johnson City and will shortly have it ready for operation.”¹⁴¹

January 1915. Mill Supplies.

“Fire losses were suffered by the following plants in December... Hice Manufacturing Co., Johnson City, Tenn., \$40,000...”¹⁴²

January 1915. The Hub.

“The Hice manufacturing plant at Johnson City, Tenn., was completely destroyed by fire, December 11. The loss is estimated at \$35,000 to \$40,000, with \$13,000 insurance. The plant manufactured wagon and carriage materials, bent wheel rims, table rims and specialties for textile mills, etc., and employed 75 men. The plant was erected in 1911 and had been very successful. A new building, the office building and about \$5,000 worth of unfinished and raw material were saved.”¹⁴³

20 February 1915. American Lumberman.

“The Hice Manufacturing Company last week completed its new mill at Johnson City, Tenn., and put it into operation. The company’s mill there was destroyed by fire some months ago. The new mill is much larger and starts off well stocked with logs.”¹⁴⁴

5 March 1915. The Mississippi Valley Lumberman.

“Johnson City, Tenn. – The Hice Manufacturing plant, which was destroyed by fire the latter part of last year, has been rebuilt and is now in operation. New capital has been added. A \$6,000 sprinkler system has been installed...”¹⁴⁵

10 July 1915. Hardwood Record.

“The Hice Manufacturing Company of Johnson City, Tenn., which completed a new mill, last week received a large order from the English government for walnut specification stock, to be used in the manufacture of stocks for rifles for the allied armies in the European war. The stock is to be shipped in the rough to the munition factories of England. The order will keep the plant busy for some time.”¹⁴⁶

16 September 1915. Johnson City Comet.

“Manufacturing Plant Doubles Capital. The Hice Manufacturing plant, one of the largest plants in the city, employing almost a hundred men, has outgrown its old capital stock, and at a recent meeting the stock was increased from \$30,000 to \$60,000. This company is operating full time and has recently had to turn down some orders.”

25 September 1915. Hardwood Record.

“The Hice Manufacturing Company has started its new mill at Johnson City, Tenn.”¹⁴⁷

October 1915. Mill Supplies.

“The Hice Manufacturing Co., Johnson City, Tenn., manufacturer of walnut timber for use in making gunstocks, has increased its capital stock from \$30,000 to \$60,000, and has purchased additional equipment.”¹⁴⁸

October 1915. The Spokesman.

“Johnson City, Tenn., Hice Manufacturing Co. will make lately noted increase to \$60,000 capital stock in order to further present business, and with view to increased capacity soon; no new machinery wanted at present, but will probably install some additional equipment within 12 months; new officers not yet elected.”¹⁴⁹

4.2.2. Wood Products Corporation, 1916 – 1918

10 May 1916. Lumber World Review.

“New Concerns... Johnson City, Tenn. – Wood Products Corporation: \$60,000 capital. Lee F. Miller, president; Allen Harris, secretary and manager... The Wood Products Corporation has taken over the plant of the Hice Manufacturing Co. and has begun operations.”¹⁵⁰

1 June 1916. The St. Louis Lumberman.

“The plant of the Hice Manufacturing Co., at Johnson City, Tenn., has been acquired by the Wood Products Corporation, who will continue its operation. The new company is officered as follows: President, Lee F. Miller; secretary-manager, Allen Harris. These two, together with A.B. Crouch, J.F. Jett, J.S. Sommers and G.T. Woodford constitute the board of directors.”¹⁵¹

4.2.3. Southern Bending Company, 1918 – 1924 (1925?)

17 August 1918. Automobile Topics.

“Incorporations... The Southern Bending Co., Johnson City, Tenn. to manufacture bent rims and dimension stock. Capital \$30,000. Incorporators: W.O. Nelson, R.M. Watkins, O.D. Hutchens.”¹⁵²

24 October 1918. American Machinist.

“Tenn., Johnson City – The Southern Bending Co., Box 37, is in the market for glue joiners to joint one side and one edge of timber 1 x 2 x 40 in. at one operation or a machine to joint the edge and another for the side. The company does not want hand jointers.”¹⁵³

November 1918. Packages.

“Johnson City, Tenn. – Southern Bending Co., Box 37, organized; W.O. Nelson, president – manager; R.M. Watkins, vice-president; O.S. Hutchens, secretary; acquired plant of Wood Products Corp.; manufacture vehicle rims, solid table rims loom supplies.”¹⁵⁴

QUALITY

SERVICE



This Trade Mark has been adopted by a concern whose products stand out pre-eminently in their particular line. The Southern Bending Company is furnishing the textile trade with hickory loom supplies absolutely unequalled. Trained men, perfect material, and modern manufacturing facilities are allowing them to do this. Some of the largest mills in North America have taken advantage of the service offered by this Company, and learned of the true economy to be had from the use of these *guaranteed loom parts.*

Southern Bending Co.
Johnson City, Tenn.

Figure 33. Textile World Journal, 4 October 1919.¹⁵⁵

Loom Supplies

Binders
Lug Straps
Lug Sticks
Pick Levers
Pitman Arms
Picker Sticks
Parallel Blocks
Cloth Roller Blocks, Etc.



RUN DOWN THIS LIST and ask yourself if your loom supplies are giving satisfaction. If you think you are not getting the service you should from your hickory loom parts, put it up to us—let us send you some supplies that are *guaranteed*. Our goods are manufactured to a rigid standard, and the prices are consistent. We try to, and do, make loom parts that are uniform in their performance. Send for working samples.

SOUTHERN BENDING CO.
Johnson City, Tenn.

Figure 34. Textile World Journal, 15 – 22 November 1919.¹⁵⁶

Hickory Lug Straps

Hickory is as suitable for lug straps as canvas, if not a great deal more so.

The results from an actual test will prove this.

The leather bunter and rivet inside the strap hold it in perfect shape. And the slots near the end allow for proper adjustment to the sweep stick. Washers are used to clamp these on.

Our hickory straps are outlasting two of the canvas ones right along.

Samples gratis on request.

Southern Bending Co.

Johnson City, Tenn.

Figure 35. Textile World Journal, 3 July 1920.¹⁵⁷

4.2.4. Unaka Oak Flooring Company, 1924 to at least 1929

10 May 1924. American Lumberman.

“Johnson City – Unaka Flooring Co. adding \$75,000 worth of new machinery.”¹⁵⁸

25 July 1924. Lumber.

“Johnson City – Southern Bending Co. succeeded by Unoka [Unaka] Oak Flooring Co.”¹⁵⁹

27 August 1925. Manufacturers Record.

“Tenn., Johnson City – Southern Bending Co., Sam R. Sells, Pres., will rebuild burned portion of plant... Fire Damage... Saw mill and portion of warehouse of the Southern Bending Co.; loss \$50,000.”¹⁶⁰

15 December 1929. “Southern Wood-Using Industries and List of Dimension Stock Producers.”

“List of Southern Wood-Using Industries... Unaka Oak Flooring Co., Johnson City...”¹⁶¹

Table 34. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year 1913.¹⁶²

Firm Name	Nature of Business	No. Employees at Time of Inspection		Hours Labor Week		Gen'l. Cond.	No. Insp.
		Male	Female	Male	Female		
Hice Mfg. Co.	Mfrs. Wagon Material	45		60		Good	1

Table 35. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year Ending 1 December 1914.¹⁶³

Firm Name	Character of Business	No. of Employes at time of inspec.				No. Hrs. Lab. Males Over 16		No. Hrs. Lab. Females & Min. Under 16		Gen'l. Con.
		Over 16		Under 16		Day	Week	Day	Week	
		M.	F.	M.	F.					
Hice Mfg. Co.	Mfrs. Wagon Material	45				10	50			Good

Table 36. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year Ending 1 December 1915.¹⁶⁴

Firm Name.	Character of Business.	No. Employes at time of inspection.				No. Hours Labor Males Over 16		No. Hours Female & Min. Under 16		General Condition.
		Over 16.		Under 16.		Da.	Wk.	Da.	Wk.	
		M.	F.	M.	F.					
Hice Mfg. Co.	Wagon Material	61		1		10¼	60	10¼	58	Good

Table 37. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year Ending 1 December 1916.¹⁶⁵

Firm Name	Character of Business	No. Employes at time of inspection				No. Hours Labor Males Over 16		No. Hours Female and Min. Under 16		General Condition
		Over 16		Under 16		Da.	Wk.	Da.	Wk.	
		M.	F.	M.	F.					
Hice Mfg. Co.	Wagon Material Mfrs.	64		2		10¼	60	10¼	57	Good
Wood Products Corp.	Spokes & Loom Supplies, Mfrs.	62				10	60			Good

Table 38. “Directory of Industries Inspected, with No. Employes, December 1, 1916, to December 31st, 1917... Washington County – Johnson City”.¹⁶⁶

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Wood Products Co.	Spokes & Loom Supplies	21			

Table 39. “Directory of Industries Inspected, with No. Employes... Washington Co. – Johnson City”, for the Year Ending 31 December 1918.¹⁶⁷

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Southern Bending Co.	Wagon Rim Mfrs.	57	2	1	

Table 40. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1919.¹⁶⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Southern Bending Co.	Mfrs. Wagon Rims	49	1		

Table 41. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1921.¹⁶⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Southern Bending Co.	Wood Working	49			

Table 42. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.¹⁷⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Southern Bending Co.	Flooring	24	1		

Table 43. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1928.¹⁷¹

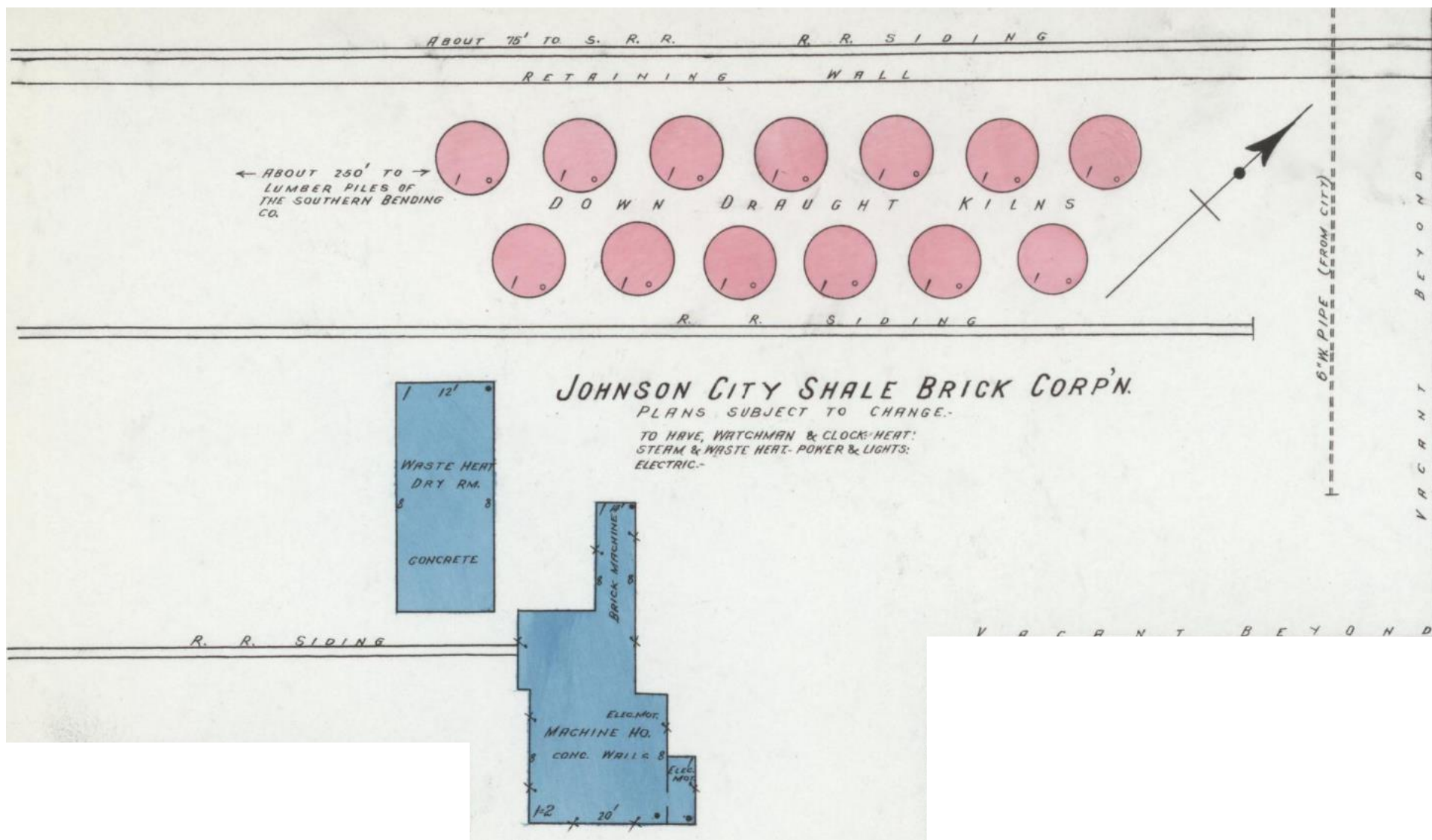
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Unaka Oak Flooring Co.	Lumber	87	19		

4.3. The Area Occupied by the Johnson City Shale Brick Corporation and Related Enterprises,
1920 – Present

Johnson City Shale Brick Corporation, 1920 – 1928
General Shale Products Corporation, 1928 – Present

This enterprise appears to have been served by the Southern Railway from about 1920.

Although the General Shale Products Corporation is still in business, it does not appear that any of the buildings of this facility survived to the present time.



Map 16. "Johnson City Shale Brick Corp'n... Plans subject to change", February 1920.¹⁷² "About 250' to lumber piles of the Southern Bending Co."



Figure 36. “Cranberry Furnace’s smokestack and ore elevator can be seen in the far distance in this... view of General Shale Brick Company, which was built [by the Johnson City Shale Brick Corporation] in the early 1920s. Note from Chris [Ford?]: A photo this rich in detail deserves a detailed explanation. The street running from left to right across the center of the photo and crossing Brush Creek is Broadway. The Wilder Block would be just off the left side of the photo. The RR tracks are the north end of a small Southern yard, the northbound Southern mainline and the CC&O spur to the furnace. A wealth of information for modelers here. What did billboards advertise in 20s? The left one is for ‘Duckhead Overalls’ featuring two black farmers saying ‘I see you wear’em too.’ Also ‘Sold Everywhere.’ The other billboard is for ‘Lucky Strike’ cigarettes, showing three ‘dandies’ in top hats and tuxedos, quite a contrast. A dual gauge spur holding 2 standard gauge boxcars leads off to the right to connect the ET&WNC furnace spur, which crosses the Southern mainline about mid-photo. There are plenty of Clinchfield hoppers in the yard, and one gon loaded with what looks like used culverts. The 2 dump cars at the brick plant are being pulled by a mule led by a black worker. Only a few people are visible. But in the yards and fields are lots of horses and cows. Also note the 2 outhouses at the brick yard, one elevated, one not.”¹⁷³ It appears that this photograph predates the fire that struck this plant in 1926, see Figure 37.



Figure 36A. A detail of the Johnson City Shale Brick Company. The inclined track leads into the “Machine Ho.,” while the forward most building on the left appears to be the “Waste Heat Dry Rm.” Both of these buildings were supposed to be built of concrete, see Map 16. Note that this complex is significantly more developed than that depicted on Map 16. Also note the two boxcars on the track in the right background, which appear to be those of the Southern mainline to Bristol. Also note the tracks in the middle background, which appear to be the Clinchfield spur to the Cranberry Furnace (visible on the horizon). The lack of vegetation around this plant indicates, perhaps, that this photograph was taken shortly after the plant was placed in operation.



Figure 36B. A detail. This image appears to include 13 “Down Draft Kilns”, which matches the number shown on Map 16.



Figure 37. "General Shale, Johnson City Plant, 1928," note that the Johnson City Shale Brick Company expanded in 1928 and became the General Shale Products Corporation.¹⁷⁴

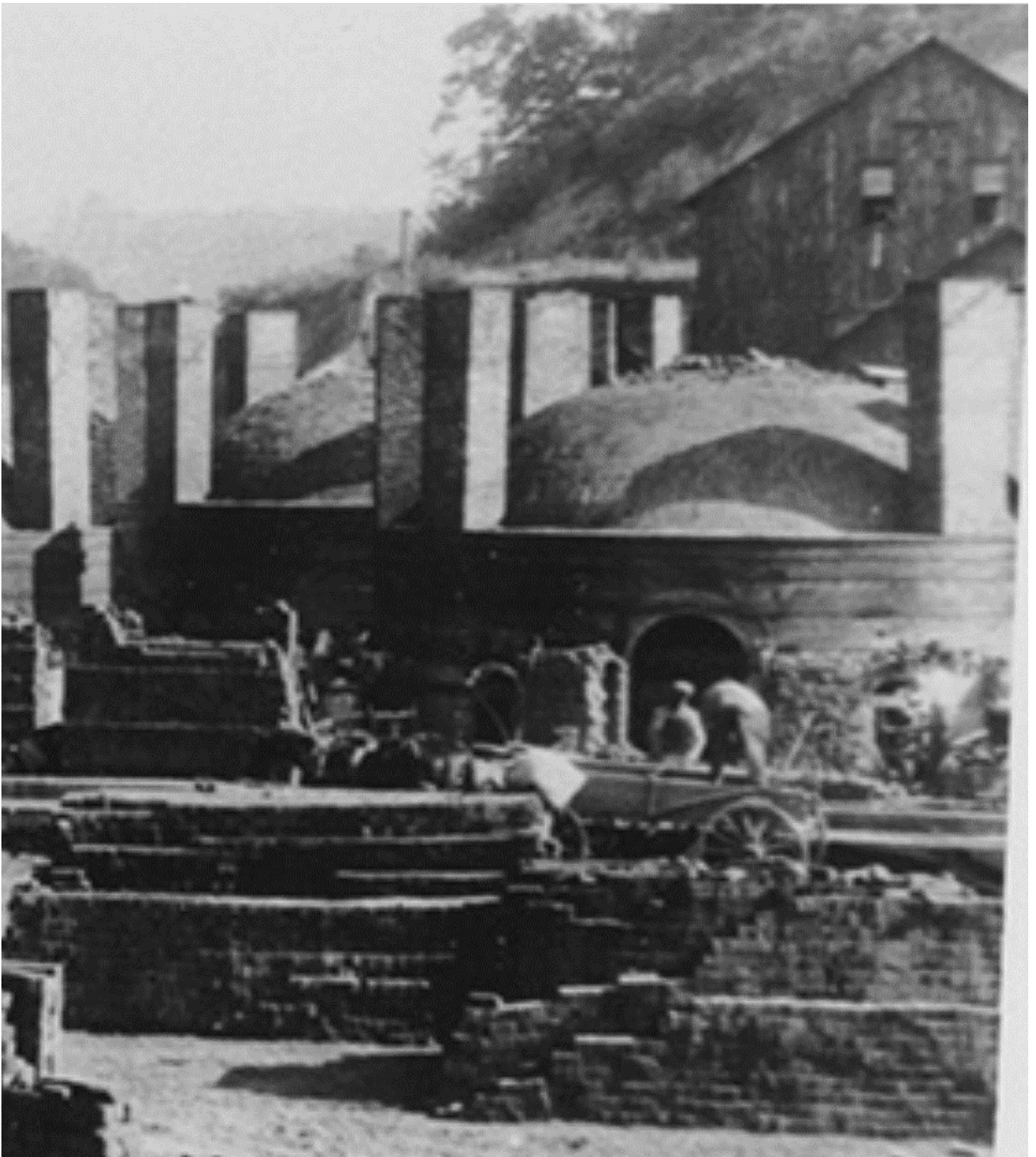


Figure 37A. A detail. Note the two men, with a horse- or mule-drawn wagon, working near kiln on the right. This gives a sense of the size of the kilns at the Johnson City Shale Brick Company. Also note the brick piles in the foreground. Given the building in the right background, it is difficult to relate this photograph to that in Figure 36. Nevertheless, this image appears to be looking south. Perhaps much of the plant was destroyed by a fire in 1926, and this building was constructed after that fire.



Figure 38. “Johnson City Brick & Tile Co. 2, 3’... Johnson City, Tenn. 8 – 38”.¹⁷⁵ Note that this locomotive appears to be listed as engine #2 and as being 3-foot gauge. Also note that “Johnson City Brick & Tile Co.” appears to be a case of mistaken identity for Johnson City Shale Brick Corporation, see Figure 39.

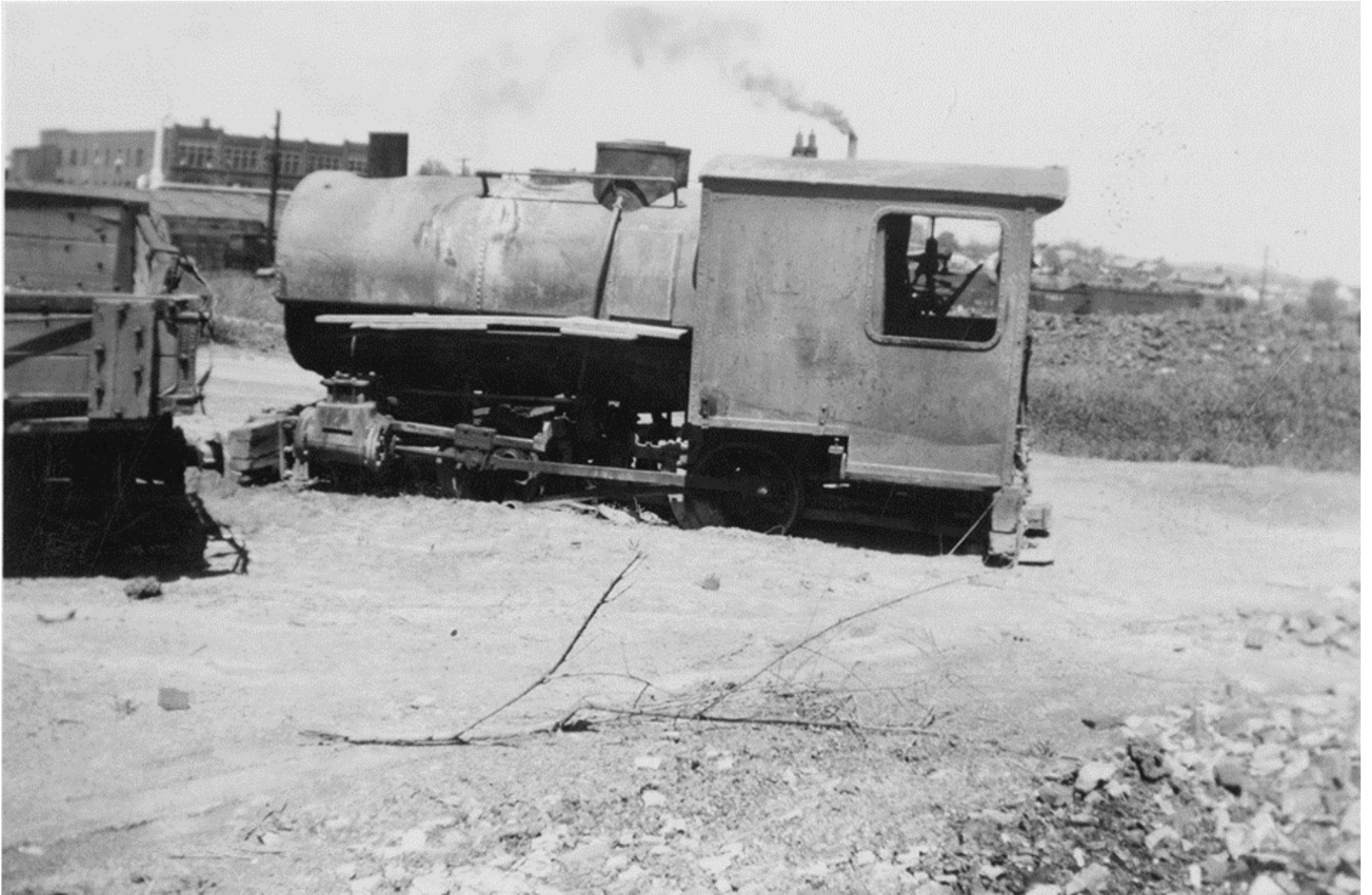


Figure 39. This appears to be a later photograph of the same locomotive that is depicted in Figure 38, which was dated August 1938. Note the Wilder block in the left background. It was occupied by the Empire Furniture Company at this time.¹⁷⁶



Figure 39A. A detail.

Johnson City Shale Brick Corporation (1920 – 1928)
General Shale Products Corporation (1928 – present)

4.3.1. Johnson City Shale Brick Corporation (1920 – 1928)

January 1920. The Clay – Worker.

“The Johnson City Shale Brick Co., of Bristol, Va. – Tenn., has been incorporated with \$500,000 capital stock, by George W. Hardin, H.D. Camp, L.H. Shumate and others. J.W. Reagan, formerly of Chattanooga, is the manager of the plant.”¹⁷⁷

1920. Brick and Clay Record.

“Johnson City Men Form Brick Company – The Johnson City Shale Brick Corporation has been incorporated at Bristol, Va., with a capital of \$200,000, by George W. Hardin, president and L.H. Shumate, secretary, both at Johnson City.”¹⁷⁸

1 January 1920. Manufacturers Record.

“Machinery, Proposals and Supplies wanted... Motors. – Johnson City Shale Brick Corp., Johnson City, Tenn. – Two 220-volt 60-cycle 3-phase 100 H.P. motors; 220-volt 60-cycle 3-phase 30 H.P. motor; 220-volt 60-cycle 3-phase 15 H.P. motor...

Shovel. – Johnson City Shale Brick Corp., Johnson City, Tenn. – 5/8-yd. electric shovel.”¹⁷⁹

4 December 1920. “Report on the Natural Resources Found of that Portion of the Tennessee River Basin Traversed by and Immediately Tributary to the Carolina, Clinchfield & Ohio Railway...

Johnson City, Tenn. – The Cranberry Furnace Co. manufactures high-grade pig iron from the magnetic iron ore mined in Avery County, N.C. Rollin Chemical Corporation manufactures sodium sulphate, muriatic acids, and other chemical products. The M-B Chemical Corporation manufactures dyes. The Johnson City Shale Brick Corporation will be in operation in a very short while and will manufacture high-grade brick from shale near the plant. There are several woodworking plants, including chair factories, furniture factories, flooring, box, and textile supplies. The Johnson City mills manufacture hosiery, and the Tennessee silk mills manufacture silk products... D.C. Boy, Industrial Agent Carolina, Clinchfield & Ohio Railway, Johnson City, Tenn. December 4, 1920.”¹⁸⁰

Brick-Hollow Building Tile

*T*O picture the splendors of autumn in its glorious luxuriance of colors, its lavish richness, its mellow softness, is to describe the natural artistic beauties of the imperishable, harmoniously blended tones of

Johnson City Face Brick

In their wide variety of pleasing textures and colors is to be found a combination of mingling, or an individual shade, appropriately adapted to every style of architecture and every type of home or building.

At our plant, which operates the largest continuous kiln in the United States, with every modern facility at hand and an unlimited abundance of the highest quality shale, the ceramic art, as applied to brick and hollow tile manufacture, has been brought into a high state of practical scientific perfection.

The same high quality typical of our face brick is likewise predominant in our common building brick and hollow tile, which are outstanding for their tremendous strength and low absorption qualities, and our enormous production assures prompt deliveries, regardless of requirements.

We have an excellent sales agreement for reputable, wide-awake dealers in territory not already allotted.

INQUIRIES SOLICITED

Johnson City Shale Brick Corp.

JOHNSON CITY, TENNESSEE

“The South’s Greatest Face Brick Plant”

Figure 40. Johnson City Shale Brick Corporation advertisement, 11 December 1924.¹⁸¹

June 1925. The Clay-Worker.

“Modern Tennessee Brick Plant. The Johnson City Shale Brick Corporation of Johnson City, Tenn., has just completed a new kiln which is said to be the largest of its kind in the country because of its enormous capacity. The kiln which is now in operation was constructed by the Youngreen patents, controlled by the International Clay Machinery Co. It is composed of two batteries of nine chambers each; the chambers being 16 feet wide and 75 feet long. They are sufficiently high to accommodate 150,000 brick each, making the total capacity of the kiln, 2,700,000.

In operation, twelve chambers or tunnels comprise a firing cycle: Five cooling chambers behind and six preheating chambers in advance of the one under direct gas fire, which leaves at all times three chambers for setting and three for drawing. Through the pre-heating feature by which all excess heat is conserved and utilized, but 24 hours is required for the direct firing of each chamber, giving a daily output of 150,000, in addition to that produced from the old-round, down-draft kilns, of which there are 12 in operation. Producer gas is used for drying and burning, and is obtained from a special plant in connection, composed of six steel-jacketed, water-sealed producers of 8-foot inside diameter.

The mechanical equipment about the plant is in keeping with the drying and burning arrangements, only the most thoroughly modern machinery and latest approved methods being used. Another addition, also just completed, will enable the company to produce not only a large quantity of texture brick in a variety of shades, but a heavy tonnage of hollow building tile as well. Shale material of excellent quality is available in enormous quantities.

Sam R. Sells is president of the company which is owned and operated by Johnson City capital entirely. Other officers are: Vice-president, E.H. Hunter; treasurer, L.H. Shumate; superintendent, John S. Phelan. All construction incident to the new additions was done under the direct supervision of Mr. Phelan, who has contributed a number of important features in connection with the Youngreen kiln. The plant machinery was purchased from the International Clay Machinery Co., Dayton, Ohio. The Johnson City Foundry & Machine Co. and the Summers Hardware Company of this city supplied practically all other materials.”¹⁸²

1926. Blue Book of Southern Progress.

“Over \$1,000,000,000 of Southern Construction in 1925... Big brick plants were completed by... the Johnson City Shale Brick Co., Johnson City, Tenn...”¹⁸³

January 1926. The Ceramist.

“The Johnson City Shale Brick Corporation, Johnson City, is operating what is said to be the largest face brick plant in the south. The company uses one of the largest gas fired continuous kilns in the country with a capacity of approximately 2,000,000 brick. A battery of old style round kilns, also is in use. The capacity of the plant is about 150,000 brick per day, but production is not quite up to this figure. Both face and common brick, and hollow tile, as well as a patented hollow brick are manufactured. These products are marketed from Tampa, Fla., to New Orleans, La., including Memphis, Tenn.; Cincinnati, Ohio; Huntingdon, W. Va.; Norfolk, Richmond and Roanoke, Va.; Savannah, Ga.; and Jacksonville, Fla.; as well as many other intermediate points.”¹⁸⁴

11 February 1926. Manufacturers Record.

“Tenn., Johnson City – Following are additional subcontractors on American Bemberg Corporation’s plant between here and Elizabethton: Virginia Bridge & Iron Co., Roanoke, Va., on structural steel; C.O. Biddle, Johnson City, on plumbing; B.G. Young, Johnson City, for sewers; Johnson City Foundry & Machine Co., miscellaneous iron and steel; Johnson City Shale Brick Co., for brick; Summers Hardware Co., Johnson City, for hardware. Hughes – Foulkrod Co., 421 Seventh Ave., Pittsburgh, Pa., Genl. Contrs.; Lockwood, Green, & Co., Engrs.; 1 Pershing Sq., New York.”¹⁸⁵

27 May 1926. Manufacturers Record.

“Fire Damage... Tenn., Johnson City – Manufacturing section of Johnson City Shale Brick Corps., loss \$200,000.”¹⁸⁶

3 June 1926. Manufacturers Record.

“To Rebuild Brick Plant at Johnson City. The Johnson City Shale Brick Corporation of Johnson City, Tenn., whose plant was recently burned at an estimated loss of \$200,000, wires the Manufacturers Record that it is making plans to rebuild as soon as possible, and that engineers of the Chambers Brothers Co. and the Link – Belt Co. both of Philadelphia, will probably design the new plant. The Johnson City Shale Brick Corporation, of which Sam R. Sells is president, is one of the largest manufacturers of shale brick in the South.”¹⁸⁷

17 June 1926. Manufacturers Record.

“Rushing Rebuilding of Brick Plant. Johnson City, Tenn. – Work in connection with the rebuilding of the burned plant of the Johnson City Shale Brick Corporation is under way. Through the courtesy of E. Taylor Chewning of the Continental Clay Products Company, Washington, D.C., the installation of new machinery to take the place of that destroyed by the fire has been very materially facilitated. Mr. Chewning turned over to the Johnson City Corporation certain machinery which was being made for him, enabling the machinery companies to divert the equipment to the local firm.

Chamber Bros. Company of Philadelphia, the Link – Belt Company of Philadelphia and the Hadfield – Penfield Steel Company, Bucyrus, Ohio, are furnishing the new equipment. The engineers of the three companies are cooperating with the brick company’s organization in laying out and constructing a very modern and efficient brick and tile plant. Complete equipment for the new plant will be shipped within the next two or three weeks and will be placed as quickly as the foundations are ready to receive it.

The company will very shortly decide upon the type of buildings to house the new equipment.”¹⁸⁸

July 1926. The Clay-Worker.

“Work on Tennessee Brick Plant Progressing. – Work in connection with the rebuilding of the burned plant of the Johnson City Shale Brick Corporation, at Johnson City, Tenn., is under way. Through courtesy of E. Taylor Chewning, of the Continental Clay Products Company,

Washington, D.C., the installation of new machinery to take the place of that destroyed by the fire has been very materially facilitated. Chewing turned over to the Johnson City corporation certain machinery which was being made for him, enabling the machinery company to divert the equipment to the local firm.”¹⁸⁹

4.3.2. General Shale Products Corporation, 1928 – Present

1928. Daily Bulletin of the Manufacturers Record.

“Brick Companies Merge with \$1,250,000 Capital Investment – To Produce 300,000 Bricks Daily. Johnson City, Tenn. – The Kingsport Brick Corporation, Kingsport, Tenn., and the Johnson City Shale Brick Corporation have consolidated as the General Shale Products Corporation, with general offices in this city. The merger represents a total capital investment of \$1,250,000, while the total plant capacity will be 300,000 brick daily, or approximately 90,000,000 annually, exceeding the production of any other shale brick plant in the Southeast, it is said.”¹⁹⁰

5 September 1928. Daily Bulletin of the Manufacturers Record.

“Johnson City Shale Brick Corp., Johnson City, advises that all new machinery for Kingsport plant of General Shale Products Corp. (formed by merger of Johnson City Shale Brick Corp. and Kingsport Brick Co.) has been purchased from Chambers Bros., Co., 52nd and Media Sts., and Link – Belt Co., Hunting Park Ave., both Philadelphia, Pa.”¹⁹¹

1925 – 1940s. “The General Shale Products Corporation, Johnson City, Tennessee, was created through a buy-out and merger of the Kingsport Brick Corporation and the Johnson City Shale Brick Corporation by General Shale Products of Delaware in 1925. The newly consolidated East Tennessee company began operation September 1, 1928 after three years of negotiation. One year later four more regional quarry companies were purchased by the Johnson City Division of General Shale Products; these acquisitions included the Oliver Springs Company, Richlands Brick Company, Bristol Brick Company, and Jellico Brick and Coal Company. The Cherokee Brick Corporation of Knoxville, Tennessee also was leased by General Shale Products from 1929 to 1936. The original company management included: Sam R. Sells as President; E.H. Hunter as Vice-President; J.C. Stone as Secretary; and Glen Bruce as Treasurer.

Throughout the 1930s and 1940s the region’s finest homes, churches, and public schools were built from General Shale Products’ red facing brick. During the company’s peak production years, the Johnson City Division became the largest manufacturer and seller of brick in the United States, annually producing 1.2 billion bricks. As a result of General Shale Products acquisition of several of its regional competitors and an increase in its production, the East Tennessee company was able to capture and control between 12% and 15% of the total market share within the United States by the end of the first half of the twentieth century.”¹⁹²

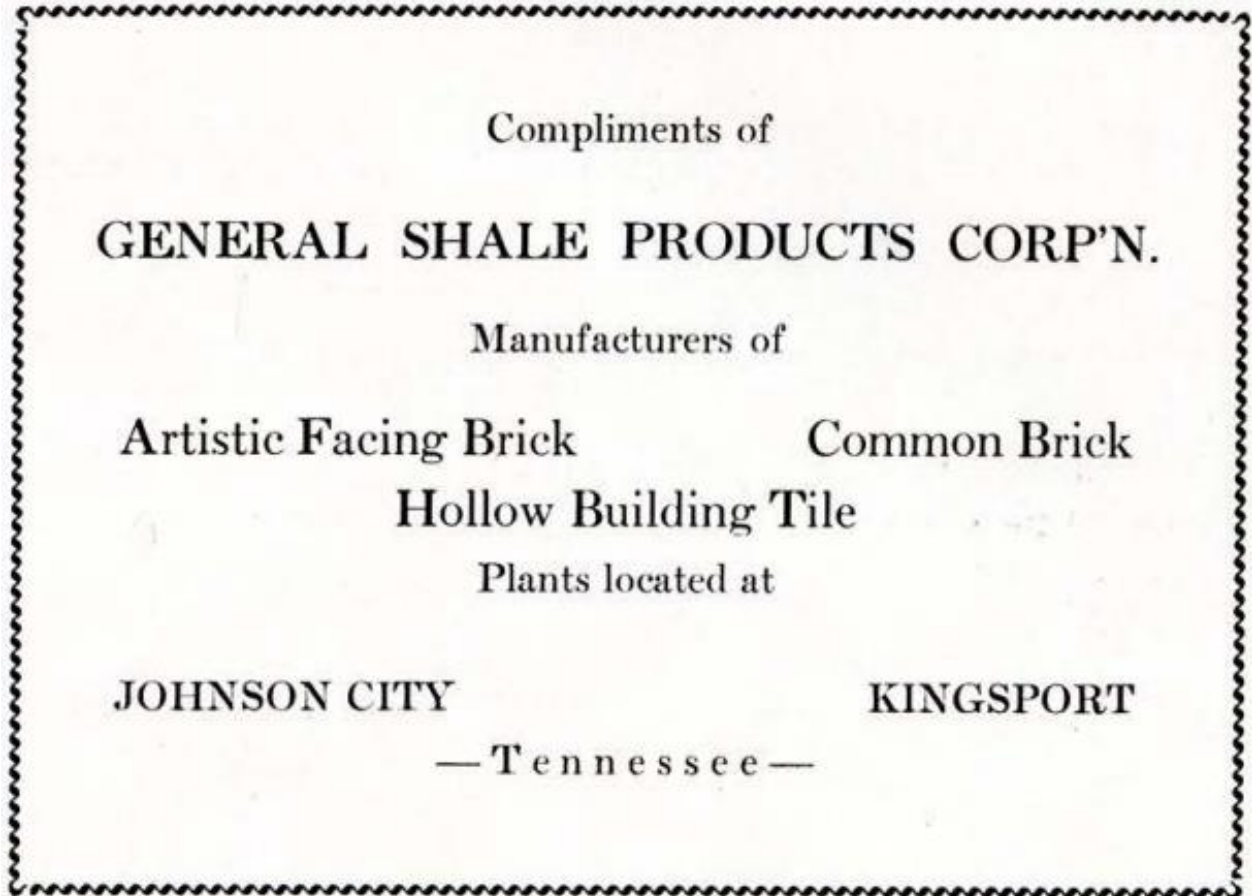


Figure 41. Old Hickory, 1929.¹⁹³ Added 24 September 2024.

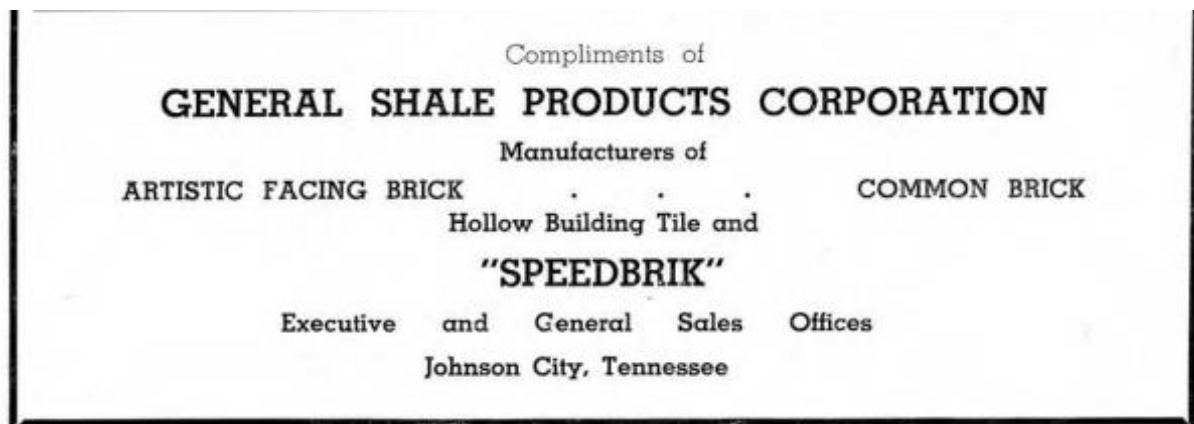


Figure 42. The Buccaneer, 1942.¹⁹⁴ Added 24 September 2024.

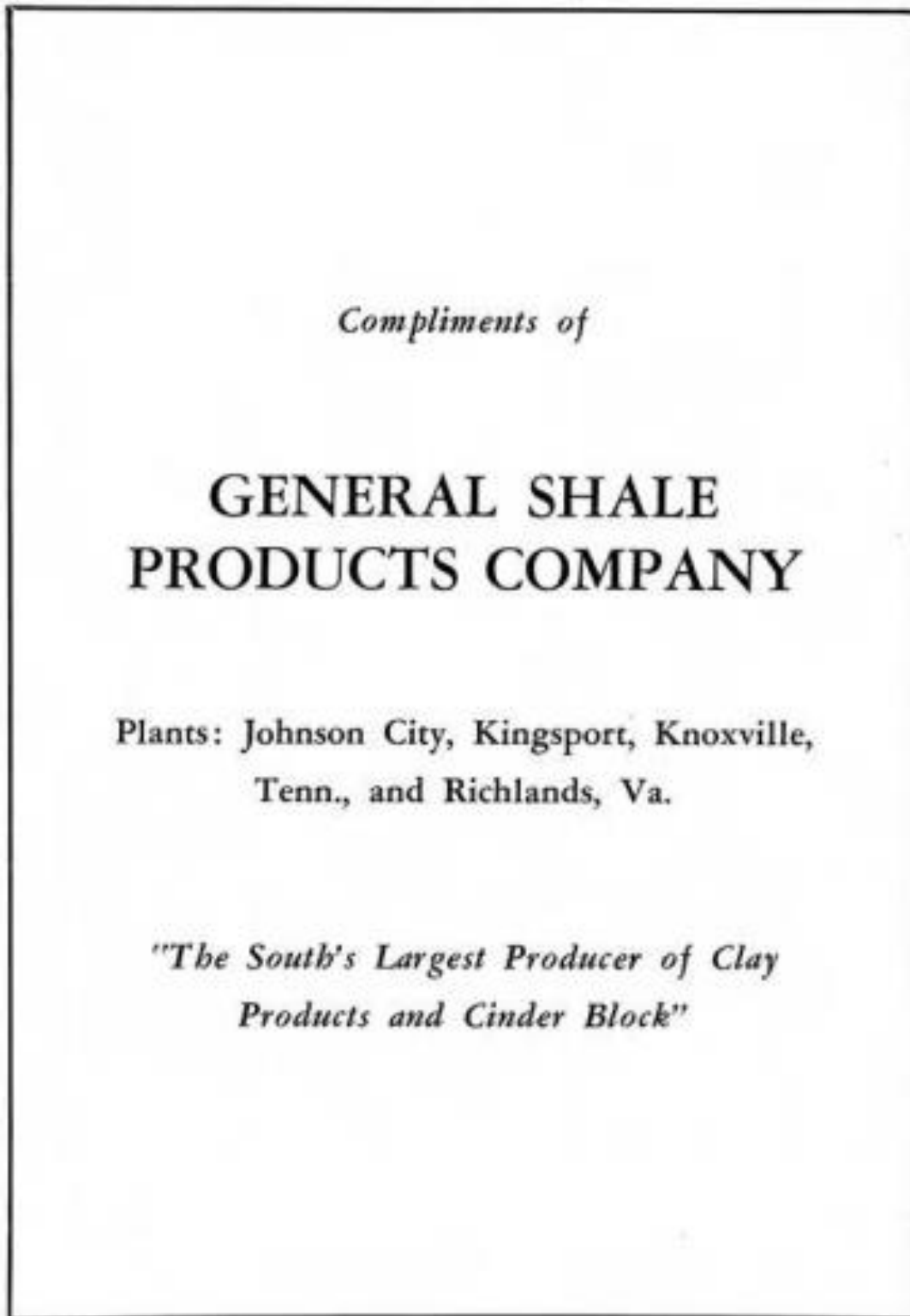


Figure 43. The Buccaneer, 1951.¹⁹⁵ Added 24 September 2024.

Information on later operations and financial matters related to the General Shale Products Corporations are available in the George C. Sells Papers.¹⁹⁶ Although the General Shale Products Corporation is still in business, it does not appear that any of the buildings of this facility survived to the present time. (Revised 24 September 2024)

Table 44. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.¹⁹⁷

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Shale Brick Corporation	Brick Mfg.	45			

Table 45. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1925.¹⁹⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Shale Brick Co.	Mfg. Brick	56			

Table 46. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1928.¹⁹⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Shale Brick Co.	Mfg. Brick	60			

Table 47. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1929.²⁰⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Johnson City Shale Brick Co.	Mfg. Brick	43			

5. Railroad Facilities in Carnegie, 1887 – 1914

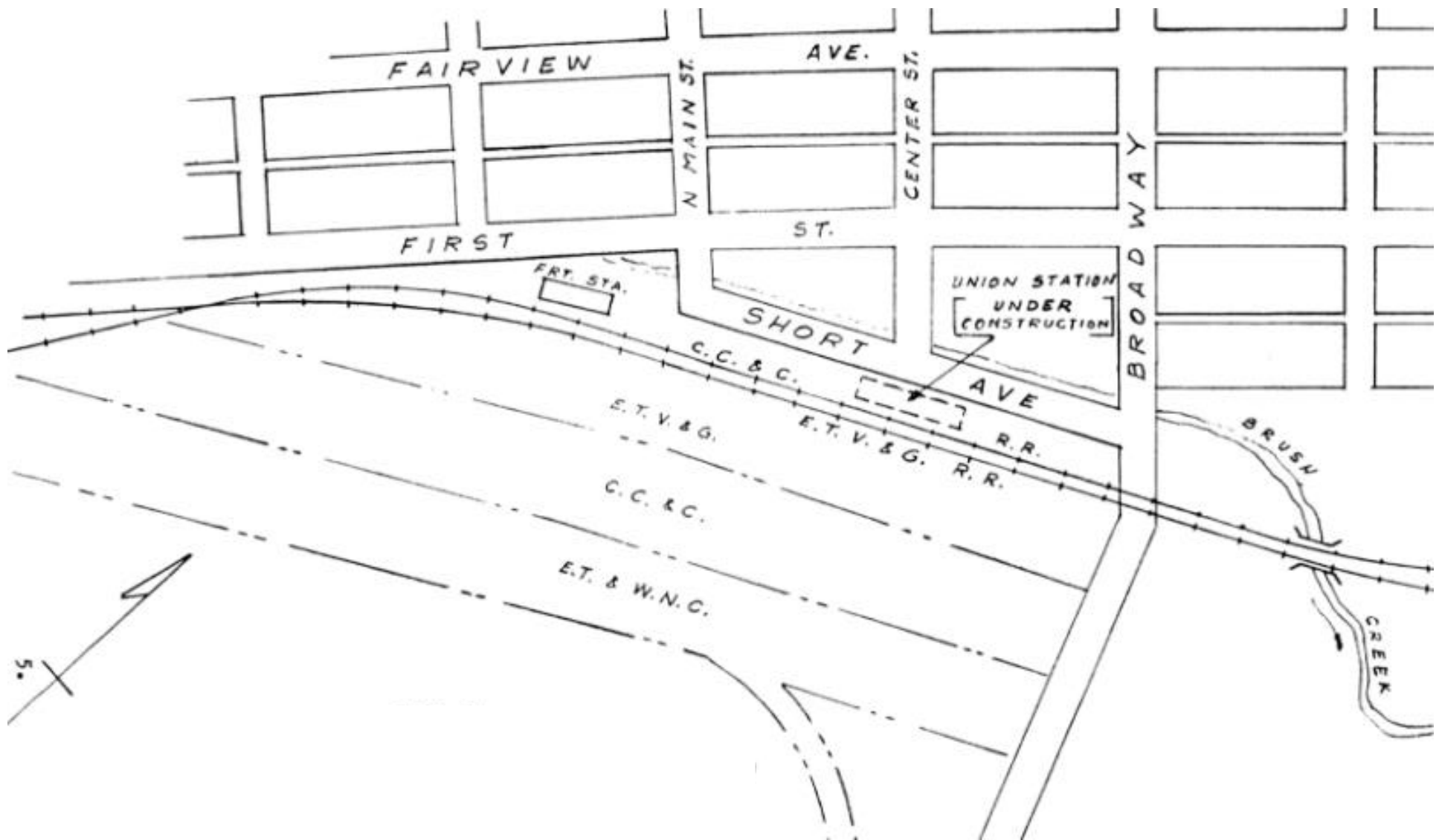
Charleston, Cincinnati and Chicago (C.C. & C., also “3C’s”) Railway, 1887 – 1893

Ohio River & Charleston Railway, 1893 – 1902

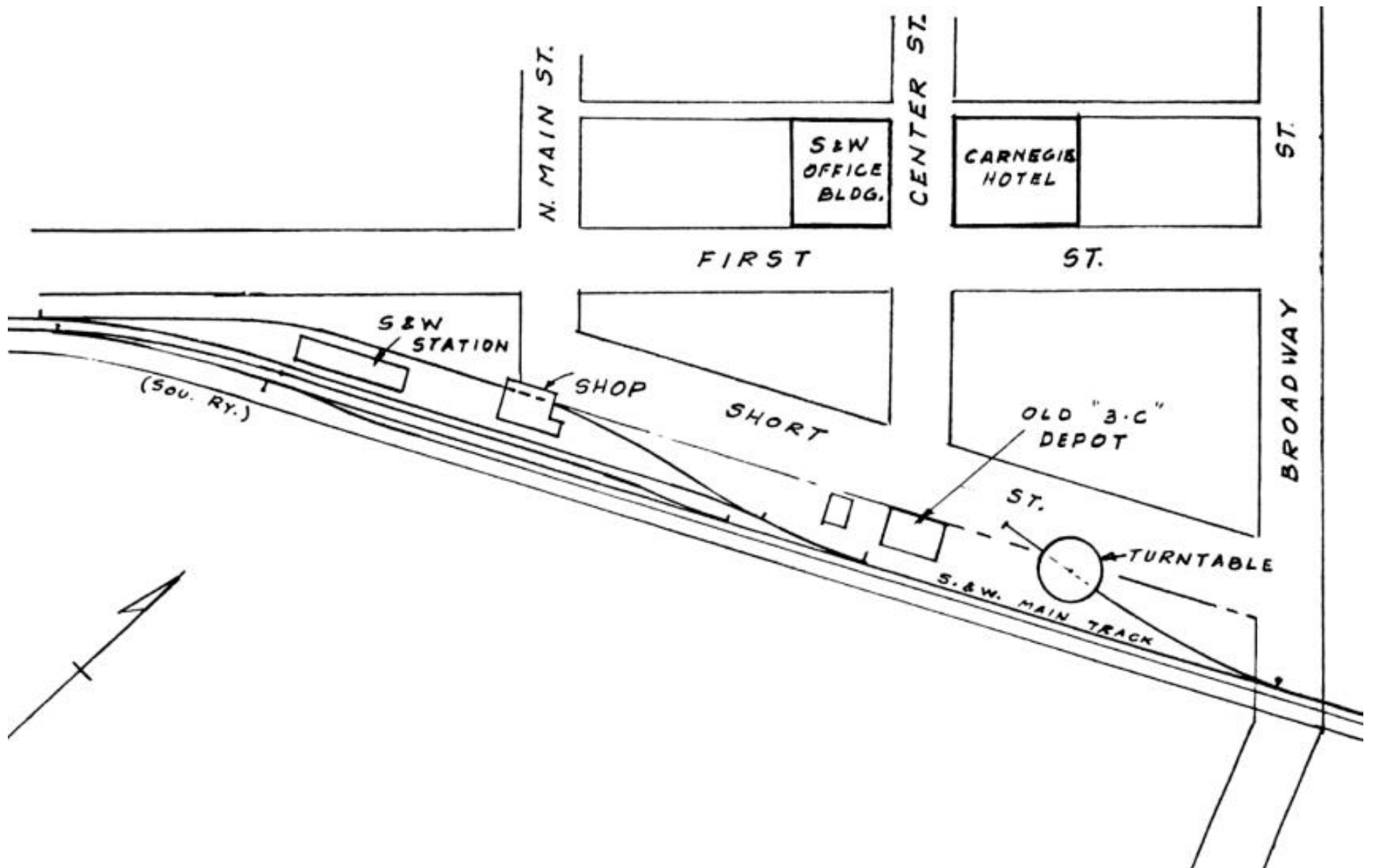
South and Western Railway Company, 1902 – 1908

Carolina, Clinchfield and Ohio Railway, at this location 1908 – 1914

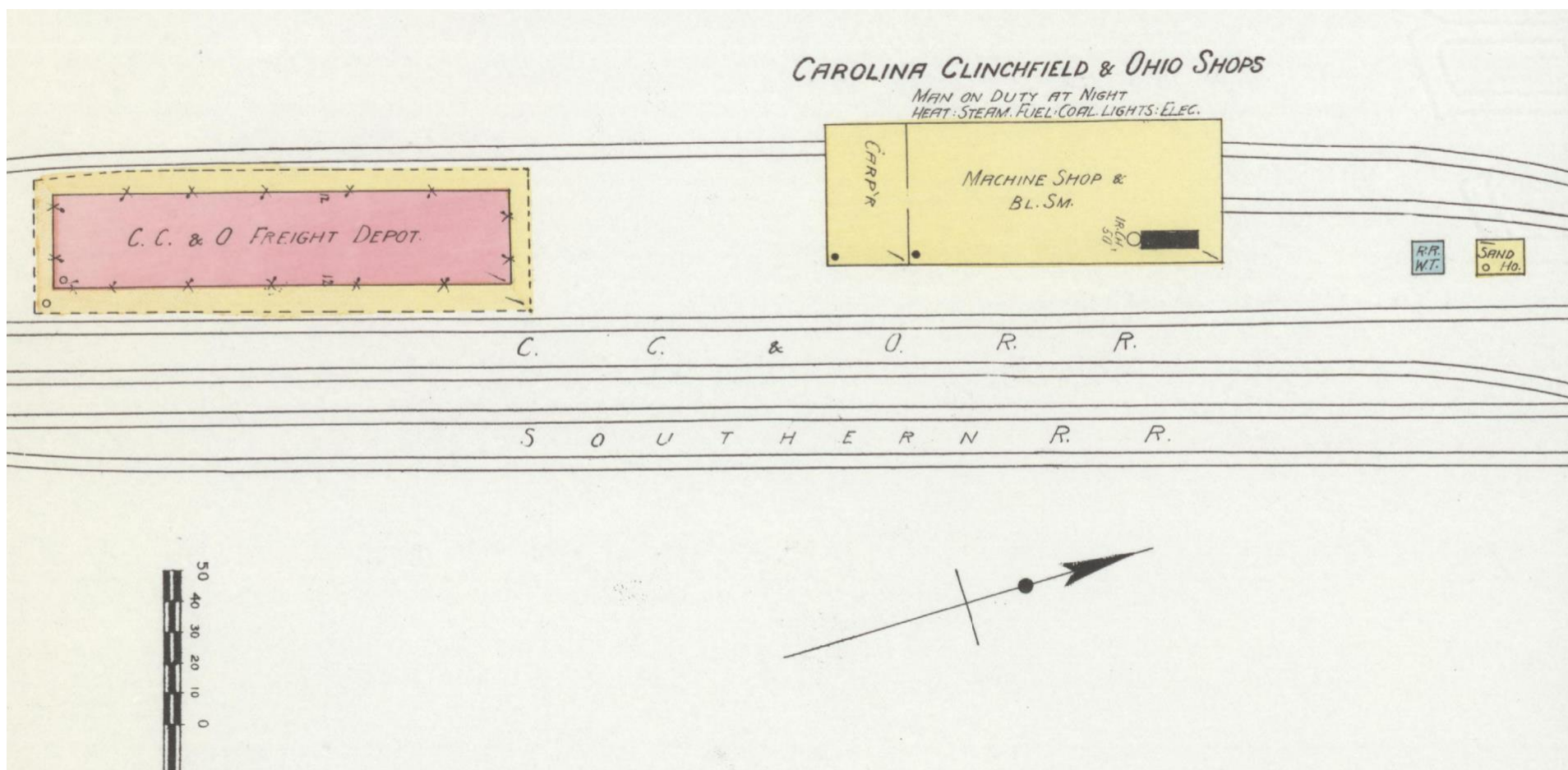
Note: E.T.V. & G./Southern Railway and Johnson City Traction developments in the Carnegie area are integrated in this section with the timeline of the Clinchfield and its predecessors.



Map 17. "3-C R.R. Facilities Johnson City (Carnegie) 1893", note the "Frt. Sta." and "Union Station Under Construction". The three parallel areas labeled "E.T.V. & G.", "C.C. & C." and "E.T. & W.N.C." appear to indicate planned allocations for a future railroad yard.²⁰¹



Map 18. "South and Western R.R. Facilities Johnson City (Carnegie) 1907". Note: the "S & W Station", "Shop", "Old '3-C' Depot" and 80-foot "Turntable", from left to right, along the "S. & W. Main Track." Also note that the "S&W Office bldg." is the "Wilder Block."²⁰²



Map 19. A map depicting the brick “C.C. & O. Freight Depot” and the “Carolina Clinchfield & Ohio Shops” (which included a machine shop, a black smith shop, and a carpenter shop), as well as a “R.R. W.T.” (Railroad Water Tank) and “Sand Ho.”, in December 1908. Regrettably, the author has not found any images of the “Carolina Clinchfield & Ohio Shops”, the “R.R. W.T.” or the “Sand Ho.”²⁰³ Interestingly, the Johnson City Comet reported that the shops were destroyed by a fire in February 1908, indicating that the shop building depicted on this map was a temporary structure. See also Map 15.



Figure 23E. A detail that includes the west and south sides of 3 C's freight station/S. & W. station/C.C. & O. freight depot. It appears that this image dates to 1926 or later.



Figure 30D. A detail that includes the west and south sides of 3 C's freight station/S. & W. station/C.C. & O. freight depot. It appears that this image dates to between 1920 and 1925.



Figure 29F. A detail that includes the south and east sides of 3 C's freight station/S. & W. station/C.C. & O. freight depot, note the Santa Fe boxcar on the right. It appears that this image dates to between 1920 and 1924.



Figure 72A. A detail of the unfinished the 3 C's depot, looking south from near the Carnegie Hotel, by August 1908.



Figure 51A. A detail of the unfinished the 3 C's depot, looking north. It appears that the stone building, on the left, was roofed over by the South & Western Railway in 1905 for use as a carpenter shop. Given that the Hotel Carnegie burned down in 1910, this image appears to date to between 1905 and 1910.



Figure 30E. A detail of the unfinished the 3 C's depot, looking north. This photograph appears to have been taken after that in figure 28 and 29, and to date to between 1920 and 1925.

Charleston, Cincinnati and Chicago (C.C. & C., also “3C’s”) Railway, 1887 – 1893
Ohio River & Charleston Railway, 1893 – 1902
South and Western Railway Company, 1902 – 1908
Carolina, Clinchfield and Ohio Railway, at this location 1908 – 1914

Note: E.T.V. & G./Southern Railway and Johnson City Traction developments in the Carnegie area are integrated in this section with the timeline of the Clinchfield and its predecessors.

5.1. Charleston, Cincinnati and Chicago (C.C. & C., also “3C’s”) Railway, 1887 – 1893

1885 – 1889. East Tennessee: Historical and Biographical.

“In 1885 Mr. Matson was appointed chief engineer of the Three C’s Railroad, one hundred and forty miles of which he built through the Carolinas. His connection with this road was severed in 1889, after which he assisted in organizing the Johnson City and Carolina Railroad, of which he was president until built... He was elected mayor of Johnson City in March, 1892, on the Democratic ticket, as one of the popular and entirely essential men of this growing and prosperous town... Johnson City has many thoroughly alive men, but none more so than Mr. Matson, to whom is due a fair share of the prosperity of the place”²⁰⁴

28 October 1886. Johnson City Comet.

“Our New Railroad. Attention is called to the full report of the railroad meeting at Charleston on the 30th inst. From present indications it is only a matter of a few months until you can take the train in Johnson City and go to Charleston or Cincinnati without change. Then look out for Johnson’s. When the coal of Kentucky and the iron ore of North Carolina meet, Johnson City will be the Birmingham of Tennessee. Land has already been purchased on which to erect furnaces as soon as coal can be brought here at reasonable rates. It is a well known fact that the Cranberry iron makes the finest Bessemer steel and the Kentucky coal has also been found to be best suited for that purpose.

Col. T.E. Matson has been appointed Chief Engineer and will make headquarters at Johnson City and will begin locating in both directions from this point...”

21 April 1887. Johnson City Comet.

“Col. T.E. Matson returned from Richmond and Yorkville last Sunday morning and left again Tuesday night for Boston. Col. thinks dirt will be broken here on the C.C. & C. road inside of four months. The intention of the company is to reach the coal fields as soon as possible. They mean business and will push the road as rapidly as possible.”

5 May 1887. Johnson City Comet.

“Mr. S.H. Miller sold to Col. E.C. Reeves and associates 20 acres of land last Saturday for \$4,000 cash. Four acres of the above tract is to be donated to the C.C. & C. Co. for depots, shops, &c.”

3 November 1887. Johnson City Comet.

“The C.C. & C. Railroad company are fitting up an office over C.K. Lide & Co.’s store, and a private office for Chief Engineer, Col. T.E. Matson. Johnson City is headquarters for the Northern Division, and Rock Hill, S.C., headquarters for the Southern Division of this road.”

12 January 1888. Johnson City Comet.

“The Johnson City Foundry and Machine Works are crowded with work. They are at work on a large lot of bridge castings for the C.C. & C. railroad and an order for car wheels from Pulaski City, Va.”

23 February 1888. Johnson City Comet.

“The Three C’s. – Four Engineer Corps to be Put in the Field and the Entire Line Located at Once. At a meeting of the officials of the Charleston, Cincinnati and Chicago Railroad Company in New York, on the 15th inst., Col. T.E. Matson was re-elected Chief Engineer and Gen. Rosser was appointed Consulting Engineer, to have charge of location north of Marion, N.C., until grading is completed on the southern division. Col. T.E. Matson returned Monday via South Carolina and was seen by a Comet representative and found to be full of important information that will be read with pleasure by our citizens and those interested in the future of Johnson City. He says it is the intention of the company to complete the location of the entire line during the coming summer and to do this four additional engineer corps will be put in the fields at once. Two of the corps will be put to work between Marion, N.C. and the Tennessee line and two between the Tennessee line and the Breaks at the Kentucky line. Reports, &c., as heretofore, to be given to the Chief Engineer. The instruments, tents, cots, &c., have been ordered, and the outfit for two corps are expected to arrive in Johnson City within a week. The other two outfits will go to Marion, N.C. . . .

There is no longer any question about the road running through Johnson City. The company propose to build the shortest and cheapest line, and Johnson City is so located as to be on that line. The second and third stories of the new real estate building and the second story of Jobe’s building, over C.K. Lide & Co., have been rented by the C.C. & C. Co., and will be occupied as offices for the Chief Engineer and Vice-President. Col. Matson expects to move his draughtsmen to Johnson City from Rock Hill, S.C., some time during May.”

28 February 1888. Johnson City Comet.

“The Bond Proposition... Said Railroad Company hereby agrees to commence the construction of said line of Railroad through said Town, within four months from date hereof and to construct and put in operation the line through the County as above set forth within one year from this date. The C.C. & C. R.R. Company agrees to erect and maintain its Shops for building cars and locomotives and the repair of the same, within the limits of Johnson City Corporation, near where the line of said Railroad crosses the track of the E.T.V. & G. R.R. as the Chief Engineer of said Railroad may determine the sight thereof. This proposition when accepted by said Town and satisfied by the voters of the same shall be in lieu of the proposition heretofore acted on, proposing to give \$50,000 of bonds to said Railroad Company. R.A. Johnson, General manager.”

26 April 1888. Johnson City Comet.

“Mr. H.T. McDaniel, chief engineer in charge of the corps now surveying the Three C’s down the Chucky river route, was in the city Saturday and said he hoped to reach Johnson City by the first of May. He will make headquarters here then and locate the line north from this point.”

5 October 1888. The Railroad Gazette.

“The Hinckley Locomotive Co., of Boston, Mass., has received an order from the Charleston, Cincinnati & Chicago for three locomotives for that road.”²⁰⁵

22 February 1889, The Railroad Gazette.

“The Schenectady Locomotive Works... have recently completed a number of locomotives for the Charleston, Cincinnati & Chicago, and will soon begin work on a new order for that road.”²⁰⁶

8 March 1889. The Railroad Gazette.

“Charleston, Cincinnati & Chicago. – McDonald, Shea & Co., of Knoxville, Tenn., who have the general contract for the line from Rutherfordton, N.C., to Johnson City, Tenn., and Minneapolis, Va., have sublet sections to A.R. Wing and J.C. Sullivan, of Johnson City, Tenn. The surveys of the uncompleted portion of the road, from Rutherfordton to Whitehouse, Ky., have been nearly all completed. Thomas E. Matson, of Johnson City, is Chief Engineer.”²⁰⁷

28 June 1889. The Railroad Gazette.

“Mr. A.N. Molesworth is Acting Chief engineer of the Charleston, Cincinnati & Chicago, succeeding Col. Thomas E. Matson, who has tendered his resignation...”

McDonald, Shea & Co., of Knoxville, Tenn., who have the contract for building the line from Johnson City, Tenn., to Minneapolis, Va., a distance of about 90 miles, put a force of 500 hands at work last week at Johnson City.”²⁰⁸

16 August 1889. The Railroad Gazette.

“Charleston, Cincinnati & Chicago. – The company has been organized in Tennessee by John T. Wilder, H.H. Carr, A.R. Johnson, J.W. Cure and J.A. Cargille...”

The company has been granted a charter at Nashville, Tenn., to construct a road from a point on the state line of North Carolina through the counties of Carter and Unicoi, in Tennessee, to some point on the State line of Virginia, in the county of Sullivan, in Tennessee, at or near Moccasin Gap.”²⁰⁹

30 August 1889. The Railroad Gazette.

“Charleston, Cincinnati & Chicago. – Geo. W. Callahan, R.F. Rivenac, Wm. Stallings, T.H. Stansill and W.J. Colvin have been awarded sub-contracts on the line between Johnson City, Tenn., and Minneapolis, Va., by McDonald, Shea & Co., and they will at once put large forces on the work.”²¹⁰

12 September 1889. Johnson City Comet.

“Important Railroad Projects... The Charleston, Cincinnati & Chicago has a last, it seems, weathered its troubles, and, as stated last week, confirmed previous contracts for over \$6,000,000 worth of construction work, which will carry this road from Marion, N.C. to Minneapolis, Va., a new town. The mineral and timber wealth which this line will develop is beyond estimate. It passes through the heart of the high-grade ores of Western Carolina and strikes into the coking coals of Eastern Kentucky, and cuts through great forests of virgin timber.

(The Charleston, Cincinnati & Chicago railroad is now under construction both ways from Johnson City. More than 1,000 men are at work within ten miles of the city, and the force is being gradually increased. Ed. Comet.)”

12 September 1889. Johnson City Comet.

“The Three C’s. The Columbia, S.C., correspondent of the Charleston News and Courier of August 31, says:

People along the line of the Three C’s road are rejoicing at the resumption and extension of work upon the line in Virginia and Tennessee. There is no doubt that – money enough has been realized from the placing of bonds to warrant very active operations. McDonald Shea & Co., who have the contract for building the road from Johnson City Tenn., to Minneapolis, Va., a distance of ninety-three miles, have just doubled their force of hands, and the workers on the section from Johnson City to the North Carolina line, twenty-six miles, have also been doubled...”

12 September 1889. Johnson City Comet.

“Three C. Notes. All the contractors pay good laborers \$1.25 per day.

The song of the Three C. negroes is now heard in the land.

John Shea, of the firm of McDonald, Shea & Co., arrived in the city Monday evening from Knoxville.

F.C. Inglesing & Co. unloaded a car of mules and a car of carts, etc., Monday and sent them out to their work.

Five wagon loads of negroes came in on No. 2 Monday and were driven out to J.C. Sullivan’s camp near Devault’s ford.

Goodman & Foster received four car loads of carts, mules, hammers, drills, wheelbarrows, scrapers and camp supplies and one car load of powder Tuesday.

Capt. S.D. Dunavant unloaded three more cars of tools and material yesterday. He has 300 hands at work between Johnson City and the Carter county line and is constantly adding to the forces.

Mr. Henry, one of Goodman & Foster’s foreman arrived from North Carolina Friday with fifty negroes who are now at work in the cut near the Watauga Tannery.

G.W. Callahan unloaded three cars of material and one car of stock Tuesday. The stock and material was taken out to his work and is now at work on the Three C’s.

Capt. S.D. Dunavant sent a man to North Carolina last week to get laborers for the Three C’s. The man returned Friday with about one hundred and was sent back Saturday after more. It is the intention of each contractor to work as many men as the work will allow.

Chief Engineer A.N. Molesworth was telegraphed Monday by Col. C.H. Hudson to furnish estimates for a side track at the Junction of the E.T.V. & G. and C. C. & C. roads. Col. Molesworth took the measurements Monday afternoon and made a diagram and estimate and sent it to him. A

side track a mile long will be put in there as soon as the road bed can be put in condition to receive the ties, The track is for the accommodation of the contractors so that material and supplies can be unloaded near the work. As the road is completed the track will be extended north. It will not be long before a work train will be put on the Three C's here."

19 September 1889. Johnson City Comet.

"Railroad Notes. F.C. Inglesing, Three C contractor went down to Knoxville Tuesday after more hands.

P.F. Cogan has been shipping material and supplies and has gone over to his work at Estillville to make a beginning.

Burney Burlson has accepted a position on a Three C engineer corps. He is under Chief Smith and is located near Shipley's Ferry.

A number of contractors were in town Tuesday buying supplies. It was a picnic for the merchants, and wagons were hauling goods out to different camps all day.

Messrs. Scott & Harrington who have the contract for track-laying, ties and 60 miles of trestles, are here and have already commenced work getting out ties. They will locate one commissary near Johnson City. They will lay track as fast as the road-bed is graded.

Mr. Wm. Kenefick, the Kansas City contractor who has a 50 mile contract on the Three C's, passed through on No. 2 Tuesday for Bristol. He will go out on the S.A. & O. to Estillville and then drive to his work. He has 50 miles from Minneapolis south. He shipped about two train loads of stock and material from Kansas City, the day before he left, for his work. It will also be shipped via Bristol and Estillville.

Col. R.D. Rodgers has returned from a trip along the line of the Three C's. He says the sub contractors are rapidly getting in shape to work. Nearly all of them have a force at work grading and are constantly adding to it. Each contractor has a good deal of work to do yet on commissaries and camps. It requires time to get ready to do work satisfactorily, but everything will be straightened out and in full blast in ten days along the entire line."

26 September 1889. Johnson City Comet.

"Three C. Notes. J.C. Sullivan sent out a hack load of negroes Sunday to his work.

Thirty mules arrived last night for J.C. Sullivan from Kansas City. They were several days on the road.

The weather has been very favorable and contractors have been getting in full time and are making a good start.

Goodman & Foster are getting along nicely with their work through the tannery property and will soon have the work through the city finished.

Capt. S.D. Dunnavant is fast getting a gap in the Bowman hill near the narrow gauge crossing. He has a 35 foot cut there, and it is about 500 feet long.

Mr. Chaffin, of Knoxville, closed the contract Saturday for Section No. 18. He is at work this week building camps and getting ready to start work.

Wagons from different commissaries have been in the city every day this week after goods. The big wagons, drawn by four big mules, are familiar objects to our citizens.

A number of negroes came down from Bristol on No. 1 Tuesday to work on the Three C's. They were met here by Goodman & Foster's 'four-in-hand' and taken to camp.

J.C. Sullivan has his work at Austin Springs pretty well covered and is preparing to put a force of hands at work on Section No. 20, at Spurgin's Gap. By the 20th of October he will have all of his work started.

J.C. Sullivan, who is working near Austin's Springs, made the largest blast reported so far since the work was begun. Last Thursday he blew over 1,200 cubic yards of stone at one blast, and only used 50 kegs of powder and one box of dynamite. He has some nice cliff work on that piece of his line and will do some big 'shooting' before he gets through with it.

Messrs. N.G. Scott & Co. expected to lay some track this week but did not get started. They will begin the first of next week at the junction of the Three C's and the E.T.V. & G. and put in a switch and lay a piece of track to be used as a side-track. Their track-laying machine will be here in about three weeks and they will put track down rapidly when they begin in earnest."

26 September 1889. Johnson City Comet.

"They Mean What They Say. Work of grading on the Three C's railroad is being pushed very rapidly from Johnson City towards Erwin. There is no longer any doubt for any person as to the road being built. Messrs. McDonald, Shea & Co. say they will run the cars into Erwin by the first of August, 1890. This will be plenty time to comply with the contract of Unicoi county. – Erwin Unakean."

3 October 1889. Johnson City Comet.

"Three C. Notes. J.H. Smalling, of Carter's, closed contract last week for building the trestle near Jas. Hunt's. He is to begin work at once.

S.H. Hendrix has leased his limestone quarry at Carter's to Messrs. Kegley & Eversole, Three C. contractors, for a term of three years.

John Shea, the all-around hustler of the firm of McDonald, Shea & Co., is at Chattanooga after hands. He is advertising for 5,000 men. He will get there, too, and the woods will soon be iuller'n they are of Three C. laborers.

Public square is crowded every day with contractors and wagons, after goods. It is a glorious sight, and we do wish our neighbor, Bristol, could enjoy it with us.

A. Sullivan sent a wagon train out to his work, section 20, last Thursday. There were ten teams, and the wagons were loaded with material, tools and camp supplies. Several large scrapers were also in the train."

4 October 1889. The Railroad Gazette.

"Charleston, Cincinnati & Chicago. – McDonald, Shea & Co. now have the entire division between Johnson City, Tenn., and Minneapolis, Va., where the line will cross the Clinch Valley division of the Norfolk & Western, covered with contractors; so far they have let the following contracts: For grading in Tennessee: John Hasson & Co., 4 miles; A. Sullivan, 9 miles; W. McD. Burgin, 3 miles; Peter Peterson, 2 miles; Crigger & Evans, 2 miles; W.G. Howell & Co., 2 miles; F.C. Englesing & Co., 2 miles; G.W. Callahan & Bro., 4 miles; J.C. Sullivan, 2 miles; Dunarent & Miller, 2 miles; S.D. Dunarent, 2 miles; Foster & Goodman, 2 miles, and J.P. Griffiths, 1 mile... W.G. Scott & Co. have been awarded the contract for all tracklaying, cross-ties and 50 miles of trestling. Gates & Bye have been awarded 20 miles of trestling, and J.G. Choffing 14 miles of trestling. The masonry is divided amongst a large number of small contractors.

The contractors expect to have 100 miles of the division completed by Dec. 1. McDonald, Shea & Co. have opened an office at Johnson City, Tenn.”²¹¹

10 October 1889. Johnson City Comet.

“Three C. Notes. J.E. Griffith received a car load of carts and other implements this morning...

A train load of rails for the Three C’s arrived and was unloaded last Saturday at the junction of the E.T., Va. & Ga. And C.C. & C. roads.

The Scott Banner says seven wagon loads of supplies left Estillville Saturday for the Three C’s near Osborn’s Ford.

Henry Inglesing, of McDonald, Shea & Co., was down from Johnson City yesterday. He says that ten miles of the track will be laid on the Three C’s road by February 1st. – Journal.

McD. Burgin received a number of new carts Tuesday from Lynchburg. They were a bright red and made a nice procession as they were being driven through the city on the way to his work.

P.F. Cogan left last Friday evening for New York for hands for his work at Estillville. He will ship three hundred Italians out at once. He told the Comet he had his work in shape for working and had several gangs at work, but as soon as he gets the men he will more dirt lively.

Messrs. N.G. Scott & Co., have the ties and everything ready to begin laying track and are waiting for a frog to arrive to put in a switch where the Three C’s intersects the E.T., Va. & Ga. Road. The frog is expected every day and track-laying may be commenced this week.”

24 October 1889. Johnson City Comet.

“Railroads are building all over the South, and Johnson City is getting its share. The town has grown to such proportions and is so advantageously located, that railroad companies are anxious to build to it, but they are not more anxious than Johnson City is to have them do so. The citizens are alive to their interest and know from actual experience what value railroads are to a city. A few months ago Johnson City voted \$75,000 to the Charleston, Cincinnati & Chicago Railroad Company. To-day two thousand men are at work on the line in Washington county, and the 90 miles from here to Minneapolis [Virginia] are covered with hands. The result is, Johnson City is alive. Real estate commands a good price and sells readily. The merchants are doing twice the business they did formerly and see more dollars in a week now than they used to in a month. Everybody is in good spirits and not afraid to spend a dollar. The result is, the town is prosperous, factories are working overtime, and new buildings are being built all over the city. The Three C’s is the cause of this prosperity, and the growth has really not begun.”

24 October 1889. Johnson City Comet.

“Three C. Notes. A gang of 50 negroes for the Three C’s got off of No. 1 Monday.

Several more car-loads of 60-pound steel rail arrived this week for the Three C’s.

Several car-loads of carts and material passed through the city Monday for the Three C’s. It was unloaded at Piney Flats and Bristol and sent out on the line.

Messrs. Kegley, Eversole & Co., who have the contract for building the butments and culverts of Sections 33 and 34, report that they are making good progress, and that inside of or about the expiration of four weeks their work on the two sections will be completed.

J.C. Sullivan put a force of hands at work on Section 12 last Saturday, and has about 200 men on it by this time. Mr. Sullivan is now erecting a camp on his work near Kingsport. When this is finished it will make four in all he will have in operation. Three of them are now crowded with laborers,

John Gates, the bridge contractor, is building a camp on his work between the Virginia State line and the second crossing of the Watauga river. He is now at work on the bridge over the Watauga in this county. He has had years of experience in such work and has made an enviable reputation as a bridge builder.

It is no uncommon sight to see gangs of 30 or 40 men pass through Johnson City almost every day, going somewhere out on the line of the Three C's to work. There need not be an idle man in the whole country for miles around Johnson City, if he wants to work. A gang passed through the city Monday, each man carrying a ¼ sack of flour, a frying pan, blanket and broadaxe. They were going out into the Cash hollow to get ties for N.G. Scott & Co.

There are now more than 2,000 men at work on the 16 miles in Washington county, and about one-half of the distance is graded. The contractors have been putting in good time, while the weather would permit, and, if the present favorable weather lasts a few weeks longer, the grading will be finished before Christmas. Some of the contractors who have heavy work are putting in extra time. W. McD. Burgin is running a force at night on his works. With the force organized as it is in the county, there is no doubt about the road being completed, ready for track laying, by the first of January, 1890. With anything like favorable weather, Messrs. N.G. Scott & Co. will have no trouble to get the track down in the proper time. With their machine, which will be here soon, they can lay 2 miles of track a day, if the road-bed is in good condition. The trestles and bridges are being built, and, from present indications, nothing will delay the work in this county.

24 October 1889. Johnson City Comet.

“Elizabethton Deserted. Mountaineer.) Quite a number of the boys have gone down to work on the Three C's.”

7 November 1889. Johnson City Comet.

“Three C's Notes. Messrs. N.G. Scott & Co. have commenced laying track and have nearly a mile laid. They began at the junction of the Three C's and E.T.V. & G. and are working north. The portion they are now laying parallels the E.T.V. & G. road for more than a half mile and they are putting in a side track between the two main lines. The track now being laid is principally for the accommodation of the contractors, to unload supplies, &c., track laying proper will not be commenced before some time next month.

We are officially informed that the Augusta division will be let to contract shortly. Preparations are now being made.

N.G. Scott & Co., have the trestle near A.B. Bowman's nearly completed.”

14 November 1889. Johnson City Comet.

“The Truth – Told by a Bristol Editor After Spending a Day in Johnson City, the ‘Coming Hub’ of East Tennessee... The Cranberry Railroad gave Birth to Johnson City. The road extends thirty miles to the steel ores in North Carolina. Although it costs but thirty or forty cents per ton to mine the ores, the company has fixed a minimum price \$2.50 per ton. As a consequence, none of

the ores are shipped now, it being impossible for the consumer to meet competition and pay the freight on unreduced ores. The Cranberry company will build furnaces by the time the 3-C can furnish them with coke, and reduce their mine output to pig.

The big steam tannery accelerated the growth of the infant town; and numerous wood working establishments and other factories aided very much too. I was told that all of the factories were so pressed to fill their orders that they had to work at night.

The town's recent boom has come from the almost complete realization of long-cherished hopes in getting the 3-C road. The road is under contract for a hundred miles to its connection at Rutherfordton, N.C. Gen. Wilder says that the grading through Washington county, eighteen miles, will be completed this month. From Johnson City to Kingsport, in Sullivan county, the work goes bravely on, both day and night. Track laying was commenced this week, and will be prosecuted with vigor. Gen. Wilder says the 3-C will reach the coal fields before the S.A. & O. gets there..."

21 November 1889. Johnson City Comet.

"The Three C's. – Freight Depot to be Built at Once – The Line to be Changed. – Asheville, N.C, to be Touched by the 'Open Sesame.' Officer's car No. 13 of the Charleston, Cincinnati and Chicago Rail road was side-tracked in Johnson City Sunday night. It contained General Manager R.A. Johnson, Maj. Jones, Superintendent of the Southern Division, Dr. Black, of Blacksburg, S.C, Judge Albin and the General Manager's Private Secretary Frick.

Col. Johnson is quartered at Vice-President Gen. J.T. Wilder's, while in the city.

Col. Johnson was visible to the Comet a few moments yesterday morning and although suffering with a cold contracted in South Carolina he was so gratified with the progress of the work in this section that he talked cheerfully and pleasantly about the intentions of the company.

It has been decided to commence work at once on a freight depot to be located near the junction of the Three C's and E.T.V. & G. roads. The building will be of brick and will be 30x150 feet with platforms on both sides. The building to be built now is intended for temporary use and is really only a wing or section of their freight depot proper. Gen. Wilder will put a force at work on the foundation just as soon as the weather will permit – perhaps tomorrow. This will be good news to the contractors as they can have their goods unloaded there and distributed with more promptness on account of the poor facilities for handling freight at the E.T.V. & G. depot.

S.W. Wilson, engineer of the syndicate of New York and Philadelphia capitalists who purchased the bonds of the road, went over the line with Chief Engineer Molesworth Monday and left Tuesday night for New York to make a report of the work to his people. He was astonished at the amount of grading that had been done and was especially pleased with the manner of work. He is an expert railroad engineer and says there are some points on the line so rough as to almost deter an engineer from attempting to get a road through, but the line is first-class in every respect and is a remarkably fine piece of engineering. He complimented Chief Engineer A.N. Molesworth very highly on the line. He found 24½ miles of graduation ready for the ties and says by the 12th of December all the work in Washington county except bridges will be finished. There was no use, he said, in sending him out here as the work was progressing rapidly and perfectly satisfactorily.

Col. Johnson says he expects to complete the line from Minneapolis to Johnson City and from Johnson City to Erwin as fast as possible. Contractors will be started to work all through Unicoi county at once. Some have already selected their work.

McDonald, Shea & Co. have the contract from Erwin to Minneapolis and are pushing the graduation with characteristic rapidity and are doing excellent work as the report of Engineer Wilson will testify. Col. P.P. Dickinson, one of the oldest and most prominent railroad builders in

the South, has the contract from Rutherfordton to the top of the Blue Ridge mountain. He has the entire distance from Rutherfordton to Marion covered with hands and will have the work between these points completed in six months.

The entire amount of steel rails necessary to lay the track from Erwin to Minneapolis has been purchased and is ready for shipment. The first train load is expected anytime..."

28 November 1889. Johnson City Comet.

"New Buildings. Ground was broken yesterday for the Three C's depot. P.C. Hoss is the contractor and will push the work rapidly to completion."

12 December 1889. Johnson City Comet.

"Three C's Notes. Steel rails are arriving almost daily and N.G. Scott & Co. will commence laying track by the first of the year.

The brick work on the depot has reached the first floor, and has been delayed a few days, waiting on timbers.

Goodman & Foster have finished their work in the city and moved their force out to Capt. S.D. Dunavant's work on Sinking Creek.

W. McD. Burgin is winding up his contract and has moved most of his force and outfit to Marion, N.C., where he has a contract between there and Rutherfordton.

Bridge work over the Watauga river is progressing as rapidly as the weather will permit. Unlooked-for freshets have already washed away a part of one caisson, which delayed the work several days.

J.C. Sullivan says he is getting along nicely with his work and will finish all in Washington county in the required time. The other contractors who have work in the county are about closing out. If the weather remains good the extension of time will not be needed."

26 December 1889. Johnson City Comet.

"Three C. Notes. N.G. Scott & Co., who have the contract for laying the track, have distributed the ties along the line in Washington county preparatory to laying track. Mr. Harrington told the Comet that when the grading was finished they could lay the track in 8 or 10 days.

The weather during the early part of December has been very favorable for railroad building and the contractors have been taking advantage of it and working all the force possible. At several places night shifts have been put on and it is evident every effort is being made to finish according to contract and it will not be their fault if it is not done."

1889. Manual of the Railroads of the United States for 1889.

Table 48. "Charleston, Cincinnati and Chicago Railroad Company

Lines of Road Projected:		
Charleston and Ohio River Division: Charleston, S.C., to Ashland, Ky.	620.0	
Augusta Division: Blacksburg, S.C., to Augusta, Ga.	140.0	
Charlotte Division: Lancaster, S.C., to Charlotte, N.C.	40.0	
Total length of projected lines		800.0 miles.

Completed December 31, 1888		
Camden, S.C., to Rutherfordton, N.C.	147.5	
Richardson, Ky., to White House, Ky.	8.5	
Total length of lines completed, December 31, 1888		156.0 miles.
Sidings, not reported. Gauge, 4 ft. 8½ in. Rail (steel), 60½ lbs.		

[Note that C.C. & C. had not completed any track in Tennessee at the time of this report.]

History. – This company was organized and chartered September 30, 1886, and succeeded to all the rights of the Charleston, Cincinnati and Chicago RR. Co. of South Carolina, the Rutherfordton Railway Construction Co. of North Carolina, and the Rutherford, Marion and Tennessee Ry. Co. of South Carolina. Work begun September 19, 1885. It is expected that the road will be completed to Marion, 318 miles from Charleston, by July next, thereby forming a connection with the Western North Carolina RR. and all points reached by it...

Work will also be begun at Johnson City, and construction will be pushed northward toward the Ohio River, and southward toward Marion, and from Marion northward, in order that the whole line of road may be completed during the year 1890...

The line is being built under contract by the Massachusetts and Southern Construction Co., of Boston, Mass., A.B. Harris, 45 Broadway, New York, N.Y., President. In February, 1889, the Massachusetts and Southern Construction Co. contracted with McDonald, Shea & Co., Knoxville, Tenn., to construct the road from Marion to the town of Minneapolis, Va., its junction with the Clinch Valley Extension of the Norfolk and Western RR., and from that point to the coking coal, making a total distance from Charleston of 575 miles, which is expected to be completed during the year 1889, making a direct communication with the coal to the sea-coast...

Directors (elected October 11, 1888). – Frank Coxe, Wharton Barker, William F. Weld, Philadelphia, Pa.; Chester W. Chapin, P.P. Dickinson, New York, N.Y.; James A. Rumrill, Springfield, Mass.; William D. Bishop, Bridgeport, Conn.; Amos Barnes, James T. Furber, Boston, Mass.; John T. Wilder, Johnson City, Tenn.; W.L. Roddey, Rock Hill, S.C.; Peter J. Sinclair, Marion, N.C.

Frank Coxe, President – Philadelphia, Pa.

John T. Wilder, Vice-President – Johnson City, Tenn.

Sec. and Treas. – Henry K. Baker – Springfield, Mass.

General Manager – R.A. Johnson...New York, N.Y.

Principal Office and Address – 45 Broadway, New York, N.Y.

Corporate Office – Rock Hill, S.C.”²¹² See Appendix B for the complete text of this item.

2 January 1890. Johnson City Comet.

“Engine No. 31, the first Three C’s locomotive ever sent here arrived last week, and was fired up yesterday and run down to the Three C’s track. John Bonan, an engineer formerly employed by the E.T.V. & G. R.R. Co., is in charge of it, and James Watson is firing. A number of flat cars have been ordered and as soon as they arrive, N.G. Scott & Co., will begin to lay track. The rails are all here.”

The author did not locate any additional information on Engine No. 31.

9 January 1890. Johnson City Comet.

“Three C Notes... J.P. Griffith will finish his work to-day.

McDonald, Shea & Co., are finishing G.R. Howell’s contract.

A number of flat cars arrived yesterday for the Three C’s.

Contractor Harrington laid one mile of track last Saturday and says he will lay four more next week.

The crossing has been put in at the junction of the Three C’s and E.T., V. & G. roads. It is a peculiar arrangement and includes five switches. It is so arranged that trains of either road can be switched on to the other at the crossing going either way, and one switch has to be changed before a train on either road can cross, which will necessitate the stopping of all trains at the junction to wait for a signal to cross.

The first tank of the Three C’s road on the Northern Division has been erected near the Barnes’ place on Brush Creek. It consists of four poles with a platform on top. On the platform is a pump and when the tank is being filled the portly form of John Bonan, the engineer, and Rabbit-foot Watson, the stoker, may be seen working at the pump handle. It only requires about six hours to fill the tank of the engine. Bonan and Watson are developing a muscle that would make John L. Sullivan envious.”

9 January 1890. Johnson City Comet.

“Special Notice. We don’t want this looked upon as a mere advertisement, but strictly business. Parties wanting galvanized iron cornice, roofing, guttering, or any kind of job work will save money and a great deal of annoyance from bad work, by contracting with the Johnson City Tin and Stove Co., to do their work. They employ better workmen and use better material than any house in the city.

As proof of our merit, we have under contract the Three C’s depot and over 100 squares of bridging on the E. Tenn. & Western N.C.R.R., and various other jobs...”

16 January 1890. Johnson City Comet.

“The Erwin Unakean, favors the proposition of the Charleston, Cincinnati and Chicago Railroad Company, asking for an extension of two years time from the 20th day of August 1890 in which to complete that portion of the road from Erwin to North Carolina line, and agreeing to complete the road from Johnson City to Erwin by the 31st day of June next, and says it is one of the most important propositions ever submitted to the voters of Unicoi county.

By voting the extension Unicoi county has every thing to gain and can lose nothing. It will be all the better for Erwin and the county if the road should stop at Erwin a while. There is not much doubt that the voters of Unicoi county will see that it is to their interest to vote for the extension, and thereby save paying interest on the bonds for 18 months or two years. In the meantime they will be getting the benefits from the road in operation from Erwin to Johnson City, and through to the coal fields.”

23 January 1890. Johnson City Comet.

“In Carnegie Addition. The grading around the freight depot is about completed.”

23 January 1890. Johnson City Comet.

“Nearing Completion. The Three C’s railroad is a great deal nearer completion in Washington county than a great many people are aware of. Every foot of grading is done north of Johnson City and the track is laid for more than five miles, and will be laid to the river in another week. The bridge over the Watauga is well under way and a mile or more of road in Sullivan county is ready for track, which will be laid as soon as the bridge is completed. South of Johnson City Capt. Dunavent will finish the grading in about ten days and by that time Harrington & Co., will have their trestle finished near Hunt’s, and will finish laying the track to the Carter county line. As fast as the track is laid it is being surfaced, full tied and put in good condition and unless something unforeseen should happen, the road will be finished by the 23rd of February and the extension of time will not be needed. The company went to work in good faith and have worked night and day to complete in the time specified and is now about to succeed, but accidents may occur to delay the work or the weather may turn bad and make it impossible to finish, and to save trouble in the future the company has asked for an extension of 60 days time in which to finish. While they may not need the time there is a possibility that they will, and under the circumstances it should be granted.

Johnson City owes the best years of its life, as it were, to the Three C’s. It has grown and prospered on the prospect of getting the road for three or four years and the growth has not really commenced. It has sprung from an insignificant village into one of the most thriving towns in the Central South and has the brightest future. Capitalists have doubled their capital and working people have been constantly employed. The cause is well known. The Three C’s road will develop our natural resources and make it possible for all kinds of iron industries and wood-working establishments to thrive here. These are some of the things the Three C’s has done for Johnson City and all it asks is 60 days more in which to finish the road. Will you grant it, or, will you like the boy, kill the goose that layed the golden egg? The election is Thursday next, and you are asked to vote on the same proposition you accepted last year.”

23 January 1890. Johnson City Comet.

“A large force of hands will soon be put to work on the Three C’s railroad in this [Unicoi] county. The first mile under contract has been graded and all the line from the Carter county line to Erwin will be graded at an early date provided the weather will permit. – Erwin Unakean.”

6 February 1890. Johnson City Comet.

“Three C’s Notes. Two side tracks have been put in between the main line and the freight depot here...

The crossing at the junction of the E.T. & W.N.C. and C., C. & C. roads has been put in. It is similar to the one put in at the E.T.V. & G. crossing.

Capt. Dunnavant is finishing his work south of Johnson City in the county and will get through virtually this week.

More than sixty miles of graduation has been paid for between Johnson City and the coal fields, and all the work has been done in four months.

Tracklaying is not being pushed as rapidly as it was but Mr. Harrington, the contractor, says he will be over in Sullivan county by the 30th of April.

The bridge at Watauga that was delayed so on account of the impossibility to get timber, is being framed in Johnson City and will be hauled out on the road in a week or so.

Encouraging reports come from the work in Sullivan county and all along the line. The weather has been favorable and all the contractors are getting in good estimates...”

13 February 1890. Johnson City Comet.

“Three C Notes. Track has been laid north as far as Hale’s mill, about 6 miles and is now being full tied, surfaced and lined up.

The Three C’s railroad company has placed an order with the American Bridge and Iron Company for several turn-tables.

The contract for the work in Unicoi county has been let to Wm. Kennefick, one of the largest contractors now of the Three C’s. He is at work in Virginia where he has a contract for 30 miles but will put a force in Unicoi at once.”

20 February 1890. Johnson City Comet.

“The Three C’s. Capt. S.D. Dunavant finished his contract last Saturday and every foot of the road in Washington county is now graded. Track is laid six miles north and is now doubled-tied and lined up. Tracklayers are now working south and are putting in the full number of ties and surfacing as they go.

J.G. Chaffin, has about completed his trestle, and as soon as it is finished and the bridge completed over the Watauga river, track will be laid into Sullivan county. A passenger train will be put on next month and will run from here to the end of the completed track. It will run as far as Kingsport in May, it is thought.

P.F. Cogan was in the city this week. He has a five mile contract from Estillville to the Tennessee State line, and says he will finish it this month, except one cut, but will knock it out soon in March. He is here to put a force in Unicoi county. His foreman, Mr. McEwen, has gone to Unicoi to put up shanties. Mr. Kennefic and he have the entire contract for Unicoi and will finish it in time for the formal opening on the Fourth of July. Seventy-five miles of the road is now ready for the track between here and the coal fields and the reports from the engineers state that the graduation will be completed before the middle of April. Track will be rapidly laid and on the Fourth of July Johnson City will celebrate the opening of 100 miles of the Three C’s road, as the greatest event in her history.

The freight depot is nearing completion, and plans are being drawn for a union passenger station, general offices, car and machine shops, all to be located and built at once in Johnson City.”

1 March 1890. Engineering News and American Railway Journal.

“Railway Buildings. – Charleston, Cincinnati & Chicago R.R. Co. is having plans prepared for the construction of machine shops at Johnson City, Tenn., at an estimated cost of \$500,000. Gen. J.T. Wilder, of Johnson City, can give particulars.”²¹³

6 March 1890. Johnson City Comet.

“Master Mechanic Maher has arrived in Johnson City and has his office in the Three C’s building. He is here to superintend the construction of the shops.”

13 March 1890. Johnson City Comet.

“Three C Notes... Wm. Kennefick is putting a large force in Unicoi county and will complete the line to Erwin and have cars running by the 4th of July. P.F. Cogan has several miles of the work and is moving things rapidly.

V.E. Steen has the contract for trestles in Carter and Unicoi counties and already has large forces at work in both counties...

The Johnson City Tin & Stove Co., have just completed the job of roofing the 3 C's depot. The best job ever done in Johnson City.”

27 March 1890. Johnson City Comet.

“A New Town. A company of eastern capitalists have decided to build a town on the river about three miles outside the corporate limits of Johnson City. They have purchased several thousand acres of land and are laying out a town on broad gauge plans. A railroad will be built from Watauga Point on the E.T. and W.N.C. road crossing the E.T., V. and G. at their town and connecting with the Three C's at Austin's Springs. Work was commenced on the road last Monday morning at 1 o'clock in order to keep other companies from getting in ahead and holding the route. All the land was bought was formerly known as Carter's...”

3 April 1890. Johnson City Comet.

“Three C's Railroad. Erwin Unakean... We are informed that Messrs. Cogan and Hunt are both pushing the work on their contracts briskly, and their force of hands are increasing every day. Mr. Hunt has a considerable force of hands at work on the old Wm. Love farm. We also learn that work on the road on Buffalo is being pushed through at a rapid rate.

If the voters of the county will only act wisely and vote to give the company the extension asked for on the 12th of April, from Erwin to the N.C. line, we will have the road completed and cars running to Erwin in July next.

10 April 1890. Johnson City Comet.

“Through the County. The Three C's road is now completed through Washington county and has been turned over to the company by contractor Harrington. The road is full tied and surfaced and is in good condition. Chief Engineer Molesworth informs the Comet that a train will be put on about the First of May and run regularly. By the First of July trains will be running to the coal fields, or rather Minneapolis, ninety miles north, also to Erwin, eighteen miles south of Johnson City. The graduation between these points is about completed and track will be rapidly laid. One hundred and eight miles of the road will be formerly opened July 4th and appropriately celebrated in Johnson City. The line between Rutherfordton and Marion, N.C., will also be opened and operated by the 4th of July and there will then only be about seventy-two miles between Marion, N.C., and Erwin, Tenn., to build, to connect Johnson City with the coast at Charleston.”

24 April 1890. Johnson City Comet.

“A second engine has arrived for the Three C's road.”

24 April 1890. Johnson City Comet.

“The Carnegie land company which last fall laid out its addition to Johnson City, continues the good work of offering sites free to manufacturing establishments, and will aid all such to a good start. The company has 800 acres of eligible land specially held for that purpose, selected and purchased with that view, and which it refuses to sell for other uses because of its determination to foster manufactories of all kinds by giving them sites without consideration. The company is building a \$100,000 hotel on a block fronting the site of the new Union Passenger depot, the latter to be on First avenue in Carnegie addition, and in close proximity to what will ultimately be the center of business. The new railroad and company offices will be in a splendid four-story brick building just opposite the hotel. The machine and car shops of the railroad will be further down on the line, between the passenger depot and the furnaces. Mr. Harris, president of the Charleston, Cincinnati & Chicago road, will soon begin the erection of a very large hotel on the block he purchased in Carnegie. It will be one of the most celebrated hotels in the South.”

24 April 1890. Johnson City Comet.

“Started with a Rush. A list of Factories, Hotels, Business and Dwelling Houses Completed and Under Construction Since January 1st, 1890. – Nearly a Million dollars Already Spent in Building...”

C.C. & C. R.R. Offices. [\$] 250,000

C.C. & C. Passenger station [\$] 30,000

C.C. & C. Freight Station [\$] 15,000...”

24 April 1890. Johnson City Comet.

“The proposition of the Three C’s railroad company, asking further time to construct their line of road from Erwin to the N.C. line was defeated last Saturday by a small vote. We think our people made a very great mistake in rejecting this proposition; but a three-fourth vote was required, and as long as one little, hump-backed, bow-legged, knock-kneed, lantern-jawed snotty-nosed crank can step up to the ballot box and kill three votes, so long will elections be a failure. – Erwin Unakean.”

24 April 1890. Johnson City Comet.

“Three C’s in Unicoi. Erwin Unakean. Since the proposition for extension failed to carry, the contractors have been pushing the work of grading with renewed energy; they say they will have the road completed and in operation through the county by the 20th of August. A part of Mr. Hunt’s hands have commenced work at the near end of the grading on the Love property, and are now working toward Erwin. They say they will have a much larger force of hands in a few days. They are also making preparations to commence work on the Rock Spring next week. We also learn that Mr. Cogan is getting along rapidly in his rock cut above Unaka Spring, in the gorge. Everything about the work appears to be moving on lively, and if we are not mistaken on the signs of the times, the doubting Thomas’ who believe that it is impossible for the Company to complete the road through the county by the 20th of August, will soon be convinced of their error.”

1 May 1890. Johnson City Comet.

“Formerly Accepted. – The Three C’s Finished in Washington County and Johnson City Pays its Money Like an Honest Man. – The First Passenger Train Carries the City Council on a Tour of Inspection. – Johnson City People Unite Today in Crowning the Three C’s ‘Queen of the May.’ – An Eventful Day in Our History. – Yesterday morning at precisely five minutes past eleven o’clock General Superintendent W.P. Harris shouted merrily ‘all aboard’ and the first passenger train on the Three C’s left the Main Street crossing to make its first trip through Washington county. The train was composed of only one coach and engine ‘Unicoi,’ No. 32, that just arrived last week. Master Mechanic W.E. Maher occupied the right hand side of the engine and with him at the throttle the passengers were at ease.

The object of the trip was to have the city council see that the railroad company had complied with its contract, accept the road and deliver the bonds. As it was impracticable to have a general excursion the company issued no invitations...”

8 May 1890. Johnson City Comet.

“Three C’s in Unicoi. Erwin Unakean. We have but little railroad news to publish this week. Work in this county, south of Erwin, moves steadily on, and grading from the state line to Erwin will soon be completed. We are informed that work is commencing in earnest on Buffalo, a large force hands being already at work and new ones arrive almost daily. Nothing has yet been done between the head of the Big Lane and Erwin, but there is comparatively nothing to do between the points above named, and we presume the company is in no hurry about letting it to contract. The company have done nobly so far and we are assured they will continue to do so.”

15 May 1890. Johnson City Comet.

“Three C News... Erwin Unakean. From the indications now, there can be no doubt but that the road will be completed to the State line by the first of August.

Several of the leaders who opposed extension were in town Monday, and expressed themselves as follows: ‘I do believe we had better give them the extension for they are going to put the road to the state line in the prescribed time.’ Like the old adage: ‘Too late to pray after the devil comes.’

Messrs. Kennefick & Hunter were at the Phillips House Sunday night. Mr. Kennefick said, while in town, that himself and sub-contractors would be able to put the road to the river in the next sixty days, if the weather remains favorable. Mr. Kennefick will build a new camp at Hiram Boothe’s this week, and will have from 50 to 100 men on the rock cut on the Crow farm next week.

Mr. Frank Hunt has moved his outfit from Clinch river to Mr. Blevins’ (near the big lane) and will push that part of the work to completion at once.

Mr. R.J. Hunt, brother of Frank Hunt, has charge of the camp at ‘Lovetown,’ and will complete the work from Erwin to the river at once.

Messrs. Hunt & Griffitt have moved the most of their force to their contract below the N.C. line; they have the next largest force to Mr. Kennefick, on the line, and they get there, you bet.

Mr. Cogan has a large force of men at work in the gorge.

Mr. Kennefick, while in Erwin Monday, said that the whole line would be covered with hands in the next ten days. Let the good work go on.”

5 June 1890. Johnson City Comet.

“Work on the Three C’s in this county is progressing rapidly at present. A good force of hands are busy pushing the work up the Nolachucky river and through the ‘Gorge’, and the grading is about completed from this place to Unaka Springs. Work is also being pushed at a good rate in the Greasy Cove and on Buffalo. All-together the grading is about one-half done in the county. – Erwin Unakean.”

26 June 1890. Johnson City Comet.

“Charleston, Cincinnati & Chicago. – Track laid since January 1, 1890, Johnson City, Tenn., north, 14 miles. Graded end of track to Minneapolis, Va., 76 miles, Rutherfordton to Marion, 25 miles. Under construction, Johnson City to North Carolina State line, 20 miles. Surveyed, Tennessee State line to Marion, 70 miles; Minneapolis to Ohio river, 160 miles; Johnson City to State line, 50 miles. Chief Engineer, A.N. Molesworth, Johnson City, Tenn.”

11 July 1890. Railroad Gazette.

“Charleston, Cincinnati & Chicago. – An arrangement has been made with the contractors by which they are to put a much larger force on the uncompleted portion of grading and tracklaying between Johnson City, Tenn., and Minneapolis, Va., about 70 miles. The grading has been completed for most of this distance, and the tracklaying will probably be completed in the latter part of August. William Kenefeck has the contract from Johnson City to the North Carolina State line.”²¹⁴

17 July 1890. Johnson City Comet.

“The union passenger depot is to be of limestone, and masons are now cutting the stone. The foundation is laid.”

7 August 1890. Johnson City Comet.

“Three C’s Railroad. Erwin Unakean. The construction of the Three C’s railroad in this county still progresses with rapidity. The grading is well nigh finished and some of the trestles are built. The trestle at Unaka Springs is nearly finished across the Nolochucky river and footman are crossing on it to-day. Two miles or more of track are now laid in this county on Buffalo, and tracklaying is in progress at the rate of one mile per day. Crossties are being delivered by hundreds on the right-of-way every day. Several night forces are at work which will hasten the road on; and if there are no further causes for delay, ‘Unicoi,’ the new engine, will whistle in Erwin in ten days.”

7 August 1890. Johnson City Comet.

“The track has been laid on the Three C’s to a point within six miles of Erwin, the county seat of Unico county. Two and three miles per day are being laid. The weather permitting, the track will be finished to the North Carolina line by the 20th inst. – J.C. Cor. Journal.”

14 August 1890. Johnson City Comet.

“The Three C’s train has reached Erwin, and by next Wednesday the road will be completed according to contract, and Unicoi county will have to pay the \$25,000 likewise.”

21 August 1890. Johnson City Comet.

“The Three C’s. Yesterday afternoon a party of men armed with rifles borrowed from the military company left for Erwin to assist in laying the last rail on the Three C’s. It was understood that a party of Rock Creek, N.C, men had sworn that the last rail should not be laid, and that the bonds should not be delivered to the road. They were armed and determined; hence the action on the part of the company to protect their rights. Up to the time of going to press we have not heard that there was any trouble.

It is expected that Unicoi county will accept the road to-day and order the bonds delivered. The bonds, for \$25,000, are now in the Jonesboro Banking and Trust Co.’s vault.

This morning at ten o’clock the officials of this road and a number of invited guests left the city for a trip on the road through Carter and Unicoi counties. The purpose of the trip was the inspection of the road by the Chief Engineer of the same and the Chairman of the County Court of Unicoi county. As this is our day of publication, we cannot publish an account of the trip and its result, but will do so next week...

Among those from Johnson City were the following:

Messrs. A.N. Molesworth, Chief engineer, W.P. Harris, Superintendent, Judge Albin, Attorney, H.H. Carr, judge Kirkpatrick, W.B. Whiteside, Dr. E.S. Miller, Jr., G. Morris, W.G. Jenkins, Mayor Jobe, The Comet and other gentlemen.

We would not forget to state that Conductor Pearson and Engineers Hamilton and Simcox were also along, and last but not least, a bountiful supply of refreshments.”

21 August 1890. Johnson City Comet.

“Three C’s is Welcomed in Erwin. Unakean, August 14. Yesterday was a big day for Erwin. The arrival of the first locomotive engine ever in town attracted a large crowd to Capt. Nelson McLaughlin’s orchard, where an excellent view could be had. They all seemed to welcome the new-comer heartily and could realize that Erwin has a railroad at last. No extraordinary demonstrations were had, but when the road is completed through the county we shall have a day of rejoicing over the Three C’s railroad.

Strictly speaking, Erwin is not the town it was two days ago. Instead of being hemmed in on all sides without any communication by rail, it has now a permanent outlet to the world. The gates of our wealth are thrown open, and the people of the North, South, East and West are invited to come and share with us. We are entering upon a new era and shall soon be a new people.”

21 August 1890. Johnson City Comet.

“New Time Table. In another column will be found the time table of the Three C’s railroad. Trains will run over the completed portion of the road from Johnson City to the North Carolina line, twenty-three miles, regularly. The schedule took effect yesterday, and the first train left here on time, 3:30 in the afternoon. The road is now open for business, and a long felt want is supplied in the country through which it passes. May the traffic increase until a second track becomes necessary for the transaction of business, is the best we have in the shop just now.”

THE

Charleston, Cincinnati and Chicago Rail-
Road Company

IS NOW OPEN FOR BUSINESS,

—BOTH—

FREIGHT AND PASSENGER, BE-
TWEEN JOHNSON CITY
AND ERWIN.

TRAINS RUN DAILY, EXCEPT SUNDAY, AC-
CORDING TO THE TIME CARD
PUBLISHED BELOW.

Charleston, Cincinnati and Chicago
Railroad Company--Central
Division.

TIME TABLE NO. 1, Taking Effect
Wednesday, Aug. 20, 3:30 p. m.

STATIONS.	No. 1.
Leave Johnson City	3:30 p. m.
" Marbleton	4:15 p. m.
" Limonite	4:50 p. m.
" Erwin	5:25 p. m.
" Unaka Springs	5:50 p. m.
Arrive at State Line	6:15 p. m.

RETURNING.

STATIONS.	No. 2.
Leave State Line	5:30 a. m.
" Unaka Springs	5:55 a. m.
" Erwin	6:20 a. m.
" Limonite	6:55 a. m.
" Marbleton	7:50 a. m.
Arrive at Johnson City	8:15 a. m.

Trains Nos. 1 and 2 will run Daily,
except Sunday.

The above Time Table is for the
government of the employees only,
the Company reserving the right to
vary it at pleasure.

W. P. HARRIS,
Sup't. and Traffic Manager.

Figure 44. Johnson City Comet. 21 August 1890.

21 August 1890. Johnson City Comet.

“J.C. Sullivan has put a force of hands back to work on section No. 20, on the Three C’s in Sullivan county. Contractor Peterson has also put a force on his work in Sullivan. All of the work between Johnson City and Minneapolis will be completed now as fast as possible, and before Christmas it is hoped trains will be running through to Minneapolis, ninety miles. It is also understood that work will commence on the gap between Marion, N.C. and the State line in a short time. The road must be completed through Mitchell county, N.C., by March, 1892, and work will have to be commenced shortly in order to get it comple[te]d in the specified time.”

28 August 1890. Johnson City Comet.

“J.P. Griffith, a contractor on the Three C’s, who was shot last Thursday by one of his foremen, Mike Golden, is improving. He is a member of the firm of Hunt & Griffith and was in Johnson City Sunday.

The Three C’s is arranging to put in a telegraph line from Johnson City to Erwin. Poles are being gotten out and distributed along the line, and it will not be long before it will be in operation. Offices will be put in at all the stations along the line.”

13 September 1890. Engineering News and American Railway Journal.

“Charleston, Cincinnati, & Chicago. – Tracklaying is being rapidly pushed on the section between Johnson City, Tenn., and Minneapolis, Va. Between Johnson City and the North Carolina state line the road is completed.”²¹⁵

25 October 1890. The Railway Review.

“The Beals steam brake is to be applied to 11 engines of the Charleston, Cincinnati & Chicago road.”²¹⁶

4 December 1890. Johnson City Comet.

“Reorganization Effected. Wm. Kenefick, a contractor on the Three C’s, telegraphed to a gentleman in this city this morning that the Three C’s Company had been reorganized, and that the money to finish the line had been secured, but it would take a week or ten days to arrange all preliminaries.”

4 December 1890. Johnson City Comet.

“Mr. Eugene Horton arrived from New York a day or two ago and brought the plans for the new depot on the Charleston, Cincinnati & Chicago Railroad to be built at the junction of Main street with said road. – The designs show that the new depot will be a very handsome building. – Work will be commenced within thirty days and pushed rapidly to completion. It will be a great convenience to the people. Mr. Horton is authority for the statement that his firm, Horton, Gildersleeve & Co. will at once open Main and Market street through their land.”

11 December 1890. Johnson City Comet.

“The Three C’s. Chancellor J.P. Smith has appointed Sam Tate, Jr., of Memphis, receiver for the Charleston, Cincinnati and Chicago Railroad Company and the Massachusetts and Southern Construction Company at the request of McDonald, Shea & Co., and Wm. Kenefick, Contractors. The appointment was made Monday of this week. The receiver will of course only have charge of the property in Tennessee...”

11 December 1890. Johnson City Comet.

“Chief Engineer Molesworth returned from New York yesterday. He says the trouble with the Three C’s will be cleared away in a short time, and the work will be finished. The company has already spent over \$3,000,000, and it is not at all likely that it will lose that amount for a few hundred thousand dollars due the contractors. There is too much at stake to stop now.”

26 December 1890. “Carnegie Realty Co. et al. v. Carolina, C. & O. Ry. Co. et al...”

“This bill was filed by the Carnegie Realty Company, the Carnegie Development Company, Wilberforce Sully, and the Carnegie Land Company, the last three complainants suing for the use of the Carnegie Realty Company against the Carolina, Clinchfield & Ohio Railway, the Southern & Western Railway Company, and the Southern Railway Company.

The bill averred that the Carnegie Land Company, engaged in the business indicated by its name, in 1890 conveyed to the East Tennessee, Virginia & Georgia Railway Company and the Charleston, Cincinnati & Chicago Railroad Company a certain town lot, in the Carnegie addition to Johnson City, which addition consisted of about 1,600 acres and was owned by the Carnegie Land Company.

It is further averred that about 1891 or 1892 the two railroad companies, just named, grantees of the said deed, became insolvent [Sam. Tate, Jr., was appointed receiver of the portion of the C.C. & C. in Tennessee, on 26 December 1890], and their properties were sold through court proceedings [scheduled for 21 August 1895]; that the properties of the Charleston, Cincinnati & Chicago Railroad Company were acquired by the Southern & Western Railroad Company, and later by the Carolina, Clinchfield & Ohio Railway; and that the properties of the East Tennessee, Virginia & Georgia Railway Company were acquired by the Southern Railway Company.

The deed from the Carnegie Land Company to the two railroad companies is exhibited with the bill, and it appears, from an inspection of the deed, as well as from allegations of the bill, that the town lot in question was conveyed to the railroad companies for \$1 in cash, and with the obligation upon the railroad companies ‘to erect, maintain, and operate thereon a passenger depot subject to the following provisions and conditions.’ Then follow in the deed certain specifications as to the use of the depot properties, the upkeep thereof and the character of improvements to be placed on the lot.

It is averred in the bill that the two railroad companies began the construction of a depot on the lot, and had completed the structure up to the roof line, with chimneys erected, at the time they became insolvent. It appears from the bill that there was no actual use of this lot for depot purposes by the grantees under the deed, or their successors. It is alleged that the Southern & Western Railway Company occupied said lot for a time and placed thereon a turntable, and that the Carolina, Clinchfield & Ohio Railway Company has erected upon the lot a house, or office, for its employes, and that the Southern & Western Railway Company and the Carolina, Clinchfield & Ohio Railway Company both used the partially constructed depot building for a machine shop, putting a roof on the building for that purpose.

It does not appear from the bill that the Southern Railway Company has ever made any claim to this lot or used the premises for any purpose whatever since it acquired the properties of the East Tennessee, Virginia & Georgia Railway Company, under foreclosure proceedings...”²¹⁷

31 December 1890. Manual of the Railroads of the United States for 1891.²¹⁸

“Charleston, Cincinnati And Chicago Railroad Co...

Completed December 31, 1890:

Camden, S.C., to Marion, N.C. – 172.34 miles.

Richardson, Ky., to Whitehouse, Ky. – 8.50 miles.

Johnson City, Tenn., to North Carolina State Line – 30.0 miles.

Johnson City North – 20.0 miles.

Total length of lines completed, December 31, 1890 – 231.0 miles.

Sidings, 10.64 miles. Gauge, 4 ft. 8½ in. Rail (steel), 60½ lbs...”

1 January 1891. Johnson City Comet.

“The announcement that a receiver had been appointed for the Charleston, Cincinnati & Chicago Railroad, and for the Massachusetts and Southern Construction Co., which was building the same, was not unexpected. The suspension of an old and wealthy Philadelphia banking-house which had been the financial agent of both companies brought to an untimely end a negotiation, nearly completed, that would have placed in bank funds enough for the payment of all arrearages and for the vigorous prosecution of this important enterprise. The disaster was unavoidable, but it is not irretrievable. The two companies have large assets in rights of way, in valuable mineral and timber lands, in sections of completed road bed, and in locomotives, cars, shops and depots. Moreover large manufacturing plants already completed and others under construction, because of the expected early connection of this road with the immense coal fields of Western Virginia and the magnetic ores of the Cranberry district of north Carolina, insure a large paying business as soon as the intermediate links shall be completed. These certainties will induce capitalists to furnish whatever money may be needed for the prosecution of this enterprise to a finish. The original promoters and investors may suffer loss, which would be a grievous pity, for they undertook a grand work and carried it forward with unremitting energy until stopped by unexpect[ed] suspension of their financial agents; but if. Unfortunately, they cannot arrange to resume their work, others will begin where they left off, and will complete the road to the Ohio river. There may be some delay before this comes about, but at worst it cannot be a great while, and in the meantime the whole financial world will have recovered from the shock caused by the failure of the Barings, and the worlds business will go on with greater volume because of its temporary check. – Baltimore Manufacturers Record.”²¹⁹

7 May 1891. Johnson City Comet.

“The Three C’s. – The Plan of Reorganization Agreed To. – The Line to be Completed in Tennessee at Once. – ‘The bondholders of the Charleston, Cincinnati & Chicago railroad Company have agreed to a plan of reorganization, under which they give up their first lien on the property for \$6,567,000, and remit a new mortgage for \$2,500,000, to be placed ahead of their bonds. This plan may rescue the road from its difficulties. The proceeds of this new loan will be used in completing a number of gaps in the line of road in Tennessee that will make the whole line of value. It is understood that the new loan can be floated. The accomplishment of this plan would

enhance the value of the present bonds, and would in consequence assist in straightening out the affairs of the suspended Barker Bros., and of the Investment Company, of Philadelphia.’

The above article is taken from the daily Indicator, a financial journal published on Broadway, New York City, and devoted exclusively to railroad, mining and commercial interests. The clipping is self explanatory and substantiates what was said in the Comet last Sunday in regard to the matter...”

30 June 1891. Manual of the Railroads of the United States for 1892.²²⁰

“Charleston, Cincinnati And Chicago Railroad Co...

Lines of Road Completed December 31, 1891:

History. – Chartered September 30, 1886, and road as above in operation in June 1891. The project is for a railroad from Charleston, S.C., to Ashland, Ky., 620 miles... a total of 800 miles... On December 10, 1890, a receiver was appointed in North Carolina and South Carolina; on December 26, 1890, in Tennessee [Sam. Tate, Jr., Johnson City]; and on February 20, 1891, in Kentucky. In February, 1891, D.H. Chamberlain, receiver of the South Carolina RR., was appointed receiver of the road in North Carolina and South Carolina...

Rolling Stock, June 30, 1891. – Locomotives, 11. Cars – passenger, 10; baggage, etc., 2; freight (box, 167; platform, 62), 229 – total cars, 241. This equipment was purchased by the receivers and paid for with receiver’s certificates, dated July 1, 1891.

Operations for year ending June 30, 1891. – Train mileage and traffic operations – no records kept. Earnings – passenger, \$46,922.69; freight, \$102,528.85; mail, \$7,562.36; express, \$1,409.91; other, \$5,604.19 – Total, \$164,028.10. Expenses – transportation, \$35,098.36; motive power, \$47,778.73; maintenance of cars, \$10,902.96; roadway buildings, \$72,356.65; general, \$37,011.75 – Total, \$203,143.45...

The company has under consideration a plan of reorganization with or without foreclosure...

Directors (elected March 10, 1891). – Frank Coxe, Philadelphia, Pa.; Franklin Edwards, North Wilbraham, Mass.; G.W. Bentley, C.J. Gray, W.G. McIntyre, New York, N.Y.; John Carlisle, Cincinnati, O.; H.K. Baker, Edward S. Brewer, D.N. Coats, Springfield, Mass.; John Brearly, Chicopee, Mass.; John J. Collier, C.M. Ostrander, Brooklyn, N.Y.; John T. Wilder, Johnson City, Tenn.

Franklin Coxe, President	Philadelphia, Pa.
G.W. Bentley, Vice-President	New York, N.Y.
Comptroller – John J. Collier, New York, N.Y.	Rec. in N.C. & S.C. – D.H. Chamberlain, Charleston, S.C.
Sec. and Treas. – H.K. Baker, Springfield, Mass.	Receiver in Tenn. – Sam. Tate, Jr., Johnson City, Tenn.
Asst. Treasurer – W.G. McIntyre, New York, N.Y.	Receiver in Ky. – A.B. Harris, New York, N.Y.
Principal Office and Address	45 Broadway, New York, N.Y.
Transfer Office	Boston, Mass.

Note: The locomotives and rolling stock were distributed across the three unconnected divisions, in Kentucky, Tennessee, and the Carolinas. The number of locomotives and rolling stock available on the “Tennessee Division” appears to have been rather limited.

CHARLESTON, CINCINNATI & CHICAGO.

In Effect Sunday, May 4, 1891.

NORTH			
STATIONS	No. 2	No. 4	
Ar Ranges		3:50 p.m	
Lv Harrisburg		3:40 "	
" Austin Springs		3:25 "	
" Carnegie		3:00 "	
Ar Carnegie	12:00 a.m		
Lv Main St. Joh'n	11:55 "		
" Okalona	11:40 "		
" Fagans	11:25 "		
" Marbleton	11:20 "		
" Unicoi	10:55 "		
" Erwin	10.30 "		
" Unaka Springs	"		
SOUTH.			
STATIONS.	No. 1	No. 3	
Lv Ranges		4:00 p.m	
" Harrisburg		4:10 "	
" Austin Springs		4:25 "	
Ar Carnegie		4:50 "	
Lv Carnegie	7:05 a.m		
" Main st. Joh'n .	7:30 "		
" Okalona	7:55 "		
" Fagans	8:05 "		
" Marbleton	8:10 "		
" Unicoi	9:00 "		
Ar Erwin	9:30 "		
" Unaka Springs			

Figure 45. Johnson City Comet. 25 June 1891. Ran until at least 17 November 1892.
Note the stops at both Carnegie and Johnson City ("Main st. Joh'n").

19 February 1892. The Railroad Gazette.

“Charleston, Cincinnati & Chicago. – The decision of the Superior Court of Tennessee for the sale of the road, noted last week, will tend to completely disarrange the plans of the bondholders who have been trying to reorganize the railroad. In the litigation that followed the bankruptcy of the company several receivers were appointed in South Carolina by the Federal Court, and in Kentucky another receiver was appointed. In Tennessee a receiver for the road was named by the State Court. As a result, the Superior Court of that State has ordered all roads built by the company in Tennessee and all franchises belonging to it must be sold. It is likely an appeal will be taken from this decision. The decision was in the suit brought by the contractors, and is in their favor.”²²¹

5 May 1892. Johnson City Comet.

“Chancery Sale of the Charleston, Cincinnati and Chicago Railroad in Tennessee. – In the Chancery Court at Jonesboro, Washington county, Tennessee. – McDonald, Shea & Co., et als, vs. Charleston, Cincinnati and Chicago Railroad Co. et als. – Pursuant to the decree of said court at its January term, 1892, in the above cause, I will, on Thursday, the 12th Day of May, 1892, in front of Jobe’s opera house, in the town of Johnson City, Tenn., sell at public outcry to the highest and best bidder, and in bar of the equity of redemption, the property mentioned and described in the pleadings and said decree and ordered to be sold, to wit:

The entire railroad of the Charleston, Cincinnati and Chicago Railroad Company, lying in the State of Tennessee, including the roadbed, track, bridges, culverts, depots, station houses, water tanks, rights of way, depot grounds, turnouts, side tracks, switches, water privileges and casements and also the franchises, privileges and corporate rights of said railroad company as an entirety.

To satisfy the costs of said cause, the recoveries in favor of complainants McDonald, Shea & Co. for \$476,207.45, and complainant Wm. Kenefick for \$105,321.16, with interest from February 6th, 1892, and all other claims established and decreed, in said cause against said railroad company, which is in process of liquidation therein as an insolvent corporation.

Terms of Sale.

Twenty-five thousand dollars (\$25,000) of the purchase price to be paid in cash, (any receiver’s certificates issued by the receiver under the order of said court, to be accepted as cash), and the balance to be paid in four equal payment, at six, twelve, eighteen and twenty-four months from the date of sale.

The notes of the purchaser will be required for said deferred payments, bearing interest from date, and secured by good personal security or by the deposit of collateral security to be approved by the complainants and the Charleston, Cincinnati and Chicago Railroad Company or solicitors, and a lien will be retained upon the property sold, as further security. W.F. Young, Clerk and Master. This April 9th, 1892.”

20 May 1892. The Railroad Gazette.

“Charleston, Cincinnati & Chicago in Tennessee. – Samuel Hunt, of Cincinnati, has bought for \$200,000 in the interest of the Reorganization Committee that portion of the road built in East Tennessee. The sale was made by order of the Court of Chancery to satisfy claims of McDonald,

Shea & Co., and W. Kenefick, contractors, amounting to \$381,000. The portion of the road in North Carolina, South Carolina and Tennessee is still in the Receiver's hands."²²²

30 June 1892. Johnson City Comet.

"Clerk and Master's Sale of Personal Property of C.C. & C. R. Co. – In the Chancery Court at Jonesboro, Washington County, Tenn. – McDonald, Shea & Co. et al vs Charleston, Cincinnati & Chicago Railroad Co. et al. – Pursuant to the decree of said court at its June, special, term, 1892, in the above cause, I will, on Thursday the 7th day of July, 1892, sell at public outcry to the highest bidder, in front of Jobe's opera house in Johnson City, the property mentioned and described in the pleadings and said decree, and ordered to be sold, to wit : All of the personal property of the said Charleston, Cincinnati and Chicago railroad Co. in the hands of the Receiver in said cause, consisting of the rolling stock, engines, cars &c.; about 5 miles of steel rails, and all other loose property on the line of said road, or in the hands of said receiver, including the maps, notes, profiles and furniture &c., belonging to the engineer's and other offices of the said railroad company, &c. &c. Said property will be sold in separate lots or parcels.

Terms of Sale.

Said property will be sold for cash in hand. This June 25, 1892. W.F. Young, C. & M."

25 August 1892. Johnson City Comet.

"The Bonds Issued and the Tools Here for the Early Completion of the Charleston. Cincinnati & Chicago Railway. – Fifteen Thousand will be Expended on Each Mile. – The prospects for work to commence on the Three C's railway never were brighter than at the present. Bonds to the amount of \$15,000 per mile have been issued for the completion of the road, and are now in the hands of Philadelphia parties to be placed, and work will be commenced soon as this is done, which will not be later than September first, and perhaps before then, as yesterday one car load of scrapers and plows arrived here, and are now on the sidetrack for Wm. Kenefick, one of the principal contractors. They came from the works in the State of New York, and several more cars loaded with carts, tents, mules, wagons etc. are expected arrive today to get in readiness for work at once."

22 September 1892. Johnson City Comet.

"Clerk and Master's Sale of Personal Property. – In the Chancery Court at Jonesboro, Washington County, Tenn. – McDonald, Shea & Co. et al. vs. Charleston, Cincinnati & Chicago Railroad Co. et al. – Pursuant to the decree of said court at its July term, 1892, in the above cause, I will on Thursday the 29th day of September, 1892, sell at public outcry to the highest bidder, in front of Jobe's Opera House, in Johnson City, Tennessee, the property mentioned and described in the pleadings and said decree, and ordered to be sold, to-wit: The engineering outfit of said Railroad Co., in the chief engineer's office at Johnson City, Tennessee, consisting of 1 Remington typewriter and desk combined, 1 iron safe, 1 single writing desk with pigeon holes, 1 press for holding profiles, 3 common chairs, 3 office chairs, 1 mirror, 2 buckets, 5 transits and tripod, 6 levels and tripods, 6 level rods, 12 pickets, 1 iron picket 1 100 foot steel tape, 12 chains, 1 blue print frame with bath, 1 press for dark chamber, 1 stationery box containing books of finished

work between Rutherfordton and Camden, 1 box containing miscellaneous plans and profiles of same, 1 box containing approximating estimates alignment and office notes and 4 drawing tables with trestles and one common table.

Terms of Sale.

Said property will be sold for cash in hand. This September 17. 1892. W.F. Young, C & M.”

3 November 1892. Johnson City Comet.

“Railroad Notes. – What is being Done All Along the Line... There is no late news regarding the completion of the Charleston, Cincinnati & Chicago railway, except as given in the Comet a few days ago, the reorganization has been completed in Philadelphia and work will begin in the spring.”

The author did not locate the article in the Johnson City Comet pertaining to the reorganization in Philadelphia.

19 January 1893. Johnson City Comet.

“Railroad Shops. – Those for the 3-C’s will yet be Built Here. – The Deed for the Site Asked For – The Long Hoped for to be Realized. – The Carnegie Land Company is in receipt of a letter from the 3 C’s Railway officials, asking them for the site for their shops which they promise to locate here, and for which Johnson City voted \$25,000 in bonds. Many people have given up all hope of the shops ever being located here, while others are still confident that they are sure to be. We want them and are justly entitled to them. The Comet feels sure that when the time comes for the erection of shops they will be built in Johnson City, the prospects for their early building are now exceedingly flattering, as much so as (or the Charleston, Cincinnati and Chicago Railway, and the Carnegie Iron Furnace, everything is being gotten in the proper shape and early in the coming spring things will begin to move here in a way that has never been seen before, and this will yet be the hub of the universe.”

20 January 1893. The Railroad Gazette.

“Charleston, Cincinnati & Chicago. – Work suspended for last two years, but it is expected to resume operations by March next. A.N. Molesworth, Johnson City, Tenn., Superintendent.”²²³

8 June 1893. Johnson City Comet.

“Two Big Assignments. – The Carnegie Land and Carnegie Iron Company – Make a Temporary Assignment on Account of the Inability to Collect Debts or Float Bonds. – ‘The Carnegie Land Company and the Carnegie Iron Company have made an assignment!’

That announcement created more excitement in Johnson City yesterday than anything that has happened for years... Gen. Wilder, the president and resident manager of the companies, has worked hard and faithfully for the up-building of this city and section, and has done all in his

power to avoid making the assignment, but lack of financial support from interested parties has rendered it absolutely necessary to protect the property and the creditors...

The following statement was telegraphed to several papers by The Comet Sunday:

‘The Carnegie Land Company and the Carnegie Iron Company, on account of the financial situation and inability to make collections, and lack of assistance from non-resident stockholders, have been forced into a temporary assignment to protect the property and their creditors. The financial pressure has been so great that the companies could not be sold or the amount due them could not be collected.

Gen. Wilder who owns one-third of the stock of both companies, has stood manfully by them and backed them up to the extent of \$55,000, while all other stockholders, holding two-thirds of the stock, have only furnished about \$18,000. Arrangements had been made to sell the bonds on the furnace in New York, but the recent stringency of the money market was so great that the party could not spare the means to take up the bonds at present. They, however, assure the companies that they will take the bonds later and pay off all the indebtedness.

The property consists of a large, new blast furnace, hotel, lands and town lots, and secured notes of \$600,000, while the total indebtedness does not exceed \$125,000, much of which is not due. There is not of this amount more than \$25,000 due to people in the South. The rest is owed to parties North and East.

The delay in completing the Three C’s railroad was the prime cause of all the trouble, and no doubt the whole thing will be straightened out and everybody paid within this year. J.W. Cure has been made assignee, and is thoroughly familiar with all the affairs of the companies...”²²⁴

5.2. Ohio River & Charleston Railway, 1893 – 1902

19 October 1893. Johnson City Comet.

“The 3-C’s Wiped Out. – The Ohio River and Charleston is the New Name. – Charles E. Hillier Says the Road will be Built. – The Charleston, Cincinnati & Chicago has been formally transferred to the bondholders’ committee, as anticipated in the Manufacturers’ Record of October 6, and they are now in possession of all the lines. The first step of the reorganizers will be to incorporate companies in South and North Carolina, Tennessee, Virginia and Kentucky. These are to constitute one consolidated company to be known as the Ohio River & Charleston. Samuel Hunt is to be the temporary general manager and A. Fripp, superintendent. Temporary headquarters will be established at Blacksburg, S.C...”

March 1893 – June 1894. Poor’s Manual of the Railroads of the United States.

“Ohio River and Charleston Ry. of Ky. Richardson to Whitehouse, Ky., 8.5 miles... The Charleston, Cincinnati and Chicago RR. Co. was reorganized in Nov. 1894, as the Ohio River and Charleston Ry... That portion of the line in Ky. was sold under foreclosure in March, 1893, and incorporated under the above title on June 25, 1894...”²²⁵

2 November 1893. Johnson City Comet.

“The O.R. & C. – The following statement of the plan of reorganization of the Three C’s is taken from a New York paper. Portions of the plan have been published in the Comet, but it is the first time it has been given entire to our readers and from such a reliable source:

‘The bondholders of the Charleston, Cincinnati and Chicago Railroad have approved the amended plan of reorganization. Receivers for the property were appointed late in 1890 and early in 1891. In May, 1892, the Tennessee portion of the road was sold to satisfy contractors’ liens and was bought in for \$200,000 by the Reorganization Committee. The rest of the property was sold in May, 1893, and was also bought in by the bondholders. The amended plan provides for the completion of the line between Richardson, Ky., and Sumpter, S.C., a distance of 510 miles, of which over two hundred miles are in operation, and a large amount of grading already done...’

15 February 1894. Johnson City Comet.

“About the Three C’s. – Gen. Wilder Talks Regarding the Road and the Carnegie Furnace. Gen. J.T. Wilder spent Friday in Bristol. Speaking of the recent visit of Samuel Hunt, president of the 3 C’s road, to Johnson City, he said that it was the plan of Mr. Hunt to have the road completed to the coal fields provided he finds the business outlook sufficient to justify the venture.

There is no likelihood, Gen. Wilder thinks, of the road being built through the mountains on either side soon, but he rather expects the construction to be completed to the Virginia coal fields...”

19 July 1894. Johnson City Comet.

“The latest step in the work of reorganizing the Charleston, Cincinnati and Chicago Railroad, or, as it is better known hereabouts, the Three C’s, is the chartering of the Ohio River and Charleston Railroad Company, in South Carolina, with a capital of \$15,000,000, and with John Goldthwait, of Boston; Samuel Hunt, of Cincinnati; R.M. Morse and C.E. Hillier, of Boston, and J.J. Collier, of Philadelphia, as directors. The Ohio and Charleston is the new name adopted by the Three C’s under the reorganization, and a similar charter will be taken out in each of the States through which the line runs. The plan is to form a company under the laws of Tennessee which will absorb the lines of the Three C’s in the other States, and this company, it is understood, will be known as the Ohio and Charleston Railroad Company. Philadelphia Times, July 10, 1894.”

2 August 1894. Johnson City Comet.

“The reorganization of the Three Cs is being pushed forward. The organization of the Ohio River & Charleston Railroad Co., which will take the place under the reorganization of the Charleston Cincinnati & Chicago, with Samuel Hunt, of Cincinnati, as president, is being perfected, The debts on the different divisions of the property are being cleared up, and it is said that as soon as practicable that part of the road between Johnson City, Tenn., and Moccasin Gap, connecting with the South Atlantic & Ohio at Big Stone Gap, Va., will be finished. – Manufacturers’ Record.”

“Ohio River and Charleston Railway”

TENNESSEE DIVISION.						
Pass'ger.	Pass'ger.	Mis	October 1, 1894.		Mis	Pass'ger.
†2 20 P.M.	†7 30 A.M.	0	LEAVE]	[ARRIVE	16	10 00 A.M.
2 25 "	7 35 "	1	Johnson City ⁹ }	Carnegie Depot	15	9 55 "
2 40 "	7 50 "	4	.....	Market Street.	12	9 45 "
2 45 "	7 55 "	5	.....	Okalona.....	11	9 40 "
2 50 "	8 00 "	6	.....	Fagans.....	10	9 35 "
2 55 "	8 05 "	8	.....	Marbleton.....	8	9 30 "
3 00 "	8 15 "	11	.....	Rose Hill.....	5	9 25 "
3 20 "	8 30 "	16	.....	Unicoi.....	0	9 00 "
3 25 P.M.	8 45 A.M.	20	.....	Erwin.....	†8 50 A.M.	†3 45 P.M.
			ARRIVE]	[LEAVE		

† daily, except Sunday...

⁹ With Southern Ry., and East Tennessee & Western North Carolina R.R.”

Figure 46. Travelers' Official Railway Guide for the United States, Canada and Mexico...,
1 October 1894.²²⁶

22 November 1894. Johnson City Comet.

“The Three C’s Dead. – From the Ashes There has Arisen, Phoenix-Like – The Ohio River & Charleston Railway Company. – The Philadelphia Times of the 15th inst. contained the following information concerning the road that will interest the people of this section:

So far as its name is concerned the Charleston, Cincinnati and Chicago Railroad Company (better known as the “Three C’s,”) no longer exists, for at a meeting of those who purchased the road and thus saved it from the sheriff’s hands, the consolidation agreement was executed and the Ohio River and Charleston Railway was organized as its successor. The meeting at which this action was taken was held in Boston on Tuesday...”

14 February 1895. Johnson City Comet.

“A meeting of the stockholders of the Ohio River and Charleston Railway Co. has been called for the 10th of April, at Johnson City, to decide whether or not the road will be bonded at \$15,000 per mile for the purpose of completing the line. It is safe to presume such action will be authorized, but we will have to await developments.”

9 May 1895. Johnson City Comet.

“Stockholders’ Meeting. – Ohio River & Charleston Railway Co. Elect Directors and Officers. A meeting of the stockholders of the Ohio River & Charleston Railway Co. was held in this city yesterday in the office of Kirkpatrick, Williams & Bowman. The majority of the stock was represented by J.J. Collier, of Philadelphia. Samuel Hunt was re-elected president, and Job H. Jackson, vice president. The directors chosen are: John Goldthwait, John C. Haynes, A.F. Freeman and Robert M. Morse, Boston; Samuel Hunt, Cincinnati; S.J. Kirkpatrick, Jonesboro, Tenn.; Job H. Jackson, Wilmington, Del.; G.H. Earle, Jr., A.B. Loeb, S.A. Stern, and J.J. Collier, Philadelphia.”

Table 49. "Ohio River & Charleston Railway..."

General Officers.	
Samuel Hunt, President – Cincinnati, O.	H.J. Bruce, Auditor – Blacksburg, S.C.
Jno. J. Collier, Secretary and Treasurer – Philadelphia, Pa.	S.B. Lumpkin, Gen. Frt. and Passenger Agt. [- Blacksburg, S.C.]
E.F. Gray, Traffic Manager, Cincinnati, O.	P.H. Freeman, Car Acct. [- Blacksburg, S.C.]
A. Tripp, Supt. Carolinas Division – Blacksburg, S.C.	W.J. Wilcox, M.M. [- Blacksburg, S.C.]
	G.C. Nutting, Supervisor Bridges and Bldgs. [- Blacksburg, S.C.]
A.N. Molesworth, Chief Eng. And Supt. Tenn. Div. – Johnson City, Tenn.	J.A. Maxwell, Supervisor of Roadway [- Blacksburg, S.C.]

General Offices, Blacksburg, S.C.

Miles of road operated, 212.24. Gauge, 4 ft. 9 in. Locomotives, 11.

[Table 50.] Freight Equipment.

The cars are marked 'Charleston, Cincinnati & Chicago R.R.' or 'C.C. & C. R.R.,' (box cars have a large 3-C monogram on side of cars), and numbered and classified as follows:"

Kind of Cars.	Numbers.	Dimensions.			Capacity.	No.
		Length	Width	Height		
Box, even Nos.	1002				40000	1
[Box, even Nos.]	1006 to 1040	34.2	8.8	7.6	40000	18
[Box, even Nos.]	1044 to 1072	34.2	8.8	7.6	40000	15
[Box, even Nos.]	1078 to 1110	34.2	8.8	7.6	40000	17
[Box, even Nos.]	1114 to 1180	34.2	8.8	7.6	40000	34
[Box, even Nos.]	1186 to 1198	34.2	8.8	7.6	40000	7
[Box, even Nos.]	1202 to 1318	34.2	8.8	7.6	40000	59
[Box, even Nos.]	1324 to 1330	34.2	8.8	7.6	40000	4
[Box, even Nos.]	1334	34.2	8.8	7.6	40000	1
[Box, even Nos.]	1336 to 1340	34.2	8.8	7.6	60000	3
[Box, even Nos.]	1344 to 1412	34.2	8.8	7.6	60000	35
[Box, even Nos.]	1416 to 1434	34.2	8.8	7.6	60000	10
Flat, odd Nos.	101 to 117	34	8.8		40000	9
[Flat, odd Nos.]	121 to 127	34	8.8		40000	4
[Flat, odd Nos.]	131 to 139	34	8.8		40000	5
[Flat, odd Nos.]	143 to 223	34	8.8		40000	41
[Flat, odd Nos.]	225 to 263	34	8.8		60000	20
Gondola, odd Nos.	3001 to 3029	34	8.8	3	60000	20
Total						303

Passenger Equipment.

Coaches, First-Class – 6 to 11	6		Mail and Express – 4, 15	2
[Coaches] Second – Class – 5, 12	2		Passenger and Baggage – 1, 14	2
[Coaches] Accommodation – 2, 3	2		Offices' Private – 13	1
Total				15

Send reports of mileage and movements of cars to P.H. Freeman, Car Accountant, Blacksburg, S.C.

For balances, remit to Jno. J. Collier, Treasurer, Philadelphia, Pa.; pay by voucher.

Note, this appears to include the equipment of both the “Carolinas Division” and the “Tennessee Division”. Of note, Poor’s Manual of the Railroads of the United States for 1900 included: “Ohio River and Charleston Ry... Johnson City, Tenn., [to] Caney River, N.C., Locomotives, 3. Cars – passenger, 1; combination, 9; [freight] (box, 20; flat, 10; coal, 3), 33; service, 6 – total cars, 41...”²²⁸

4 July 1895. Johnson City Comet.

“Will Complete the Road. According to previous announcement the stockholders of the Ohio River & Charleston Railway Company met in this city last Thursday, the 27th ult., to consider an issue of \$3,000,000 in securities to complete the road. Mr. J.J. Collier, of Philadelphia, represented a large number of Boston and Philadelphia stockholders by proxy. Three-fourths of the stock was represented and voted unanimously to issue bonds necessary for the immediate completion of the line...

For these and other reasons the Comet has no hesitancy in saying that there is every indication that work will be commenced upon the line between this point and a connection with the Cincinnati, Portsmouth & Virginia at an early day.”

11 July 1895. Johnson City Comet.

“Within Ninety Days Work to be Commenced on the Ohio River and Charleston Railway. – Contract has been Let for grading Ten Miles of the Line from Unaka Springs to a Point Beyond Wilder’s Forge. Good Times Ahead. – Work will be commenced on the Ohio River & Charleston Railroad, formerly the ‘Three C’s,’ between Johnson City and Marion, N.C., within the next ninety days, possible inside of thirty days.

The contract has been let for grading ten miles of the line south from Unaka Springs to a point beyond Wilder’s Forge. The ten miles through the Chucky River gorge includes some of the heaviest work on the line, and it is to be completed as quickly as possible, the main object being to reach the rich deposits of Cranberry magnetic iron ore near Wilder’s Forge.

Considerable heavy work was done beyond Unaka Springs before the collapse of the Three C’s Company and will be finished up...”

22 August 1895. Johnson City Comet.

“Clerk and Master’s Sale of Personal Property. – In the Chancery Court at Jonesboro, Washington County, Tenn. – McDonald, Shea & Co. et al vs. Charleston, Cincinnati & Chicago Railroad Company et al. – Pursuant to the decrees of said court at its July term, 1895, and the former orders and decrees in said cause thereby revived, I will on Tuesday, September 3, 1895, at the ‘Carnegie depot’ in Johnson City, Tennessee, sell at public outcry, to the highest bidder, the property mentioned and described in the pleadings and said decrees and ordered to be sold, to wit: All the personal property belonging to the Charleston, Cincinnati & Chicago Railroad Company and the Massachusetts and Southern Construction Company embraced in said decrees of sale, and remaining unsold, situated in and around said depot and lying loose along the line of said Charleston, Cincinnati & Chicago Railroad in Tennessee, and consisting of office furniture, trucks, scales, lumber, bridge timbers, cross ties, telegraph poles, bridge and track bolts, spikes, chords, angle plates, boilers and pumps, tile pipe, turn tables, switch stands and frogs, etc., etc., including

steel rails and rolling stock, except the angle plates and steel rails in controversy in the petition of G.W. Chamberlain, receiver, and as to which the order of sale has been suspended by the court. Terms of Sale – Said property will be sold for cash in hand. This August 21, 1895. W.F. Young, C. & M.”

19 December 1895. Johnson City Comet.

“The schedule on the Ohio River & Charleston was changed on the 10th inst. No. 1 leaves here at 7 o’clock and returns at 9:45. The afternoon train is abandoned.”

4 March 1897. Johnson City Comet.

“Serious Accident. A rather serious accident occurred last Thursday on the O.R. & C. Railway near the stave factory. Some men engaged in painting the stack of the factory had a support rope stretched across the track and the section hands returning in the dark did not see it and were knocked off the car. Henry Blevins had his skull fractured and another man by the name Nave was injured about the hips. Neither are as yet able to be out.”

17 June 1898. Poor’s Manual of the Railroads of the United States.

“Ohio River and Charleston R.R. – On June 17, 1898, a decree of foreclosure of the company’s property in North and South Carolina, on a bill filed by the Finance Co., of Philadelphia, Pa. The railroad company has defaulted on its bond interest due June 1, 1898.”²²⁹

1 August 1898. Poor’s Manual of the Railroads of the United States.

“The line from Marion, N.C., to Camden, S.C., 174.27 miles, and a branch from Blacksburg to Gaffney, N.C., 10.5 miles, were sold Aug. 1, 1898, under foreclosure of a mortgage dated June 1, 1897, and were reorganized as the South Carolina and Georgia Extension Ry...”²³⁰

18 August 1898. Johnson City Comet.

“Ohio River and Charleston. – Railroad Rumors that Mean Much for Johnson City. – ‘The Ohio River, & Charleston Railroad has passed into the hands of a syndicate which includes Samuel Hunt, of Cincinnati, O., Simon A. Stern and J.J. Collier of Philadelphia, Pa. The transfer was made at the sale of the property ordered by the court to take place at Marion, N.C., The transfer practically places the line in the hands of the company which has been operating it, Mr. Hunt being president of the company. In connection with the sale comes the report that the portions of the road in Tennessee and North Carolina are to be connected, as already outlined in the Manufacturers’ Record...’”

6 October 1899. The Railway Age.

“Ohio River & Charleston. – The contract for building an extension from Unaka Springs, Tenn., to the mouth of the Caney River, North Carolina, has been awarded to J.C. Lipscomb of

Gaffney, Tenn., and grading has been commenced. A.N. Molesworth, C.E., Johnson City, Tenn.”²³¹

29 December 1899. The Railway Age.

“Ohio River & Charleston. – A.N. Molesworth, C.E., Johnson City, Tenn., informs us that work is in progress on the extension from Unaka Springs, Tenn., to the mouth of Caney River in North Carolina, 15 miles, of which 4 miles are in Tennessee and 11 miles in North Carolina. The grading is almost half completed, and it is expected to begin tracklaying in January.”²³²

31 May 1900. Poor’s Manual of the Railroads of the United States.

“Ohio River and Charleston Ry. (Supplementary Statement). Johnson City, Tenn., [to] Caney River, N.C., 33.5 miles. Incorporated Nov. 13, 1894, under the laws of the State of Tennessee, being [formed?] by consolidation of the Ohio and Charleston Ry. Co. of South Carolina, the Ohio River and Charleston [Ry.?] Co. of North Carolina, the Ohio River and Charleston Ry. Co. of Tennessee, and the Ohio and Charleston Ry. Co. of Virginia, with a capital stock of \$15,000,000, the company’s railroad consisting of 171.40 miles of completed and operated railway, located in the States of North Carolina and South Carolina, which portion in the two States was sold, and by reason of which the capital stock was subsequently reduced from \$15,000,000 to [?]000. Grading is completed from Johnson City, Tenn., to Gate City, Va., 40 miles, and from Churchport, [?] to Minneapolis, Va., 40 miles – a total of 80 miles. Locomotives, 3. Cars – passenger, 1; combination, 9; [freight] (box, 20; flat, 10; coal, 3), 33; service, 6 – total cars, 41.

Operations, year ending May 31, 1900. – Earnings (passenger, \$3,013.76; freight, \$15,093.61; other, \$1,404.90. Operating expenses and taxes, \$23,220.09. Deficit from operations, \$3,707.82...”²³³

31 May 1900. Johnson City Comet.

“Contract Let for Depot. The O.R. & C. railroad company has let the contract for building the freight and passenger station at Hunt Dale, N.C., the terminus of the extension now being constructed. J.T. Linville & Co., of this city, secured the contract and will have it ready for occupancy by June 2rd, next. The road is expected to be completed by that time. Mr. Linville is one of our most enterprising and reliable contractors and may be relied upon to complete this building in the specified time.”

7 1900. Johnson City Comet.

“J.D. Crowell is helping J.T. Linville & Co. build the O.R. & C. depot at Hunt Dale.”

12 July 1900. Johnson City Comet.

“Wood-Pulp Industry. General John T. Wilder, United States pension agent, states that the Chestoa Manufacturing plant of Johnson City is figuring on putting in a wood pulp plant in connection with their saw mill, some time before next fall. He stated that a large portion of the

lumber cut in that section of the country is unfit for saw mill purposes, but it could be used in the manufacture of wood pulp.

The sixteen-mile extension of the Ohio River and Charleston railway has been completed and the road will be open for operation Monday morning. It is built through the heart of the finest timber belt in the state, and already there are nearly a score of saw mills located on the extension and busily engaged in sawing timber.

General Wilder anticipates quite a boom in that section of the country as a result of the opening up to commerce of the fine timber country. – Journal and Tribune.”

The author has not found any more information on this enterprise.

OHIO RIVER & CHARLESTON RAILWAY.						
SAMUEL HUNT, President and General Manager, Cincinnati, O.			J. J. COLLIER, Sec'y & Treasurer, 310 Chestnut Street, Phila., Pa.			
ELIAS R. DICK, Vice-President, Philadelphia, Pa.			T. D. RHODES, Assistant to the President, Cincinnati, O.			
A. N. MOLESWORTH, Supt. Tenn. Div. & Chief Eng'r, Johnson City, Tenn.						
No. 1	Mls	August 15, 1900.		Mls	No. 2	
7:20 A M	0	lve..	Johnson City ¹ .arr.	34.0	3 10 P M	STANDARD—Central Time. ern Ry. + Daily, except Sunday. Connection—With South-
7:25 "	1.0	.....	Market Street.....	33.0	3 05 "
7:40 "	4.9	.....	Okolona.....	29.1	2 50 "
7:50 "	7.3	.....	Marbleton.....	26.7	2 40 "
7:55 "	8.4	.....	Taylors.....	25.6	2 35 "
8:05 "	10.6	.....	Unicol.....	23.4	2 25 "
8:15 "	13.7	.....	Fishery.....	20.3	2 15 "
8:20 "	16.2	.....	Erwin.....	17.8	2 05 "
8:40 "	19.5	.....	Chestoa.....	14.5	1 35 "
9:00 "	20.0	.....	Unaka Springs.....	14.0	1 25 "
9:30 "	24.0	.....	Lost Cove.....	10.0	1 05 P M
10:00 "	29.0	.....	Poplar.....	5.0	12 35 Noon
10:20 "	32.0	.....	Peterson.....	2.0	12 20 "
10:30 A M	34.0	arr....	Huntdale...lve.	0	12 10 Noon	

Figure 47. The Official Guide of the Railways and Steam Navigation Lines of the United States, Porto Rico, Canada, Mexico and Cuba, 15 August 1900.²³⁴

30 August 1900. Johnson City Comet.

“Excursion to Hunt Dale. There will be an excursion to Hunt Dale over the O.R. & C. railroad on the 8th of September. The run will be made to Hunt Dale and back to Unaka Springs for dinner. The Odd Fellows of this city will picnic at Hunt Dale on this day. Rates will be announced later.”

7 March 1901. Johnson City Comet.

“A New Connection. In connection with a visit of A.N. Molesworth, general superintendent of the Ohio River and Charleston railway, the Knoxville Journal and Tribune prints the following:

‘Mr. Molesworth says that the country has gone forward so fast and the Southern has bought so much new track to be operated that it is almost impossible for it to get cars enough for its own use, and therefore makes it hard on the O. R. & C, which must look to the Southern for cars to get its freight away from Johnson City. He reports business good on his line.

Mr. Molesworth’s company is planning to build nine miles of road this spring and summer to connect the O.R. & C. with the Virginia and Southwestern. The road will be built from Carnegie, where the O.R. & C. enters Johnson City, to a point not far from Bluff City, where the Virginia and Southwestern crosses the Southern. The line would give Johnson City a competing line with the Southern from all points east and give the same railroad facilities at Bristol.”

7 March 1901. Johnson City Comet.

“Pressure for Cars. – O.R. & C. Road unable to Get Sufficient Rolling Stock. A.N. Molesworth, superintendent and chief engineer of the O.R. & C. Railway, is here from Johnson City, the headquarters of the road. He came to consult with Superintendent Ewing, of the Southern, in regard to cars for his road, which uses Southern cars for freight shipments altogether.

Mr. Molesworth was only partially successful. Superintendent Ewing told him that the Southern was hard up for cars itself and could use 200 more a day on this division and branches if it had them. However, he promised to furnish the O.R. & C. all the cars possible. These cars are needed to haul lumber out of Mitchell county, N.C., and lumber and minerals from Unicoi county, Tenn.

Mr. Molesworth says that his road, which formerly ran from Johnson City to Erwin, a distance of twenty miles, had now extended the line fifteen miles further into Mitchell county, N.C., where a number of mills are cutting lumber. ‘A new bridge of the Howe truss variety has been put across the Nolachucky river at Unaka Springs,’ said Mr. Molesworth, ‘which was formerly the end of the road, and Mr. A.V. Deaderick is now constructing a large new hotel at those famous springs...”

5.3. South and Western Railway Company, 1902 – 1908

2 January 1902. Johnson City Comet.

“New Railway Chartered. Secretary of State John W. Morton has granted a charter to the South and Western Railway company, with \$100,000 capital stock. The incorporators are S.J. Kirkpatrick, J.H. Bowman, S.C. Willaims. It is proposed to build a railroad from North Carolina – Tennessee line, where the Nolachucky river passes through Unicoi county to the Virginia – Tennessee line in Sullivan county, and beyond said points to any point in the states of Virginia, Kentucky, Ohio, North Carolina or South Carolina.

The gentlemen interested have nothing to say at this time for publication regarding the intentions of the new company.”

3 July 1902. Johnson City Comet.

“O.R.C. Sold. Last Thursday the O.R. & C. road was absorbed by the South & Western Railway Company, a company recently organized by Geo. L. Carter for this purpose. Mr. Carter has had an option on the O.R.C. and Va. & S.W. roads for some time and has extended the O.R. & C. several miles into North Carolina and has a force still at work building the line to Lincolnton, N.C. The option on the Va. & S.W. expired, but it is understood that the deal can still be closed upon the same terms.

It is not given out just upon what terms the exchange of stock was made, but the South & Western Railway Co. authorized the issuance of \$600,000 in bonds to complete the construction of the road to Lincolnton. There is a well founded rumor that the road will be completed north to the coal fields at an early day.”

31 July 1902. Johnson City Comet.

“The Three C’s Scheme. – Gen. Wilder Talks of the Advantages of this New railroad. – Knoxville Journal and Tribune. The Ohio River and Charleston, now known by the name of the South and Western railroad, which has been recently re-organized by Baltimore capitalists, was formerly known as the Three C’s and is one of the features of Johnson City, which people generally believe is certain to be of great value if finished.

Gen. John T. Wilder has been deeply inter[est]ed in this road. From what he says there is much to be gained by its early completion. The present owners are not the Seaboard Air Line people, but while this is the case it is more than probable that they have some understanding with the Seaboard Air Line people, as to future business and traffic arrangements for the S. & W. road will when completed give the S.A.L. a very large business and all of the coal that it may wish.

The road is now completed to a point in North Carolina just south of Bakersville called Sink Hole, fifty-one miles from Johnson City. Of this distance there are twelve miles of rails to be laid, though all of the other work is completed and ready for the rails. It is about 225 miles from Johnson City to Ashland, Ky., which is opposite Ironton, Ohio. Of this section 92 miles out of Johnson City are graded and with some work would be ready for the ties...”

25 September 1902. Johnson City Comet.

“Steel Rail Expected. It is expected that the steel rails for the extension of the South & Western beyond Green Mountain will begin to arrive in a few days. Twelve cars have been shipped. A very heavy rail will be laid, one that is particularly adapted to a mountainous section where heavy trains are to be run. The road is being built with a view of making it a through line from the Ohio river to the Atlantic coast.”

16 October 1902. Johnson City Comet.

“The railroads brought in hundreds on excursion trains, especially the South and Western and the E.T. & W.N.C. railroads. Our mountain people like big shows, and we are proud of having a town where such shows are given. We are always glad to welcome them into our midst.”

SOUTH & WESTERN RAILWAY CO.

GEO. L. CARTER, PRES'T.

PASSENGER TRAIN SCHEDULE NO. I.

DAILY EXCEPT SUNDAY

In Effect Sunday, November 1, 1902.

South Bound		STATIONS	North Bound		
No. 1	Mls		Mls	No. 2	
A. M.		Lve.	Arr.	P. M.	
7 20	0	Johnson City	t	64	3 20
7 25	1	Market Street		63	3 15
7 40	4 9	Okolona	f	59 1	3 00
7 50	7 3	Marbleton	f	56 7	2 50
7 55	8 4	Taylor's	f	55 6	2 45
8 05	10 6	Unicoi	t	53 4	2 35
8 15	13 7	Fishery	f	50 3	2 22
8 20	16 2	Erwin	t	47 8	2 12
8 28	18 2	Love's	f	45 8	2 07
8 40	19 5	Chestoa	t	44 5	2 00
8 50	20	Unaka Springs		44	1 55
8 55	20 2	Archer	f	43 8	1 50
9 07	24	Lost Cove	f	40	1 40
9 20	26 5	Eagle Cliff	f	37 5	1 30
9 40	29	Poplar		35	1 15
9 55	32	Peterson	f	32	12 55
10 20	34	Huntdale	t	30	12 40
10 30	37	Relief	f	27	12 30
10 45	41	Green Mount'n	t	23	12 15
11 15	47 4	Toecane	t	16 6	11 45
	56 3	Booneford		7 7	
	58 8	Phillips		5 2	
	62	Bailey		2	
	64	Spruce Pine		0	
		Arr.	Lve.		

—Flag Station. t—Telephone.

MEL H. WEILER, Trainmaster.

Figure 48. Johnson City Comet, 27 November 1902. This ran at least until 3 December 1903.

25 December 1902. Johnson City Comet.

“Work on the South & Western is being rushed. Trains are now running 70 miles out from Johnson City, and it is estimated that the road will be completed to Lincolnton within eighteen months... – Burnsville (N.C.) Eagle.’

31 December 1902. Poor’s Manual of the Railroads of the United States.

“South and Western Ry. – Johnson City, Tenn., to Sprucepine, N.C., 64 miles. Gauge, 4 ft. 8½ in. Rail (steel), 65 lbs. This company was chartered April 17, 1902, and acquired the Ohio River and Charleston Ry., 33.5 miles... which it has consolidated with other properties. It contemplates the completion and connection of its line from Virginia coal fields south and west. The extension from Caney River to Sprucepine, 30.5 miles, having been opened for traffic on July 1, 1903. Equipment, Dec. 31, 1902 – Locomotives, 2. Cars – passenger 2; freight (box, 15; flat, 14), 29 – total, 31...

Directors (elected May 19, 1902). – George L. Carter, T.F. Davis, J.N. Powell, C.B. Anderson, Bristol, Va.; T.P. Trigg, Abingdon, Va. Officers: George L. Davis, Pres., Bristol, Va.; T.P. Trigg, Vice-Pres., Abingdon, Va.; C.B. Anderson, Sec. & Treas., Bristol, Va. General Office, Bristol, Va.”²³⁵

22 January 1903. Johnson City Comet.

“Trains Run to Boonford. Effective January 26th all trains on the South & Western Railway will run from Johnson City to Boonford, N.C., fifty-six miles from this place and eight miles only from Spruce pine, to which point this road is about graded. Slowly, but surely, this road is pushing over the Blue Ridge on its way to the ocean.”

14 May 1903. Johnson City Comet.

“After Rolling Stock. M.H. Weiler and B.J. Sitton have gone to Cincinnati to purchase some rolling stock for the South & Western. Mr. Weiler is Master of Trains for the road and Mr. Sitton is accompanying him as an expert machinist. They will buy both engines and passenger cars and when they arrive a double daily passenger service will be inaugurated.”

28 May 1903. The Johnson City Comet.

“Under Construction. It will not be long until the clang of the motorman’s gong will make a merry echo in our streets. Foreman Alf Smalling began the active construction of the [trolley] track on Roan street on Wednesday morning with a large force. The track will be laid from the car shed at the power house out Roan to Main, thence to West Main and out this street to a point opposite the Soldiers’ Home and will then run through land recently purchased by the street railway company to the north entrance of the reservation. The ties are being cut at Will Gray’s mill in Carnegie and the rail is expected in a few days. With good weather and no unnecessary delay Johnson City will have a street car line in operation again within a few weeks. As soon as the line is constructed to the Home a line will probably be built down Watauga avenue, New street, Second avenue and North Main to the S & W passenger depot. This line, when constructed, ought to put Hotel Carnegie in operation and perhaps will.”

11 June 1903. Johnson City Comet.

“Engine Arrives. Engine No. 33, recently purchased in Cincinnati by the South & Western for passenger service, arrived Friday and will be put in service next week. A regular passenger train will be put on the road in a few days and operated for the convenience of the traveling public.”

Engine #33 appears to have been a 4-4-0, built by the Pennsylvania Railroad at Altoona c. 1871, with 18 x 24” cylinders and 62” drivers. It was “scrapped by the CC&O in 1916.”²³⁶

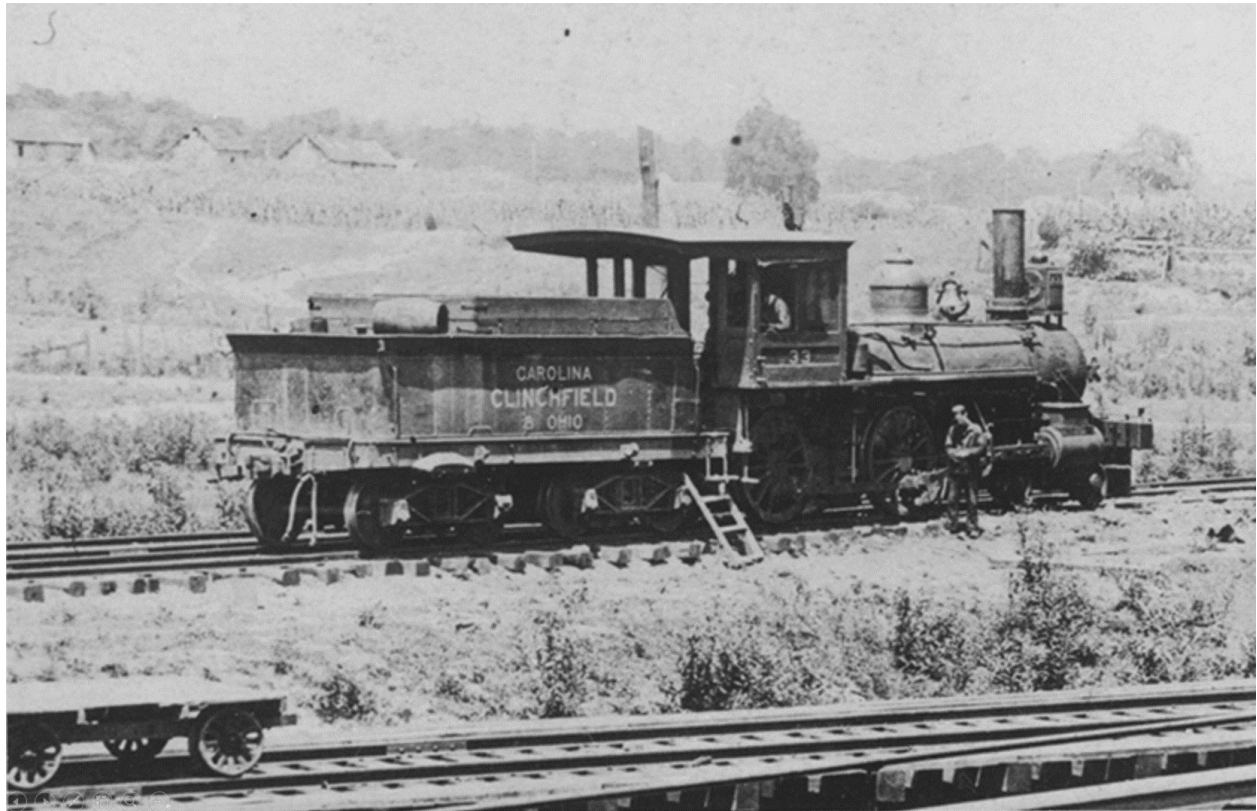


Figure 49. Historian Johnny Graybeal captioned this photograph: “During the days when the Three C’s, OR&C, and the S&W were short line operations, motive power requirements were low. Locomotive No. 33 served the short line OR&C and S&W for many years, pulling the mixed trains in from the middle of nowhere. By the time this photo was taken, the little No. 33 had definitely seen better days. Courtesy Clifford Maxwell Collection, Archives of Appalachia, ETSU.” Another source included: “This photograph is one of the earliest known pictures of a steam locomotive lettered for the Carolina, Clinchfield and Ohio Railway. The photograph was taken in August 1908, near Carnegie, Tennessee. The locomotive is a 4-4-0 ‘American’ type which was popular on American railroads around the turn of the century. Photo from the collection of Gene Pat Price, Erwin, Tennessee.” This engine had previously belonged to the South & Western Railway as their # 33.²³⁷

1 October 1903. Johnson City Comet.

“Contractor Blackwell Interviewed. Several days ago the Knoxville Journal and Tribune published the following interview with Contractor Blackwell about the work on the road:

George H. Blackwell, a railroad contractor, who has been interested in the construction of the South & Western railway from North Carolina through East Tennessee, spent several hours in Knoxville Monday. Mr. Blackwell recently went over that road from Johnson City to North Carolina. He reports the work of construction progressing well on the North Carolina division. About sixty-five miles have been completed on that end of the road and he thinks work will be started from Johnson City northwest toward the Virginia coal fields at an early date. The construction of this line to North Carolina has been pushed all summer and heavy expenditures have been made in getting a good grade. It has been announced for several months that this line will be finished across Tennessee from Johnson City to connect with the Virginia & Southwestern system in Scott county, Virginia...

Mr. Blackwell is confident that the decision to complete the line has been reached.

‘The reason for the long delay,’ said he ‘is that the short line from Johnson City to the Virginia state line would have been of little value to the company until the North Carolina division is finished. The company has been making the expenditures where the line is most difficult to grade...’”

25 February 1904. Johnson City Comet.

“A.N. Molesworth and his corps of engineers are about a mile east of here today and is working west towards the furnace.”

3 November 1904. Johnson City Comet.

“Building Repair Shops. The South & Western railroad is building a machine shop near its depot in this city. It will be equipped with sufficient machinery to do all their repair work and a great deal of the outfit is already in place. The company is doing a good business and may put in a foundry later. It begins to look as if the line would be built to the coal fields in the near future. Mr. Carter is not talking, but work is going on all along the line.”

17 November 1904. Johnson City Comet.

“Machine Shop. As announced some weeks ago the S. & W. people are building a machine shop in Carnegie near the depot. The structure is almost ready for the roof and the machinery is already in place. This shop is only temporary. It will some day be moved to a point where more room can be had and a foundry added.”

17 November 1904. Johnson City Comet.

“Street Car Extension. The Johnson City Traction Co. is rapidly extending its line to Hotel Carnegie and the S. & W. depot. It will be completed next week if the weather is favorable. When the cars begin running to the depot the S. & W. trains will not stop at the Market street crossing for passengers and persons desiring to take trains will have to go to Carnegie. The street car line will be connected with the S. & W. sidings and will be thus enabled to transfer freight from the

depot to the Soldiers Home or to other parts of the city. The street car line will be laid out Second Avenue to the hotel instead of on First Avenue as heretofore stated.”

November 1904. The Official Railway Equipment Register.²³⁸

Table 51. “South & Western Railway Company.”

General Officers.		
Geo. L. Carter, Pres. and Gen Mgr. – Bristol, Va.	H.G. Morison, Asst. Gen. Counsel – Bristol, Va.	Mel. H. Weiler, Superintendent and Car Accountant – Johnson City
J.C. Stone, Sec., Treas. & Aud. [– Bristol, Va.]	J. Fred. Johnson, Purchasing Agent [– Bristol, Va.]	
J. Norment Powell, Gen. Counsel [– Bristol, Va.]	Jno. A. Muse, Gen. Fri. and Pass. Agt. [– Bristol, Va.]	
General Offices, Bristol, Va.		
Miles of road operated, 64. Gauge, 4 ft. 8½ in. Locomotives, 4. No Express or Sleeping Car Co. operating over this line...		

[Table 52.] Freight Equipment.

The freight cars of this company are lettered ‘S. & W.,’ and numbered and classified as follows:

Kind of Cars.	Numbers.	Dimensions.				Capacity.	No.		
		Inside.						Pounds	
		Length		Width		Height			
ft.	in.	ft.	in.	ft.	in.				
Flat	153 to 182	34		8	10		40000	5	
[Flat]	501 to 510	36		8	10		50000	10	
[Flat]	3001 to 3017	34		8	10		60000	11	
Box	1072 to 1370	33	6	8		6	9	40000	13
[Box]	1408 to 1431	33	6	8		6	9	60000	3
Work	197								1
Total									43
Passenger Equipment									
Passenger – 7, 8	2	Baggage – 51	1	For balance remit to or draw on J.C. Stone, Treasurer, Bristol, Va. Send bills for repairs to cars to J.C. Stone, Auditor, Bristol, Va.					
Combination – 14, 15	2	Total	5						
Report movements and mileage or per diem to Mel. H. Weiler, Superintendent and Car Accountant, Johnson City, Tenn.									
Freight Connections and Junction Points.									
East Tennessee & Western North Carolina – Johnson City, Tenn.				Southern – Johnson City, Tenn.					
November, 1904.									

1 December 1904. Johnson City Comet.

“Working at Home. The South and Western machine shop is practically complete, the machines have all been trained and the plant is in working order with a full force of mechanics

under Master Mechanic Hobbs, of Bristol. All machinery is new and the latest improved models. It is a splendidly equipped plant, well manned and will be able to do the company's work nicely. A foundry will be added in a short time and the plant will be complete."

1 December 1904. Johnson City Comet.

"Building Sidetrack. The J.M. Buck Lumber company began construction of a sidetrack through its new lumber yard Monday morning. The yard is located on the south of the Southern railway and opposite the South and Western crossing. It will be reached by both roads. The ties are being delivered and track laying will begin at once."

12 January 1905. Johnson City Comet.

"Extending Line. The Johnson City Traction Co. has completed its line to the S. & W. depot and will begin operating cars to that point in a few days."

9 March 1905. The Johnson City Comet.

"Uptown Depot. The South and Western railway has arranged with the Johnson City Traction Co. for the establishment of an up-town depot for the reception of freight. The Traction Co. will build a depot at the plant of Roan street and all freight for the S. & W. will be received and receipted for there during the day and delivered in the afternoon to the S. & W. depot over the car line. This will be a great convenience to shoppers, particularly jobbers, as they will save the long wagon haul over bad roads.

The street cars are now running regularly to the depot and passengers intending to take trains must go to the Carnegie station as they will not stop at Market street hereafter. This will be a great accommodation to the traveling public as it will avoid tedious waits in the open air at Market street."

9 March 1905. The Johnson City Comet.

"Railroad Notes. The S. & W. people have an engineer corps at work from Johnson City south making a map of the line. Other corps are at work on the line between here and Kingsport and in the mountains beyond Spruce Pine.

It is reported here that Wm. Ritter is interested in the purchase of the S. & W. with the Seaboard directors. It is also stated that the Southern has offered them a million dollars profit on the deal."

13 April 1905. Johnson City Comet.

"The S. & W. engineers were running a line through here yesterday."

15 June 1905. Johnson City Comet.

“The South & Western has just made a new survey through Johnson City. They come into this city west of the union depot through the Miller lands, cross the Southern near the Soldiers’ Home, on Harry Lyle’s property and go out to Holston avenue and down through Carnegie.”

15 June 1905. Johnson City Comet.

“Railroad Notes. Parties interested with George L. Carter have purchased a large quantity of building material from the Summers – Parrott Hardware Co., of this city and had it shipped to Spruce Pine. From the character of the material it is evident that temporary quarters are to be erected for workmen. Just when work will be started on the extension south is not known, but the indications are that the company will get busy at once as a certain gap in the Blue Ridge mountains must be held and it looks like the Southern has designs upon it for the line it wants to build from Marion to this city.”

22 June 1905. Johnson City Comet.

“Grading Begun. A force was put to work Tuesday of this week grading the extension of the South & Western from Spruce Pine South. The force is small for the time being on account of the character of the work but will be enlarged as circumstances will permit. Work will not be commenced on the north end until the engineers find a suitable route. They are still surveying routes through this city.”

29 June 1905. Johnson City Comet.

“Great Activity is Reported Toward Extending the S. & W. Road Eastward. A Knoxville citizen who has just returned from a trip to the eastern terminus of the South & Western railway reports great activity toward extending the line eastward. He stated that barracks are being built for a large force of men at Spruce Pine, the terminus of the road, just east of the Blue Ridge, and that Geo. L. Carter, who is back of the road, is after five hundred more men to put to work on the line. A large force is at work improving grades and curves on the line already built.

George Carter had left Johnson City day before yesterday for the east. It was learned that he had just placed an order for six hundred freight cars for the S. & W. road. This indicates that there will soon be need of these cars and that construction of the proposed extension to the Seaboard Air Line on the South and to the Virginia coal fields on the north, is to be carried forward on a large scale.”

29 June 1905. Johnson City Comet.

“South & Western Promotions. The office force at the South & Western depot has been rearranged on account of the growth of business. Joe Horton has been promoted from agent to chief clerk to Trainmaster Mel H. Weiler, a new office just created, and J.J. Range has been promoted from check clerk to agent. His place as check clerk will be filled later. The changes take effect July 1. Some changes may be made in the operating force later on account of the new work of construction.”

24 August 1905. Johnson City Comet.

“Double Daily Service. The S. & W. will establish a double daily train service for the accommodation of passengers between Johnson City and Sprucepine, including intermediate points, beginning Monday, Aug. 28. In addition to former schedule, which is slightly changed, a train will leave Sprucepine daily including Sunday at 7 o’clock a.m. arriving here at 10:30; returning at 3:40 p.m.. allowing ample time for those desiring to visit the Soldiers’ Home or those who wish to make purchases in this city, Eastern time will be adopted, which, for those who are not familiar, means one hour faster than our former time. Reduced rates will be granted for Saturdays and Sundays similar to those now in effect for trains running in opposite directions.”

19 October 1905. Johnson City Comet.

“New Wood Working Plant. Johnson City is to have a new wood working plant in the near future. T.J. Galloway and Haskett Wood have arranged to build and equip a planing mill upon their land near the S. & W. crossing in Carnegie. It is the intention of the promoters to cut non-shipping lumber into dimension stock of all kinds, particularly chair stock. The building will be equipped with the most modern machinery obtainable for that purpose. Work will be commenced upon the building at once.”

9 November 1905. Johnson City Comet.

“S. & W. Completed to the Toe River. Spruce Pine, N.C., Nov. 6. – The South & Western extension east of Spruce Pine has been completed to the banks of the Toe River, two and a half miles from this point. Here a wait is necessary pending construction of the bridge over the river. The work of putting in the stone abutments is progressing with vigor, and it is expected the work will be completed by January 15th.

In the meantime a great deal of grading is in progress between the river and Marion, N.C., and there are no fewer than thirty-five corps of engineers on this piece of road, only twenty-five miles in length.”

7 December 1905. Johnson City Comet.

“Repairing Ruins. The S. & W. people are putting a roof on a portion of old stone structure built in boom days in Carnegie for a passenger station and will use it as a carpenter shop. The portion being enclosed in the western wing.”

14 December 1905. Johnson City Comet.

“Right of Way Wanted. The S. & W. is still undecided as to which route it will use through Washington County but a decision is expected within three months. The Limestone route has been surveyed and a new survey is now being made over the old Three C’s route. Rights of Way are to be asked for on both lines and when all are secured the officials will decide upon the final location of the road. Hon. H.C. Jackson will assist in securing rights of way via Limestone and the Commercial Club will appoint a committee to act with S. & W. officials on the line via this city. The company is taking deeds for a 100-foot right of way along the entire line.”

28 December 1905. Johnson City Comet.

“Engineer Corps Here. The white tents of a large engineer corps are pitched in Carnegie and the talented gentlemen are at work north of here in an effort to locate the South & Western permanently.”

4 January 1906. Johnson City Comet.

“Two New Engines. The South and Western Railway Co. has received two new locomotives, a 65-ton passenger engine and an 85-ton freight engine. Both are first-class machines and are being limbered up to go into service at once.”

Regettably, the author has not found more information on these locomotives.

15 February 1906. Johnson City Comet.

“If the South & Western keeps on running lines and buying rights of way it will have a sample of every farm in Washington county.”

14 June 1906. Johnson City Comet.

“Taking Options. South and Western officials are taking options upon all farms touched by the survey that crosses the Southern above the Soldiers’ Home. It begins to look like the road will finally be located by that route and the indications are that official announcement of that fact will be made in a few days.”

28 June 1906. Johnson City Comet.

“Immense Turntables. The S. & W. railroad company has received two 80-foot turntables with a carrying capacity of 250 tons. One will be placed in the yard here and the other taken to another division.”

30 June 1906. Poor’s Manual of the Railroads of the United States.

“South and Western Ry. – Johnson City, Tenn., to Sprucepine, N.C., 63.12 miles. Gauge, 4 ft. 8½ in. Rail (steel), 60 lbs... The extension from Caney River to Sprucepine, 30.5 miles, was built by the present company, the last section of it, from Booneford to Sprucepine, 7.7 miles, having been opened for traffic on July 1, 1903. Equipment, June 30, 1906: Locomotives, 6. Cars – passenger, 4; baggage, etc., 1; freight (box, 253; flat, 50), 303 – total, 308.

Operations, year ending June 30, 1906. – Trains run (passenger, 5,312; freight, 18,322; mixed, 80,128), 103,762 miles. Passengers carried, 44,171; carried one mile, 883,422. Tons freight moved, 97,407; ton-miles, 3,996,753. Earnings (passenger, \$31,803; freight, \$154,312; other, \$9,401), \$195,016. Operating expenses, \$163,972. Net earnings, \$31,044. Payments: Interest on bonds, \$44,050; other interest, \$12,581; taxes, \$5,282-total, \$61,913. Deficit, \$30,869; surplus forward (\$20,470; additions during year, \$115,059), \$135,529; net surplus, June 30, 1906, \$104,660.’.

Directors. – George L. Carter, Johnson City, Tenn.; A. Walter, W.W. Porter, H.R. Dennis, New York, N.Y.; A.A. Phleger, Bristol, Tenn. Officers: George L. Carter, Pres.; A.A. Phleger; Vice-Pres.; J.C. Stone, Sec. & Treas., Johnson City, Tenn. General Office, Johnson City, Tenn.”²³⁹

9 August 1906. Johnson City Comet.

“Steam Hammer Installed. The South and Western people have just installed an unusually large steam hammer in their repair shops in this city. It is the latest model and one of the most powerful manufactured.”

9 August 1906. Johnson City Comet.

“Several railroad contractors have been in the city this week, having been over the proposed line of the S. & W. from Kingsport to this city. The route via the Soldiers’ Home is alone being considered at this time.”

16 August 1906. Johnson City Comet.

“The Southern Active. – Johnson City Will Get Many Dollars Worth at New Work This Year. The Southern will spend about \$50,000 in enlarging its yard capacity in Johnson City. The General Manager, Assistant General Manager Geo. R. Loyal, and Superintendent of the Knoxville division, were in the city this week and went over the work to be done here with yardmaster Burchfield. Among the things to be done the double track will be extended from the present terminus opposite the Planter’s Hotel to Carnegie. An additional track 350 feet long will be built opposite the depot in the Love enclosure, the creek having been changed for this purpose. The greatest amount of work is to be done in the Carnegie yards where six or eight additional tracks are to be laid on the south side of the main line, extending from the S. & W. crossing several thousand feet down the main line almost opposite the furnace yard lead. When this yard is completed the terminal will be changed to Carnegie, even now all trains are made up there for all points. It will be a surprise to many of our people to know that only two freight schedules run through this city to Bristol and that twelve crews run out of here daily to different places. In fact, Johnson City is the third largest shipping yard on the Knoxville division, Knoxville and Chattanooga only being larger, in the order named.

Without waiting for the completion of the Carnegie yard all engines are being banked there instead of on the present track opposite the depot. The order will be effective at once.”

30 August 1906. Johnson City Comet.

“One Contract is Awarded – Ten Miles of S. & W. R.R. from Johnson City Northward has been Let. – Carpenter & Boxley, of Roanoke, Get the Contract and Will Put a Force to Work at Once – (From Thursday Morning’s Extra) – henceforth there will be something doing on the South & Western Railroad in and around Johnson City. Contractors Carpenter & Boxley, of Roanoke, have been awarded the contract for ten miles of work, beginning on Cherokee and crossing the Southern above the Soldiers’ Home and extending northward toward Kingsport. The ten miles will extend nearly to the farm of W.F. Carter.

This settles the location of the route north of the city and as to where the road will cross the Southern, but just how it will run through the city proper is not definitely known. The company has secured options on all lands paralleling the Narrow Gauge and Southern roads, and is now taking options on the land along the route through Young's Gap.

Contractor Boxley has purchased six cars of lumber for camps and will have it delivered at once to the Soldiers' Home siding. He states that it is to be the finest road ever built in the south. There will be no grades over 6-10ths of one per cent. to the coast. His work calls for some extra heavy cuts, as all the ridges north of this city are to be practically leveled. He will erect his shanties and start the earth moving within a few days.

Later. – Thursday morning land owners along the Young's Gap route were notified that the options would be closed and the land taken. This means that the mainline will go through the gap, but it is also known that a line will be built parallel with the Southern and Narrow Gauge roads through the city, not only for passenger traffic but to form a belt line to reach every factory and factory site in the city.

The company now has a right-of-way to every factory in the city and has enough land under option to make a fortune if it should care to sell at advanced prices. The options have nearly all been taken in the name of George L. Carter.

This may be called modern railroad building – to speculate in real estate along the route. The people will not object, however, so long as the road is built and the community generally benefitted.”

13 September 1906. Johnson City Comet.

“Terminus of S. & W. Extended to Altapass, North Carolina. Mr. Lewis Walker, general passenger agent of the South and Western Railway, has announced that on September 16th, four miles of newly constructed line, extending from the present terminus, Spruce Pine, to Altapass, N.C, the latter being on the summit of the Blue Ridge mountain, where the line begins the descent into the Piedmont section, will be put into operation and the terminus of the road will be extended to Altapass, N.C.”

4 October 1906. Johnson City Comet.

“Johnson City generally gets what it goes after. It wanted the general offices of the South and Western railroad company and went after them. As a result the Carnegie Hotel and the Wilder block have been leased for twenty-seven months by the company and will be occupied by the first of November or as soon as they can be put in proper repair. We congratulate Mr. Carter and associates upon the wisdom displayed in this decision and felicitate Johnson City upon its victory. It's no trouble to show Johnson City's advantages to wise people and win them.”

South and Western Offices Will Come to Johnson City

**Hotel Carnegie and Other Buildings Have Been Leased
and the Transfer Will be Made Within Next
Sixty Days—Means Much to Our City.**

The Bristol Herald of the 5th instant says: "It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter having been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted and otherwise repaired and will afford ample room for the use which will be made of them.

Figure 50. Johnson City Comet, 4 October 1906.²⁴⁰

10 October 1906. The Coal Trade Journal.

"It was announced in Bristol this week semi-officially that the principal offices of the Clinchfield Coal Co., Dawson Coal & Coke Co., Virginia - Pocahontas Coal & Coke Co., Crane's Nest Coal & Coke Co., South & Western Ry. Co., and the other business of the George L. Carter and Thomas F. Ryan interests, would, within 26 days, be moved to Johnson City, a small town 25 miles south of Bristol. The reason assigned for leaving Bristol was their inability to obtain sufficient office room under one roof in Bristol. This will be a loss to Bristol of a pay-roll said to aggregate \$25,000 monthly, and about 200 men."²⁴¹

11 October 1906. Johnson City Comet.

“Official Announcement. – South and Western Officials Tell Something of the Plan. ‘After a consultation between Alfred Walter, president of the Seaboard Air Line railway and chairman of the board of directors of the South & Western, and George L. Carter, president of the latter and head of the George L. Carter syndicate, in Bristol last night it was officially announced that the headquarters of the South & Western would be moved to Johnson City within sixty days.

The Carnegie hotel and the Wilder block at Johnson City have been leased by the company and will be used as the general offices of the operating and engineering departments. Everything will be moved from Bristol and it is probable the officials who now reside here [Bristol] will move to Johnson City.

The George L. Carter syndicate, which is building the South & Western, is backed by George L. Carter and Thomas F. Ryan and is one of the largest corporations in this entire region. The construction of the South & Western railway from the coal fields of Virginia to the South Atlantic Seaboard will be completed within two years...

This location of the offices will mean much for Johnson City as the pay roll of the office force alone is said to exceed \$25,000 per month, and the city has been kept continually flooded with laborers and transients connected with the road for two years and much revenue has been derived therefrom.

One reason assigned for leaving Bristol is the lack of office room...

The Carter interests have taken options on nearly all of the available real estate at Johnson City and the immense yards, the largest on the road, will be constructed in the suburbs of the city and options for the same will be taken up as soon as possible, The road will enter the same depot with the Southern and ground has been secured for this purpose at what is considered a very reasonable price.’ – Bristol Correspondent in Knoxville Sentinel.”

22 November 1906. Johnson City Comet.

“Carpenter & Hill, Contractors are now at Work Constructing the South & Western Railway between Kingsport and Johnson City. They have established several camps along the route. The work is being pushed with all possible speed and it is said that all the millions of dollars necessary to complete the road from the coal fields to a deep-water connection in North Carolina is provided for. In the future the company will make Johnson City its headquarters.”

6 December 1906. Johnson City Comet.

“South & Western Offices Will be Moved to Johnson City as Soon as Buildings are in Readiness. The offices of the South & Western railway and allied interests will be moved from Bristol to Johnson City before Christmas, if the buildings that are to be used at Johnson City can be prepared for occupancy by this time. Otherwise, the moving will not occur until about the first of the year...”

20 December 1906. Johnson City Comet.

“Construction Work All Let. – South & Western has Entirely Abandoned Work on New roadway. The South & Western railway has entirely abandoned the building of its own railway, having now let the entire distance yet to be constructed to contractors. Consequently a number of

men who have been employed by the road in connection with the work of construction will leave the employ of the company or will go to into other departments. Two paymasters have gone and it is said that others will leave this department January 1.

At first the South & Western did its own construction exclusively, but gradually the roadway was let piece by piece until the road now has no more active construction work. All that will be necessary in this department will be to retain supervising engineers and their corps of assistants and clerks and the number of these will be small.

The South & Western is expecting its entire clerical force at Johnson City, the future headquarters of the road, by January 1, or shortly thereafter. The transfer was to have been made last Friday, but was delayed by the failure to complete the office buildings of the company. The electric lighting apparatus is not in working order, and the walls have not been papered. – Bristol Courier.”

20 December 1906. Johnson City Comet.

“Offices will be Moved Next Week. – South & Western Railway Company to Make Johnson City Headquarters. The offices of the South & Western railway, Clinchfield Corporation, etc., will be moved to Johnson City about the middle of next week. Some delay was occasioned in the preparation of the office building at Johnson City, on account of the failure of the steam heating plant, which is being installed, to arrive promptly. The material arrived at Johnson City several days ago, and workmen are busily engaged in installing it and completing other work on the buildings, preparatory to their occupancy by the South & Western offices next week. The offices will be in the Carnegie hotel building and the Wilder building at Johnson City. – Bristol Herald.”

17 January 1907. Johnson City Comet.

“From Kingsport to Johnson City. – Work on South and Western will be Pushed with all Possible Speed. Kingsport, Tenn., Jan. 11. – With all possible speed, the construction work on the South and Western railway will be pushed between Kingsport and Johnson City in the near future.

The Herald’s correspondent was reliably informed yesterday that Carpenter & Boxley, the contractors, are having machinery transported over the country roads to the South and Western... It is said that Carpenter & Boxley have the contract for grading the South and Western from the south fork of Holston river, east of Kingsport to Johnson City...”

7 February 1907. Johnson City Comet.

“Will Come Up Town. Beginning at once the S. & W. will run passenger trains up town over the tracks of the E.T. & W.N.C. R.R. Trains will be backed in from the S. & W. crossing each morning and will come up town first of evenings. This will be a great accommodation to the traveling public and will be much appreciated.”

7 February 1907. Johnson City Comet.

“Getting Ready Fast. The South & Western railway is rapidly getting ready for business. The first order for rolling stock has been placed in the East calling for 120 190 ton Engines, 25 Coaches, and 5,000 extra heavy hopper-bottom coal cars. It is expected that trains will be running

to Marion N.C. during this year. Every effort is being made to complete the road within 12 months. The right of way from Altapass, N.C. to the Virginia Coal fields is alive with workmen and machinery and the coming summer will put many miles beneath the twin bars that will carry commerce to the seas.”

Regrettably, the author has not found any additional information on these locomotives.

20 June 1907. Johnson City Comet.

“Through the City. – South & Western Now Building in Corporate Limits. The South & Western is now being built through the corporate limits of Johnson City. Contractors Carpenter & Boxley have a steam shovel at work on the heavy cut on the Joe Lyle land.

The line leaves the main line north of the Embreville crossing and swings down by the John F. Lyle store house in front of the main entrance to the Soldier’s Home and crossing the Jonesboro road hugs the Southern down to the passenger depot and then follows the E.T.W.N.C. out to the Harris Mfg. Co. and uses the old road south.

General Manager Caples says it will be necessary to use the old road to Erwin for at least two years as work on the new route just south of this city has been held up by lawsuits over the right-of-way.

The large brick warehouse at the Standard Oak Veneer Co.’s plant is to be removed to allow the track to pass that point and manager C.B. Allen has been notified to vacate.

This road will pass through the city on a slightly higher grade than the Narrow Gauge and the latter will be raised to correspond with the new grade.

This means the tearing away of the foundry and all the buildings facing the right-of-way from the big spring on Buffalo street to Roan street.”

27 June 1907. Johnson City Comet.

“Folsom Leonard, the 12-year old son of W.J. Leonard, who lives about two miles south of this city, had his leg badly mashed Tuesday between two dirt cars on the S. & W. road near the Southern crossing. It required several stiches to dress the wound.”

Table 53. "South & Western Railroad."

General Officers.		
Mark W. Potter, Chairman of the Board, New York City	John A. Muse, Auditor of Disb., Bristol, Va.-Tenn.	Lewis Walker, Gen. Frt. and Pass. Agt., [Bristol, Va.-Tenn.]
Geo. L. Carter, President, Bristol, Va.-Tenn.	J.N. Powell, Gen. Counsel, [Bristol, Va.-Tenn.]	F. Powell, Gen. Claim Agent, [Bristol, Va.-Tenn.]
M.J. Caples, Gen. Manager, [Bristol, Va.-Tenn.]	J.C. Stone, Secretary and Treasurer, [Bristol, Va.-Tenn.]	M.H. Weiler, Train Mast. & Car Acct., Johnson City, Tenn.
I. McQuilken, Comptroller, Johnson City, Tenn.	J. Fred. Johnson, Purchasing Agent, [Bristol, Va.-Tenn.]	S.R. Jennings, Train Master, Dante, Va.
General Offices, Bristol, Va.-Tenn.		
Miles of road operated, 67.6 Gauge, 4 ft. 8½ in. Locomotives, 12. No Express or Sleeping Car Co. operating over this line...		

[Table 54.] Freight Equipment.

The freight cars of this company are lettered 'S. & W.,' and numbered and classified as follows:

Kind of Cars.	Numbers.	Dimensions.												Capacity Pounds	No.					
		Inside.						Outside.								Doors.				
		Length	Width		Height		Length	Width at Eaves or Platform		Height from Rail.						Side.				
			ft.	in.	ft.	in.		ft.	in.	ft.	in.	ft.	in.			ft.	in.	ft.	in.	
Flat	100 to 124	40	9	3													60000	25		
[Flat]	153 to 157	34	8	10													40000	5		
[Flat]	501 to 510	36	8	10													50000	10		
Box	1000 to 1249																60000	250		
[Box]	1429 to 1431	33	6	8	6	6	35	2	9	4	10	11	12	10	4	11	6	4	60000	3
[Box]	1302 to 1314	33	6	8	6	6	35	2	9	4	10	11	12	10	4	11	6	4	40000	13
Gondola	2000 to 2024	25	6	8	6	4	7											60000	25	
Tool	197																	40000	1	
Total																			332	
Passenger Equipment.		Report movements and mileage or per diem as follows: Tennessee and North Carolina Division to M.H. Weiler, Train Master and Car Accountant, Johnson City, Tenn.; Virginia Division S.R. Jennings, Train Master, Dante, Va.																		
Passenger – 7, 8, 10, 11	4	For balances remit to or draw on J. McQuilken, Comptroller, Johnson City, Tenn.																		
Combination – 14, 15	2	Send bills for repairs to cars to J.C. Stone, Treasurer, Bristol, Va. – Tenn.																		
Baggage – 51	1																			
Private – Clinchfield	1	Freight Connections and Junction Points.																		
Total	8	East Tennessee & Western North Carolina – Johnson City, Tenn.						Southern – Johnson City, Tenn.												

July, 1907.

August 1907. The Official Railway Equipment Register.²⁴³ Note: the flat cars have been renumbered.

Table 55. "South & Western Railroad."

General Officers.		
Mark W. Potter, Chairman of the Board, New York City	I. McQuilkin, Comptroller, Johnson City, Tenn.	Lewis Walker, Gen. Frt. and Pass. Agt., Johnson City, Tenn.
Geo. L. Carter, President, Johnson City, Tenn.	John A. Muse, Auditor of Disb., [Johnson City, Tenn.]	F. Powell, Gen. Claim Agent, [Johnson City, Tenn.]
Archer A. Phlegar, First Vice-President, Bristol, Va.-Tenn.	J.N. Powell, Gen. Counsel, [Johnson City, Tenn.]	M.H. Weiler, Train Mast. & Car Acct., [Johnson City, Tenn.]
M.J. Caples, Second Vice-President and Gen. Manager, Johnson City, Tenn.	J.C. Stone, Sec. and Treas., [Johnson City, Tenn.]	
	W.A. Starritt, Act. Pur. Agent, [Johnson City, Tenn.]	S.R. Jennings, Train Master, Dante, Va.
General Offices, Bristol, Va.-Tenn.		
Miles of road operated, 75. Gauge, 4 ft. 8½ in. Locomotives, 13. No Express or Sleeping Car Co. operating over this line...		

[Table 56.] Freight Equipment.

The freight cars of this company are lettered 'S. & W.,' and numbered and classified as follows:

Kind of Cars.	Numbers.	Dimensions.										Capacity Pounds	No.								
		Inside.			Outside.				Doors.												
		Length	Width	Height	Length	Width at Eaves or Platform	Height from Rail.		Side.												
							To Eaves	To over all	Width	Height											
ft.	in.	ft.	in.	ft.	in.	ft.	in.	ft.	in.	ft.	in.										
Flat	100 to 124	40	9	3										60000	25						
[Flat]	125 to 128	34	8	10										40000	4						
[Flat]	129 to 149	36	8	10										50000	21						
Box	1000 to 1249	36	8	6	8		9	10	12	5	14	6	8	60000	250						
[Box]	1302 to 1317	33	6	8		6	6	35	2	9	4	10	11	12	10	4	11	6	4	40000	16
Gondola	2000 to 2054	25	6	8	6	4	7							60000	55						
Tool	197													40000	1						
Spreader	T138														1						
Caboose	1														1						
Total															374						
Passenger Equipment.		Report movements and mileage or per diem as follows: Tennessee and North Carolina Division to M.H. Weiler, Train Master and Car Accountant, Johnson City, Tenn.; Virginia Division S.R. Jennings, Train Master, Dante, Va.																			
Passenger – 7, 8, 10, 11	4	For balances remit to or draw on J. McQuilken, Comptroller, Johnson City, Tenn.																			
Combination – 14, 15, 20, 51	4	Send bills for repairs to cars to J.C. Stone, Treasurer, Bristol, Va. – Tenn.																			
Private – Clinchfield	1	Freight Connections and Junction Points.																			
Total	9	East Tennessee & Western North Carolina – Johnson City, Tenn.					Southern – Johnson City, Tenn.					August, 1907.									

24 October 1907. Johnson City Comet.

“Big Land Deal – This afternoon the South & Western bought from Joe P. Lyle all of the land owned by him between that company’s industrial line and the Southern’s right-of-way, it being about 40 acres; also 25 acres south of the industrial line and along the main line of the South & Western. The consideration is \$23,000. Mr. Lyle is to take over the store and dwelling house near the south gate of the Soldiers’ Home, formerly owned by John F. Lyle, as a part of the transaction. Attorney Isaac Harr is preparing the necessary papers for Mr. Lyle, and Attorney J.B. Cox is representing the company. Many think that this purchase by the company means that it will locate its shops on this land. If this should happen, it will mean a great deal for Johnson City.”

24 October 1907. Johnson City Comet.

“Commercial Club Meets... M.J. Caples, of the South & Western spoke relative to the difficulties of that road in going through Johnson City. He said that the route by Jonesboro would have not only cost less, and was the shortest line and best grade but that the company had decided to come by Johnson City as it was the foremost town on its line. The business men should appreciate this spirit of the road toward our city. Mr. Caples also said that if the work on the road progressed as now the road would be completed by next summer...”

31 October 1907. Johnson City Comet.

“Forces Materially Reduced. – The South & Western Reduces its Pay Roll until Money gets easier in New York. The financial crises in New York has affected the South & Western railroad, the new road being constructed from coal to deep water through this city. All construction work except contracts and in tunnels has been stopped and the office force in this city will be reduced about 50 per cent on account of this reduction...”

Table 57. "South & Western Railroad."

Miles of road operated, 75. Gauge, 4 ft. 8½ in. Locomotives, 13. Southern Express Co. operates over this line. No Sleeping Car Co. operating over this line...

Freight Equipment.

The freight cars of this company are lettered 'S. & W.,' and numbered and classified as follows:

Kind of Cars.	Numbers.	Dimensions.										Capacity	No.							
		Inside.			Outside.				Doors.					Pounds.						
		Length	Width	Height	Length	Width at Eaves or Platform.	Height from Rail.		Side.											
							To Eaves.	To over all	Width	Height										
ft.	in.	ft.	in.	ft.	in.	ft.	in.	ft.	in.	ft.	in.									
Flat	100 to 111	40	9	3										60000	11					
[Flat]	113 to 124	40	9	3										80000	12					
[Flat]	125 to 128	34	9	3										40000	4					
[Flat]	129 to 149	36	9	3										50000	21					
[Flat]	150 to 249	36	9	3										50000	100					
Box	1000 to 1249	36	8	6	8		9	10	12	5	14	6	8	60000	250					
[Box]	1302 to 1317	33	6	8	6	6	35	2	9	4	10	11	12	10	4	11	6	4	40000	16
Gondola	2000 to 2098	25	6	8	6	4	7												60000	99
Coal Hopper	6000 to 6099	30	3	9	5		31	11											100000	100
Scoop Car	1999	40	9	3															60000	1
Caboose	1																			1
Total																				615

The Return of Cars to Home Territory will be Facilitated by Routing as Follows:

Marks.	Special Instructions. Localities toward which this Company desires that its cars should gravitate:	
South & Western	All cars should be returned to Johnson City, Tenn., via Southern Ry.	
Miscellaneous Equipment.	Report movements and mileage or per diem to M.H. Weiler, train Master and Car Accountant, Johnson City, Tenn. For balances remit to or draw on J.C. Stone, Treasurer, Johnson City, Tenn. Send bills for repairs to cars to I. McQuilkin, Comptroller, Johnson City, Tenn.	
Tool – 197		1
Spreader – T138		1
Total		2
Passenger Equipment.	Freight Connections and Junction Points.	
Passenger – 7, 8, 10, 11	4 East Tennessee & Western North Carolina – Norfolk & Western – Fink, Va.	
Combination – 14, 15, 20, 51	4 Johnson City, Tenn. Southern – Johnson City, Tenn.	
Private – Clinchfield	1	
Total	9	

January, 1908

6 February 1908. Johnson City Comet.

“Railroad Shops Destroyed – Sunday Morning Blaze Hits the South & Western Hard. Fire destroyed between ten and eleven thousand dollars worth of property for the South & Western Railroad.

General Manager Caples wires Train Master Weiler from New York to put a force of men to work rebuilding the shop on the old site immediately.

Sunday morning about 2 o'clock fire broke out in the South and Western machine shop, destroying the shop, one passenger coach, besides doing slight damage to one locomotive and shop machinery. With the thermometer close to zero, a few of the fire ladies braved the cold and fought fire to keep from freezing. Mel H. Weiler, train master; E.J. Vandergrift, master carpenter; H.L. Hobbs, master mechanic, and Lynch Thompson, conductor, were quickly on the scene, shifting cars and saving the rolling stock of the road, and by their timely assistance and presence of mind is due to a great extent the slight loss the road sustained. Mr. Vandergrift had two fingers frozen and Mr. Weiler injured his leg that was broken a short time back.

The higher officials can congratulate themselves that while absent from the city their interests were in the hands of faithful employees of the operating department who performed valuable assistance in protecting the railroad's property at the cost of much physical suffering to themselves.

If Carnegie or the higher officials of the South and Western have a medal laying around loose, we move it be awarded to the fire ladies and the above mentioned gentlemen, for they were walking icicles and worked like 'Trojans.'

The new shop will contain about one-third more square feet of floor-space, but, like its predecessor, will be only a temporary affair.”

13 February 1908. Johnson City Comet.

“S.&W. Shops Will be Moved. – Will Come Up Town and Occupy the Johnson City Foundry and Machine Shop. The S.W. people will not rebuild their temporary shop that burned two weeks ago in the Carnegie yards, but will move the machinery to the Johnson City Foundry and Machine Company's plant about March 1st and occupy that until the permanent shops are located. The plant up town was recently purchased by the S.&W., but was leased to Mr. G.W. Sitton, and it being operated by him, but arrangements have been made by which the move is agreeable. This gives the S.&W. a much more complete machine shop and one that has few equals in East Tennessee. It seems to be a rational move on their part from a financial standpoint. This probably means that all trains will soon be running up town, either over the Narrow Gauge third rail, over their own line, and that the Carnegie yards are to be abandoned.”

9 March 1908. Industrial World.

“The South & Western will rebuild its machine shop at Carnegie, Tenn., which was destroyed by fire February 2.”²⁴⁵

12 March 1908. Johnson City Comet.

“Charleston, Clinchfield and Ohio. – The South and Western will be Changed as Above at Approaching Meeting. The South and Western will soon pass into history as far as the name is

concerned and the Charleston, Clinchfield and Ohio will do business at the old stand. This road started in business about twenty years ago as the Charleston, Cincinnati and Chicago, familiarly called the 'Three C's,' and to many it is still known by that name... The stockholders' meeting will be held on March 23d in this city and many matters of importance will come up..."

5.4. Carolina, Clinchfield and Ohio Railway Facilities in Carnegie, 1908 – 1914

7 March 1908. The Railway and Engineering Review.

"Carolina, Clinchfield & Ohio. – Official announcement relative to the taking over of the South & Western is as follows, dated April 1, at the office of the secretary: 'By amendment to its charter, obtained March 7, 1908, the name of South & Western Railroad Co., a corporation organized and existing under the laws of the state of Virginia, was changed to Carolina, Clinchfield & Ohio Railway, and that corporation has acquired by purchase the properties formerly owned by South & Western Railroad (of Tennessee), and South & Western Railroad Co. (of North Carolina), and is proceeding with the construction of a line of railroad from Dante, Virginia, to a point on the line between the states of North Carolina and South Carolina, of which line the portion from Johnson City, Tenn., to Altapass, N.C., is in operation, and the portion from Dante, Va., to Fink, Va., is being operated by this company under lease.'"²⁴⁶

26 March 1908. Johnson City Comet.

"S. & W. Purchase Approved. – Carolina, Clinchfield and Ohio Railroad Takes Over All Property of the Tennessee Corporation. A meeting of the stockholders of the Carolina, Clinchfield and Ohio railway was held... and the purchase of the South & Western railroad, a Tennessee corporation, by the Carolina, Clinchfield and Ohio, duly approved... The Carolina, Clinchfield and Ohio railroad, of which George L. Carter is president, will take over all the property now held by the South and Western railroad, of Tennessee, and the new name, it is understood, will become effective very soon. The transfer will be a formal one. – Bristol Herald – Courier."

21 May 1908. Johnson City Comet.

"Outlook is Favorable. There has been a distinct note of optimism in local business circles in the past week. Besides encouraging signs of improvement evident throughout the whole country, the measured activity of the C., C. & O. in pushing its work is promotive of much satisfaction. The work of that railroad both in the city and long the line is being pushed. Rush work is in progress on the temporary shops at Carnegie, work is going on in the west end, and several additional work trains have been set going. Taken together the outlook is favorable, and it would seem that the long expected big expansion for Johnson City will soon set in."

4 June 1908. Johnson City Comet.

"Temporary Shops – Of C.C. & O. to be Completed – Trains to Run by September. Work will be resumed on the C.C. & O. temporary shops at Carnegie at once [see Map 19]. The large wheel lathe is in position, and the building will be roofed over this week and next."

September 1st will probably see the road open for train service to Marion, N.C., and by November 1st, it is expected that the line will be completed with connection with the Seaboard Air Line at Bostic, N.C. When this is done, the large increase of rolling stock in use will necessitate much better shop facilities, and many more shop and repair track employes.

It is semi-officially reported that the road will be in operation north to coal fields by January 1st. In that event it seems probable that new and permanent shops will be begun early in the coming year...”

August 1908. “A Journey Over the C., C. & O.” by Colonel Fred A. Olds.

“... The porter gravely announced Johnson City and as we cross a narrow-gauge railway it is pointed out as the line to the Cranberry iron mines in the corner of Mitchell County. General Robert F. Hoke being largely interested, a great deal of this high class ore is brought to Johnson City, where during the panic they have never stopped work. This little railway, 34 miles long, is the East Tennessee & Western North Carolina. At Johnson City station was an alfalfa field and a plant of the leather trust and the neatest, cutest, and tiniest Y.M.C.A. I ever saw, in a box car, painted bright yellow with a little portico and flowers and fairly shedding forth an air of welcome. It was labelled ‘Railway Y.M.C.A.’ I saw a field sown in cow peas, the first since leaving McDowell County.

I had the pleasure of meeting most of the officers of the C., C. & O. at the handsome general offices. They were like all the under officials, courteous to the last degree, and very obliging. I told them of my trip over and they were warm in their congratulations and they asked many questions. I did not have the pleasure of seeing President George L. Carter, but Assistant Chief Engineer A.W. Jones and Mr. W.J. Steppins, engineer of bridges, were most kind and furnished admirable photographs which are used to illustrate their story of the ‘Rhododendron Route.’

There is a curiosity on the road four miles north of Johnson City, this being the largest fill in the world. It is at Knob Creek and is 1,500 feet long and 100 feet high. There is not to be a single trestle on this road. Everything is to be steel, concrete, stone or earth...”²⁴⁷

24 September 1908. The Johnson City Comet.

“Notice to the Traveling Public – On and after Saturday, September 26, 1908, the Carolina, Clinchfield and Ohio railway will operate its passenger trains from Johnson City, Tenn., over the tracks of the East Tennessee and Western North Carolina railroad.

Carnegie will be closed as a passenger station, and trains will start from and arrive at depot, corner of Cherry and Buffalo streets, opposite Southern Ry. depot, on same time they are scheduled to leave from and arrive at Carnegie.

John A. Muse, Acting General Passenger Agent.”

28 January 1909. Johnson City Comet.

“J.T. Cecil, chairman of the transportation committee of the Commercial Club, and M.I. Gump, of the committee, secured a promise from the C., C. & O. Ry. Officials to erect at once a freight depot up town, and work on a temporary building for that purpose will begin next Monday morning. The roads to the freight depot in Carnegie have simply been impassible [impossible?]

impassible?]. This arrangement will be highly beneficial and greatly appreciated by the merchants.”

25 March 1909. Johnson City Comet.

“M.J. Caples Inspecting Cars – M.J. Caples, General Manager of the C.C. & O. Ry., left Saturday, accompanied by Mrs. Caples, for Wilmington, Del., to inspect some passenger coaches that are under contract for the C.C. & O. road. From there he and Mrs. Caples will go to Cuba for a short vacation.”

15 April 1909. Johnson City Comet.

“Johnson City’s – The Completion of Carolina, Clinchfield and Ohio Railroad from Coal to Coast – After Years of Waiting Trains are Rolling Our way. – Banquet to Railroad Officials of Line Entering Here – Hotel Carnegie Scene of Festivity: Many Prominent Guests Present - Finley, Carter and Howe – Presidents of the Three Great Railroad Systems Occupy Seats at the Banquet Table. – Johnson City’s dream, Gen. J.T. Wilder’s ambition and George L. Carter’s handiwork is now a reality.

The great trunk line following the buffalo trail from coal to coast is completed through the mountains and is rapidly pushing forward over the lowlands to deep water.

Johnson City, the child of mountains pregnant with wealth sufficient to maintain it forever, officially launched this great enterprise last night with a banquet at Hotel Carnegie in honor of the presidents of the three great railway systems entering the city, W.W. Finley, of the Southern; Geo. L. Carter, of the Carolina, Clinchfield and Ohio; F.P. Howe, of the East Tennessee and Western North Carolina. It was a notable event and many prominent persons were present as guests of the Commercial Club.

Hotel Carnegie, the scene of the festivities, one of the largest hotels in East Tennessee, had been beautifully decorated for the occasion with ferns and potted plants, the color scheme of green and white being carried out in the dining room draperies. A clever conceit was the ‘C., C. & O.’ monogram in green with white background and green border, on the wall opposite the entrance to the dining room. The credit for the success of the banquet is due the club’s committee, Amzi Smith, Tate L. Earnest, J.P. Summers, J.W. Hunter.

At the long tables that filled the room were seated over 200 persons, and it was a fine looking body of men.

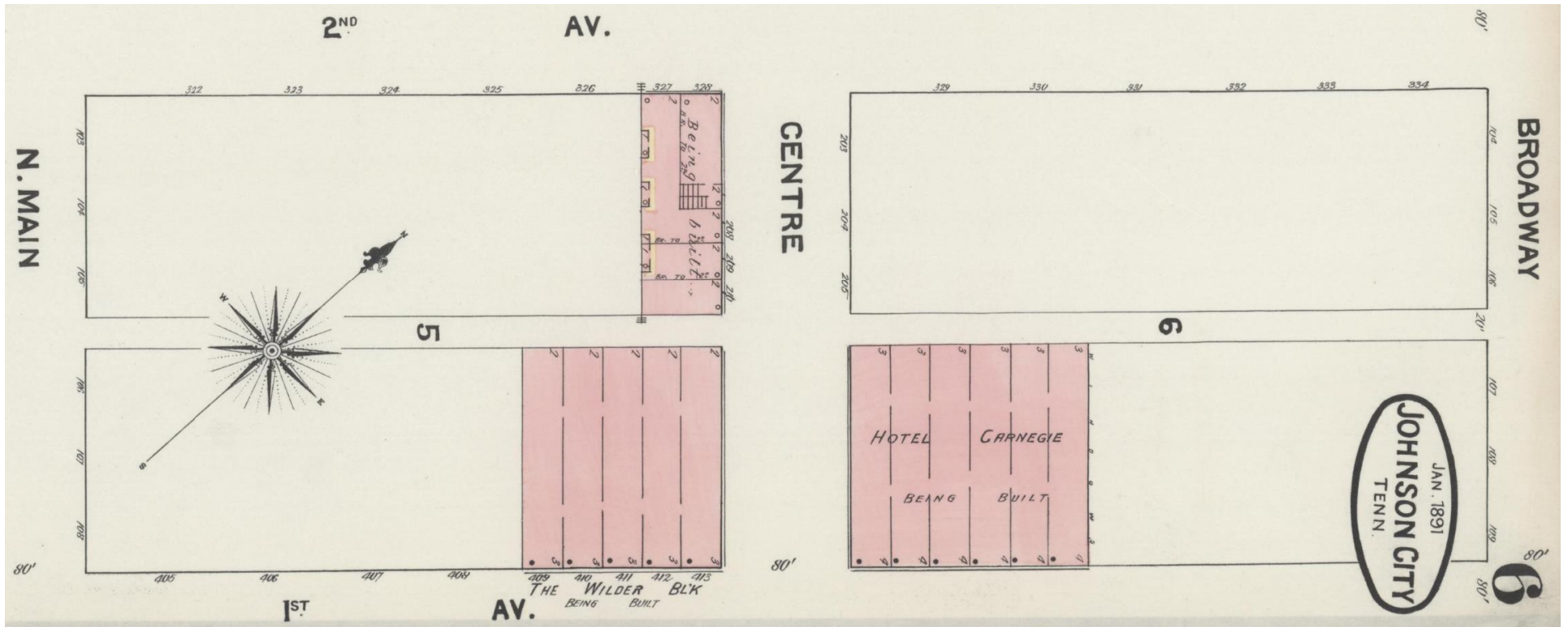
Hon. W.P. Brownlow presided as toastmaster in his characteristic manner and acquitted himself with honor. The speakers of the evening, President Finley, M.J. Caples, J. Norment Powell, Gen. J.T. Wilder, Col. W.A. Henderson and S.C. Williams, were seated on his right and left...”²⁴⁸

26 November 1914. Johnson City Comet.

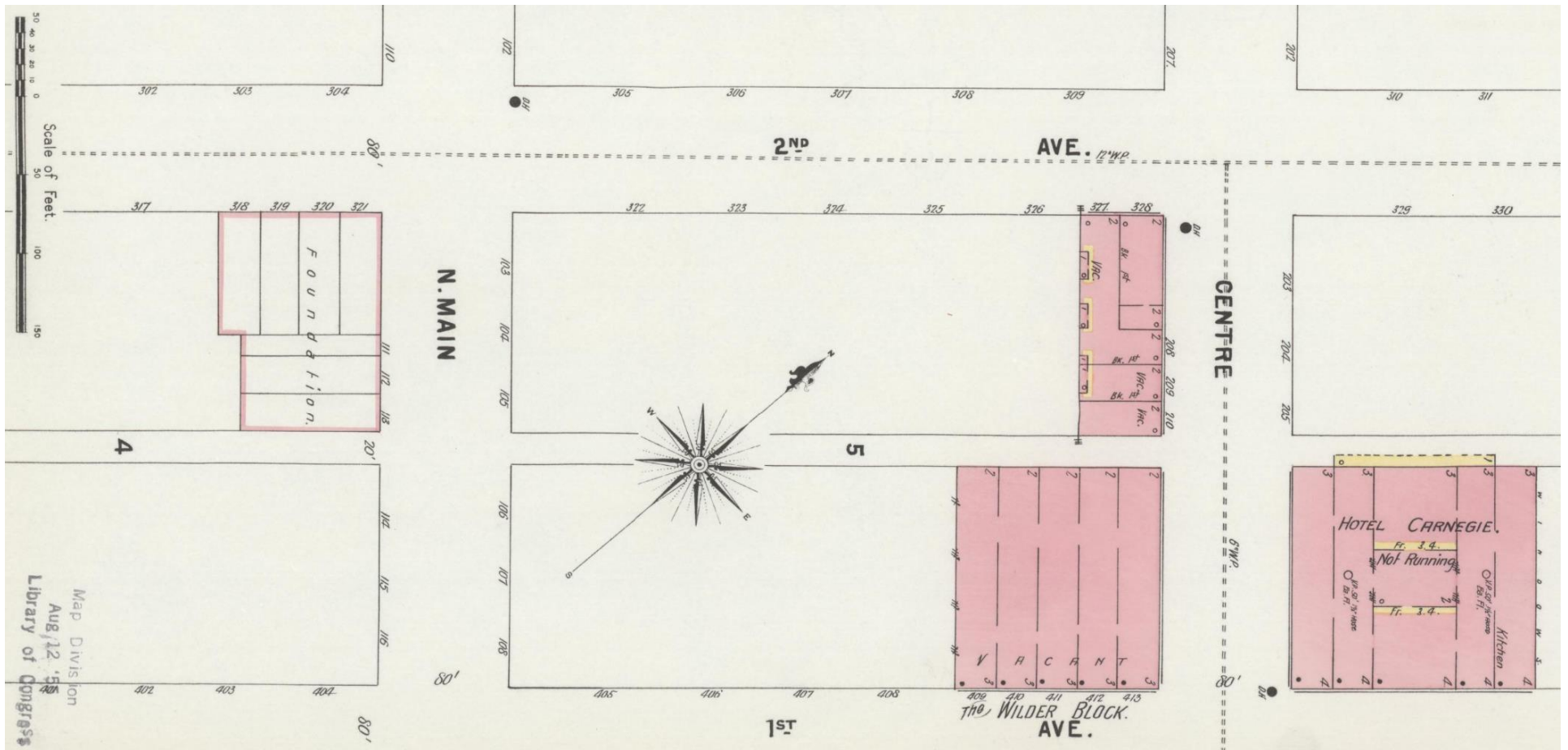
“Offices May Move to Carter Building. – The Carter building, formerly occupied by the Love – Thomas Co., is being remodelled and made into a modern office building. The first and third floors are being divided into many office rooms. A new vault will be built and a fourth floor may be added. It is stated upon good authority that the building will be occupied by the C.C. & O. Railway as a general office building and the office building in Carnegie abandoned.”

6. The “Wilder Block” and the Hotel Carnegie, 1889 – 2008
(Maps and Figures)

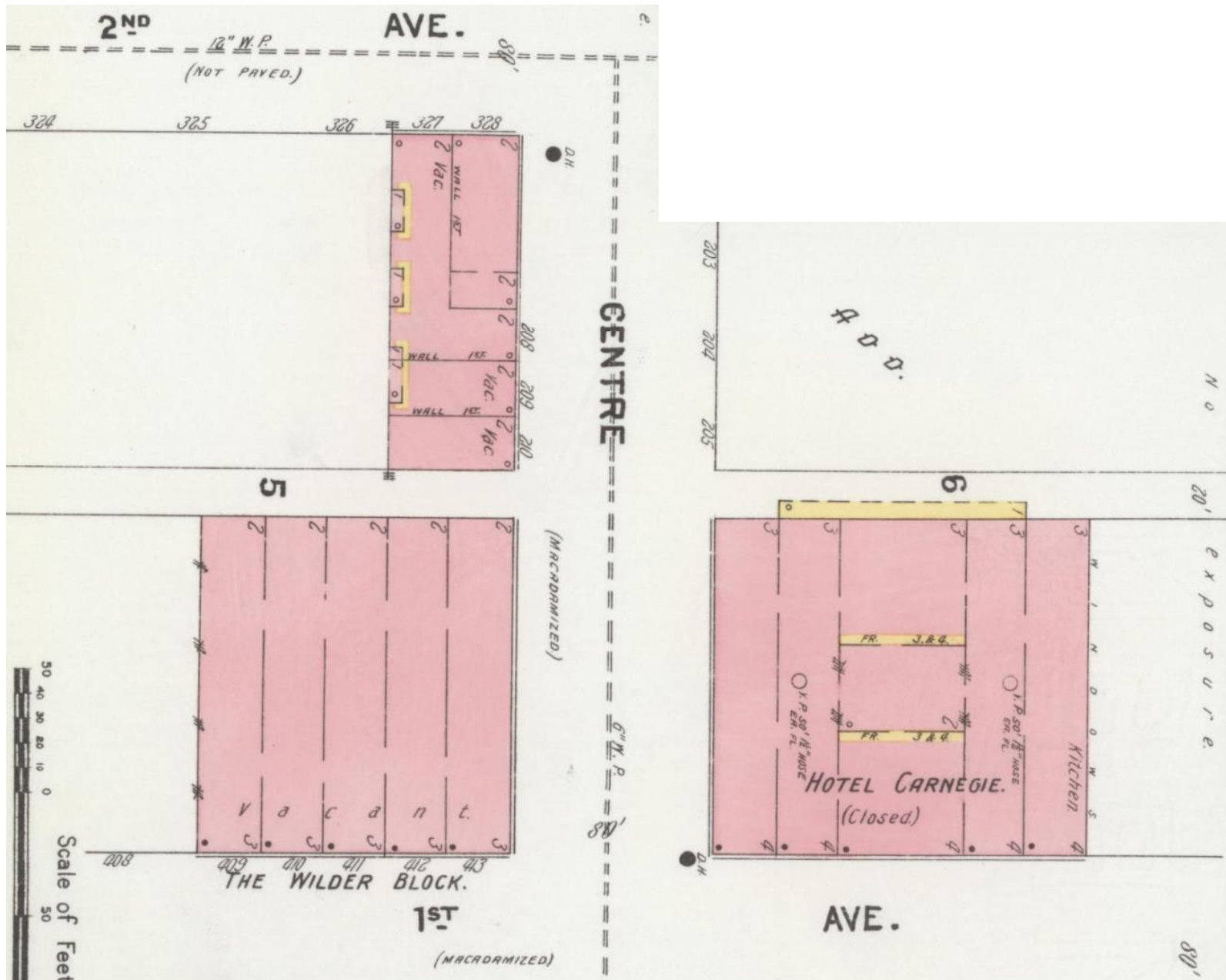
At the intersection of First Avenue and Centre Street, in Carnegie



Map 20. This map depicts “The Wilder Bl’k” “Being Built” and the “Hotel Carnegie” “Being Built”, from left to right, with the Singhiser and Chandler block “Being Built” behind (north) of the Wilder Block, in January 1891.²⁴⁹



Map 21. This map depicts “The Wilder Block” “Vacant” and the “Hotel Carnegie” “Not Running”, from left to right, with the Singhiser and Chandler block behind (north) of the Wilder Block, in May 1897.²⁵⁰



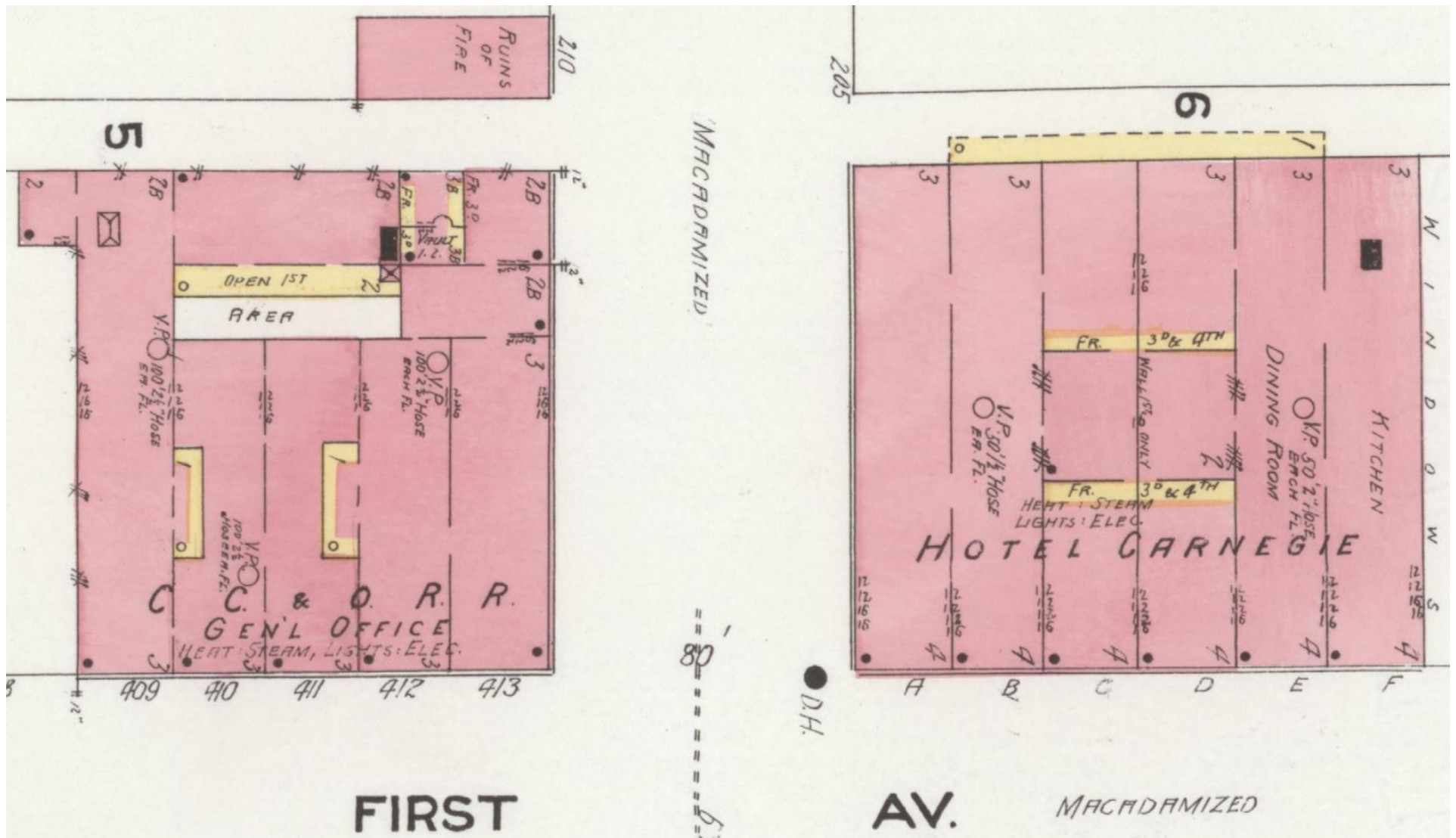
Map 22. This map depicts “The Wilder Block” “Vacant” and the “Hotel Carnegie (Closed)”, with the Singhiser and Chandler block behind (north) of the Wilder Block, in April 1903.²⁵¹



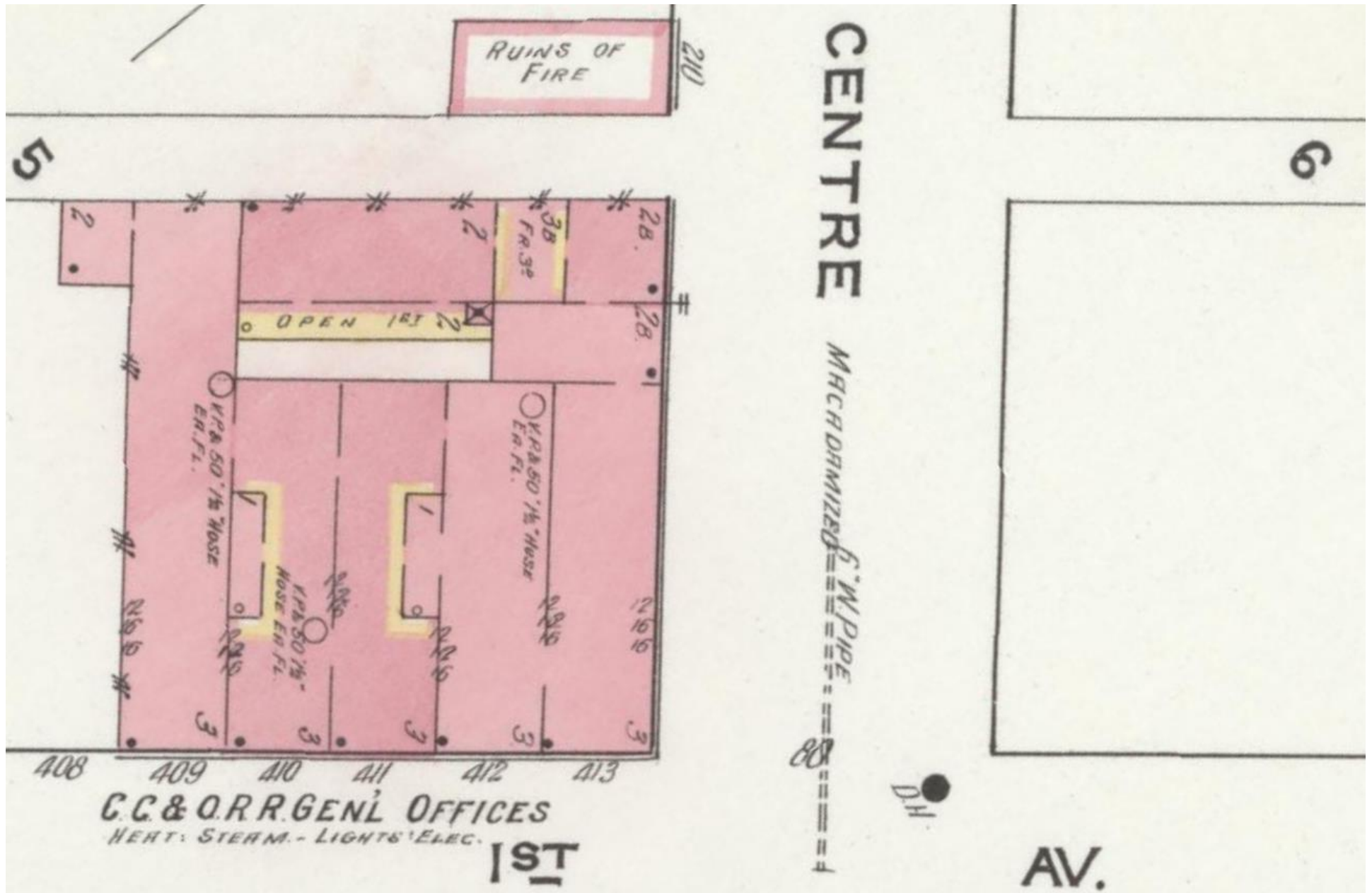
Figure 51. “The Carnegie Hotel [right]. The Carnegie Addition, located northeast of Johnson City, was planned and implemented by the Carnegie Land and Improvement Company led by famed Civil War general John T. Wilder. The lavish 125-room Carnegie Hotel, built in 1891 at the southwest corner of Broadway and Second (Fairview) Avenue for \$125,000, was but one of the many features planned for this development, but much of it was never realized due to the Panic of 1893. The hotel burned to the ground April 3, 1910. (Frank Tannewitz Jr. collection.)” This view shows the south sides of these buildings, which face First Avenue and are separated by Centre Street. The buildings on the right side of the photograph, in the rail yard, appear to be those begun by the 3C’s Railroad. It appears that the small stone building, in front of the Hotel Carnegie, was roofed over in 1905 for use as a railroad carpenter shop. Given that the Hotel Carnegie burned down in 1910, this image appears to date to between 1905 and 1910. The unfinished structure, near the tree on the right, was supposed to serve as the 3C’s depot, see Map 18. The locomotive in the center of this photograph appears to be either a Class A-4 or Class A-7 0-6-0 of the Southern Railway. Also note that there is an additional locomotive directly behind this 0-6-0.²⁵²



Figure 52. A similar view to that given in Figure 51.²⁵³ Note unfinished 3C's/O.R. & C./S. & W. depot building on the left edge of the image, as well as the Singhiser and Chandler block behind the Wilder block. Since the Singhiser and Chandler block burned down in 1907, this photograph was taken in 1907, or earlier.



Map 23. This map depicts the “C.C. & O.R.R. Gen’l Office” in the Wilder Block and the Hotel Carnegie, from left to right, at First and Centre avenues, in December 1908. Note that the Singhiser and Chandler block behind (north) of the Wilder Block, has become “Ruins of Fire”, which occurred in 1907.²⁵⁴



Map 24. This map depicts the “C.C. & O.R.R. Gen’l Office” in the Wilder Block in July 1913. Note that the Hotel Carnegie is not depicted, having been destroyed by a fire in 1910.²⁵⁵ Regrettably, this area was not covered by the February 1920 Sanborn Insurance Company map set.

6.1. "Wilder Block", 1889 – 2008

Major Tenants:

South and Western Offices, 1906 – 1908

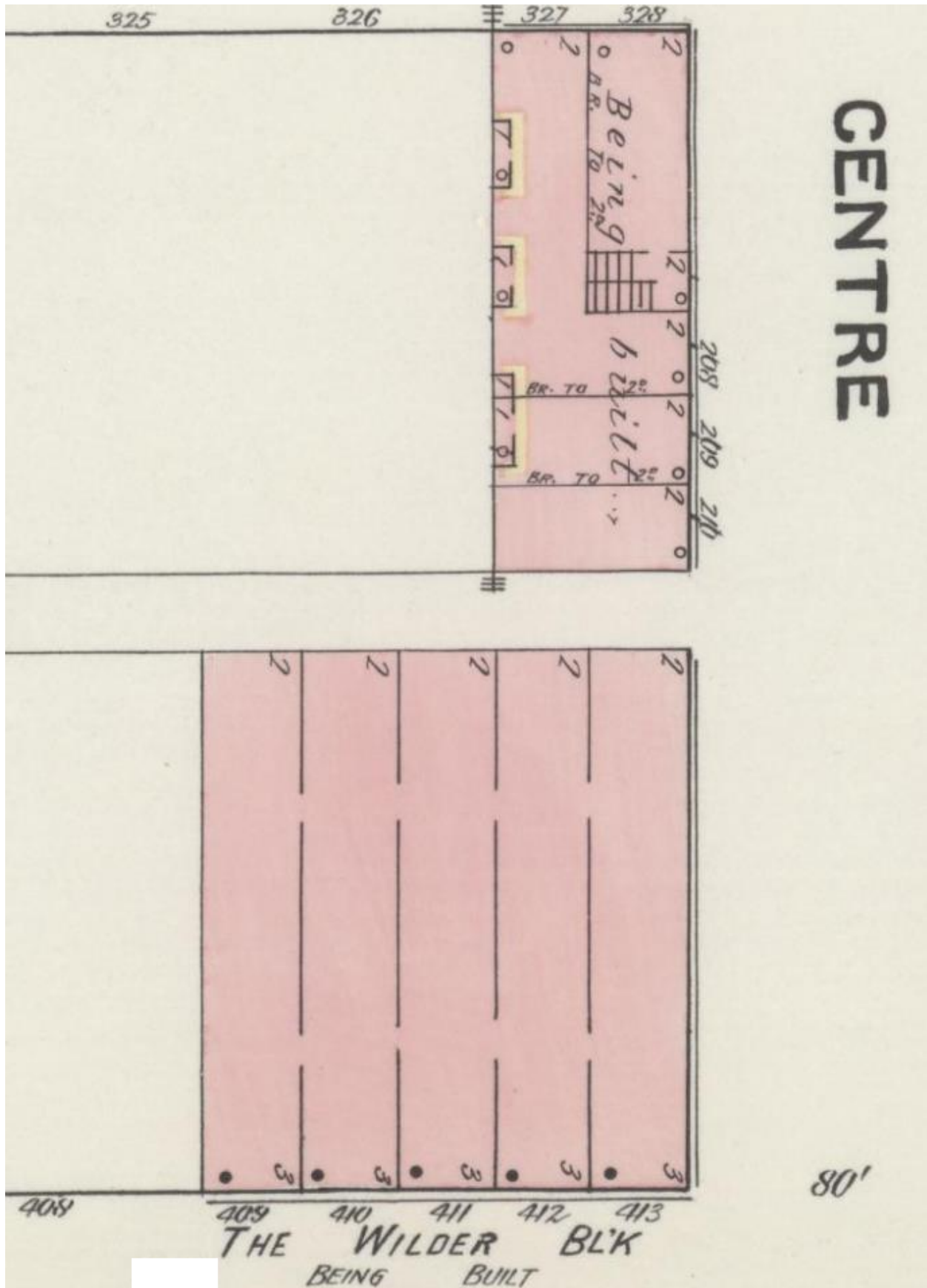
Clinchfield Offices, 1908 – 1914

Empire Chair Company, 1916 – 1935

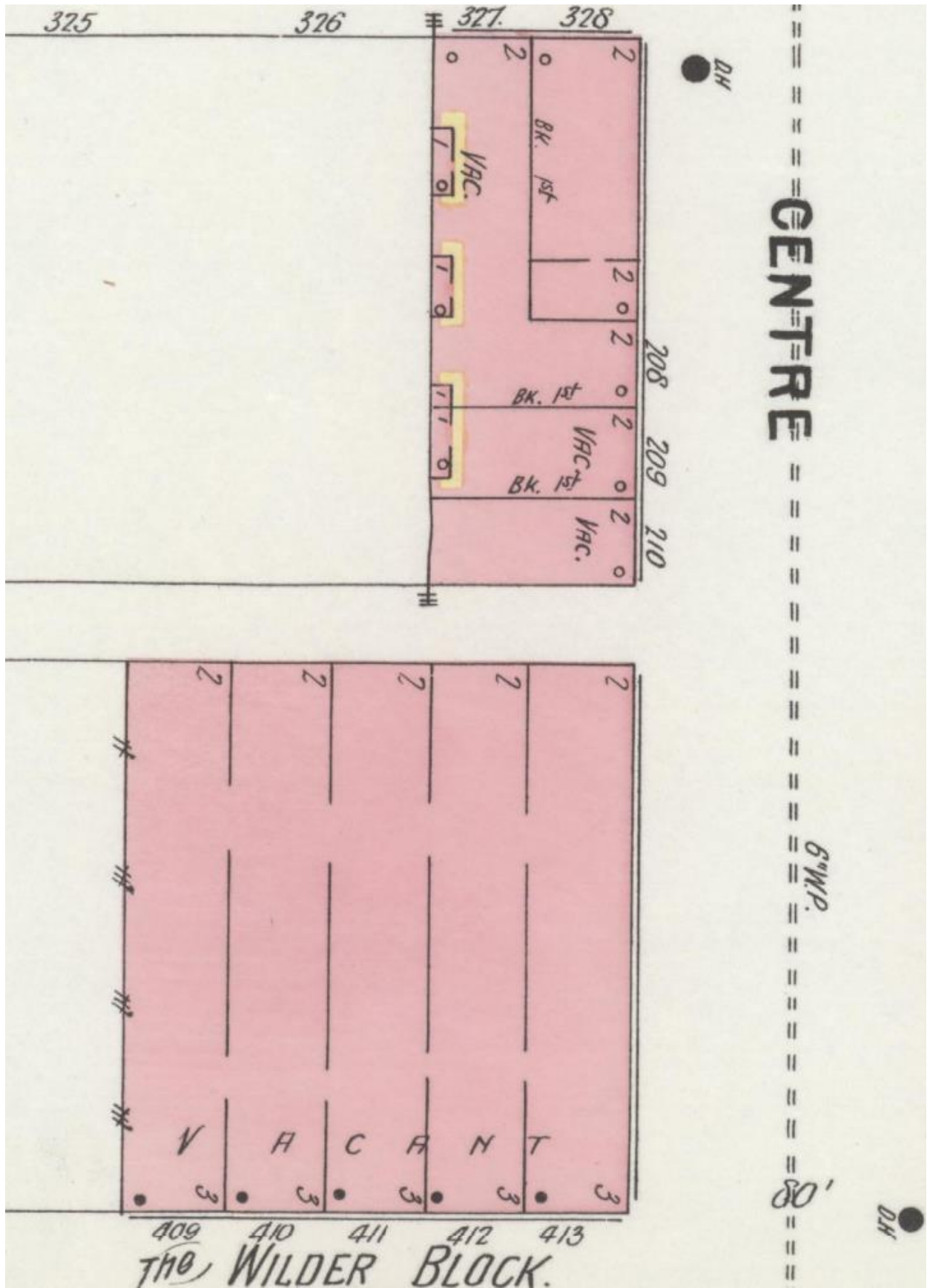
Empire Furniture Company/Corporation, 1935 to at least 2006

Served by a Southern Railway spur beginning around 1917 into at least the 1950s.

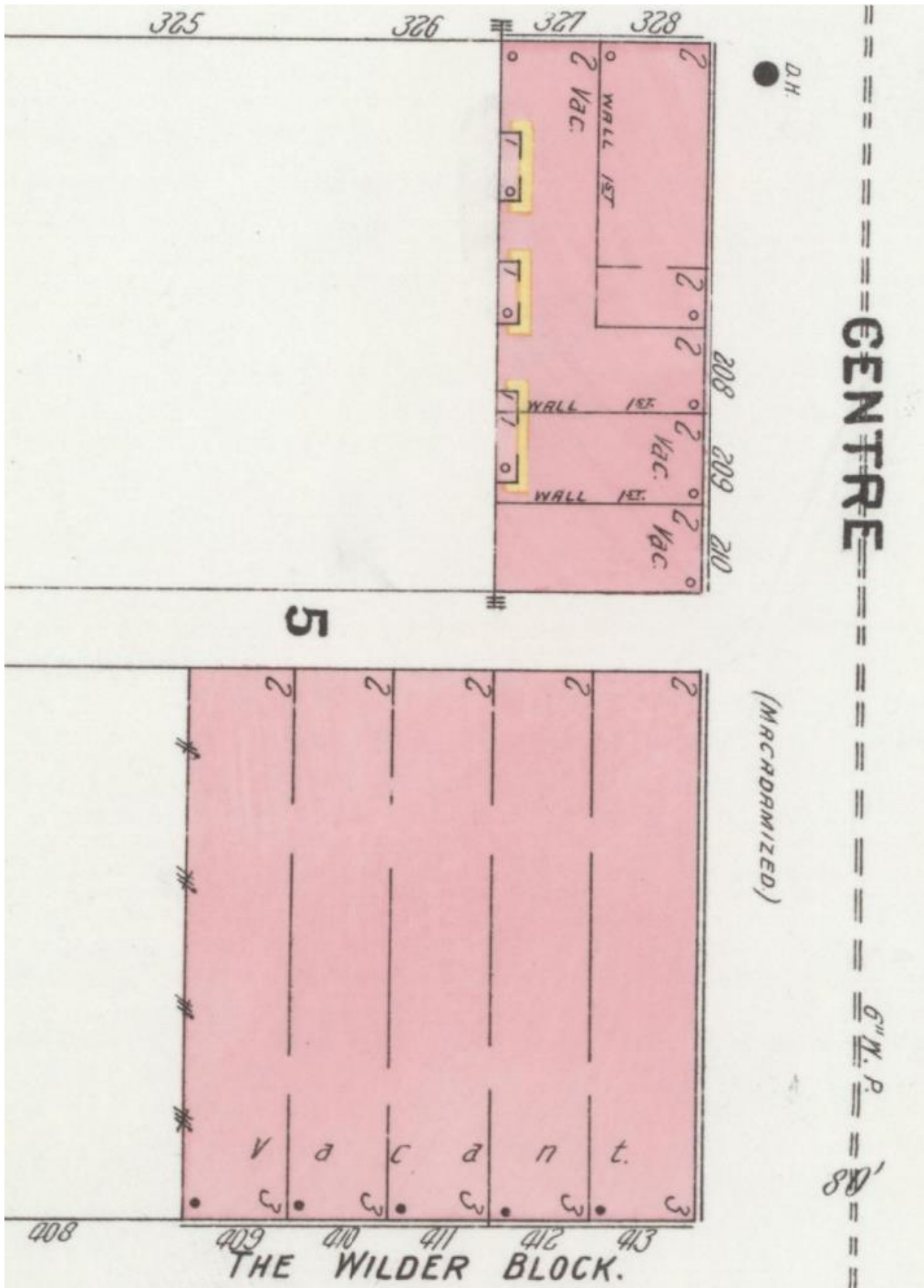
Demolished in 2008.



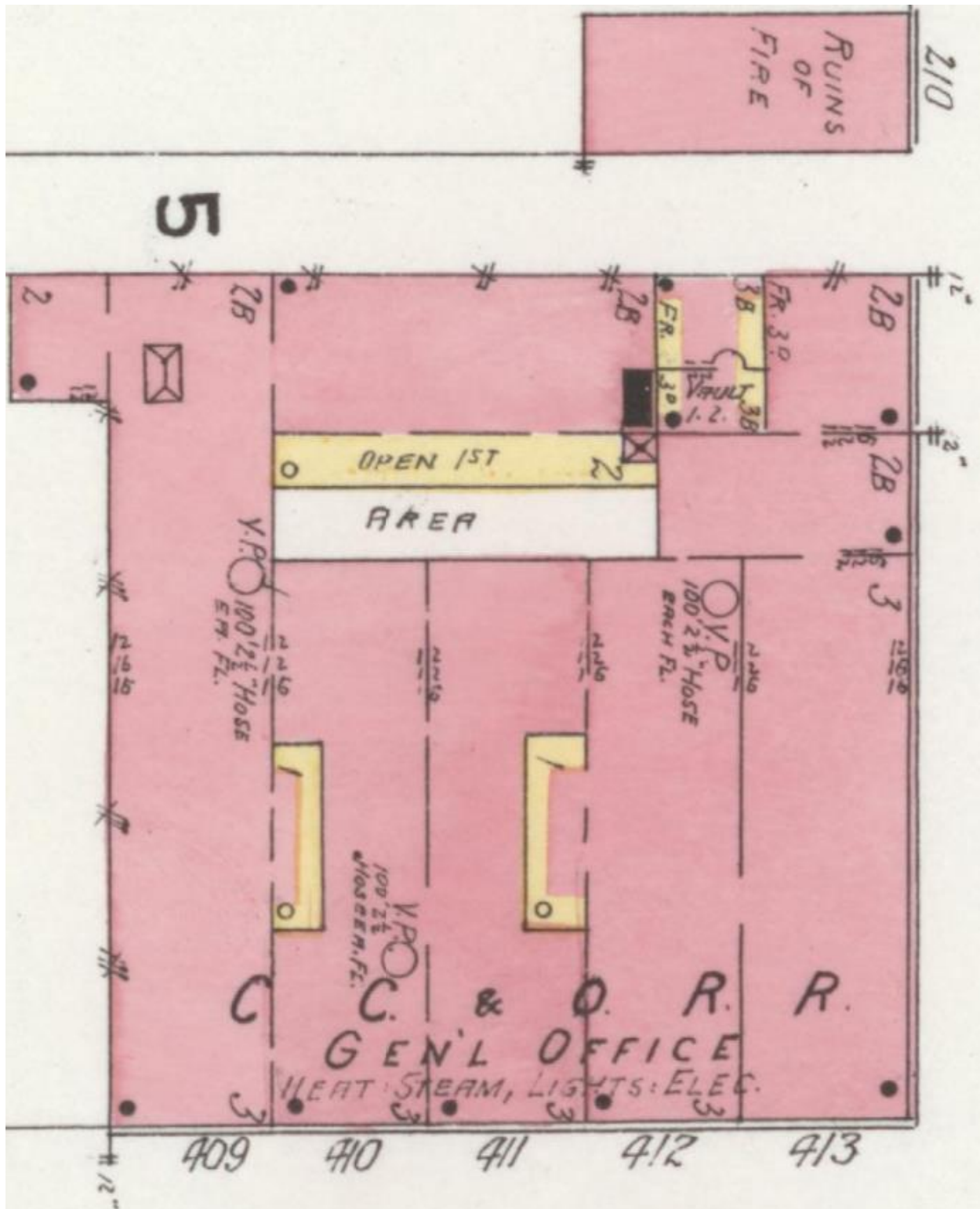
Map 20A. A detail of "The Wilder Bl'k" "Being Built" and the Singhiser and Chandler block "Being Built" behind (north) of the Wilder Block, in January 1891.



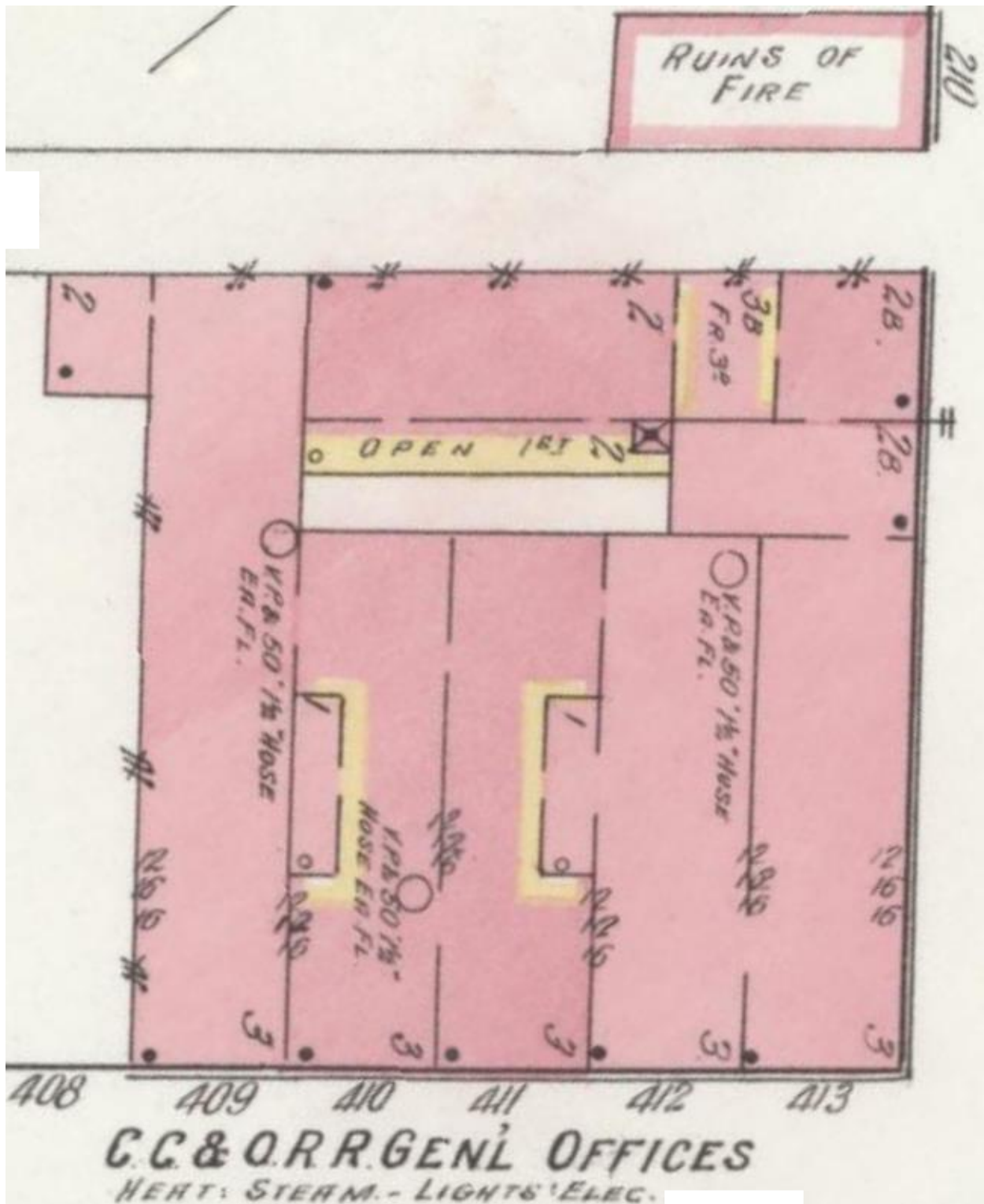
Map 21A. A detail of "The Wilder Block" "Vacant" and the Singhiser and Chandler block (mostly vacant) behind (north) of the Wilder Block, in May 1897.



Map 22A. A detail of "The Wilder Block" "Vacant" and the Singhiser and Chandler block (mostly vacant) behind (north) of the Wilder Block, in April 1903.



Map 23A. A detail of the "C.C. & O.R.R. Gen'l Office" in the Wilder Block, with the Singhiser and Chandler block behind (north) of the Wilder Block, reduced to ruins by a fire in 1907, in December 1908. Note the small addition to the west (left) side of the Wilder building.



Map 24A. A detail of the "C.C. & O.R.R. Gen'l Office" in the Wilder Block in July 1913.

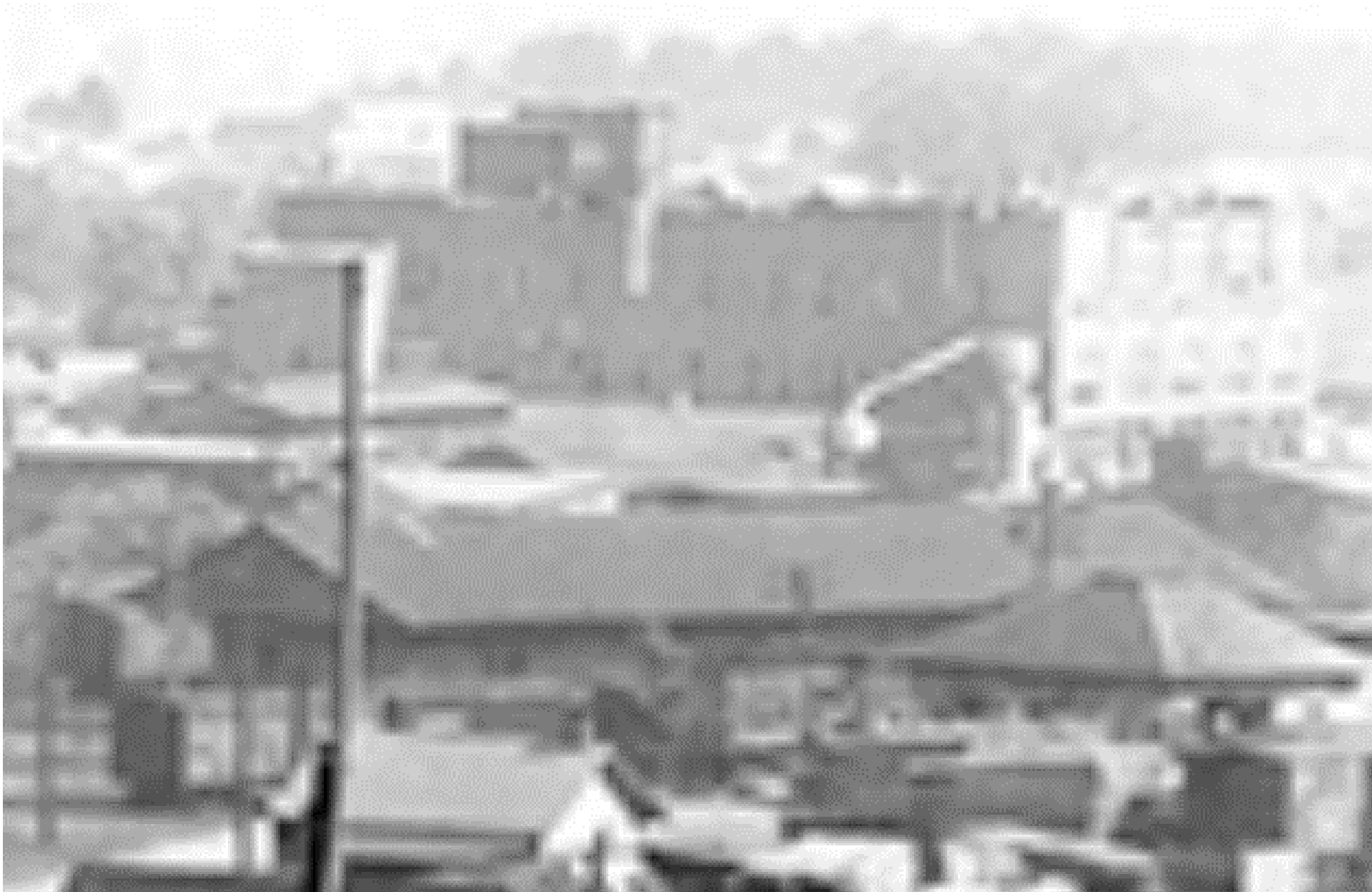


Figure 23F. This detail depicts the Wilder block, with the “old C.C. & O. Depot” and the office of the “Southern Bending Corporation” in front of it. Also note the freight cars in front of the Wilder Block, as well as the cyclone adjacent. This photograph appears to date to between 1920 and 1925.²⁵⁶



Figure 30F. A detail of the Wilder Block, note the freight cars adjacent to the building, indicating that the Empire Chair Company owned the building at the time this photograph was taken. This photograph appears to date to between 1920 and 1925 and, given the lack of a cyclone, appears to predate Figure 23F.



Figure 51B. A detail of the Wilder block. Note the box cars of the V. & S.W. and the Southern, as well as the hoppers of the Clinchfield. This image appears to date to between 1905 and 1910.

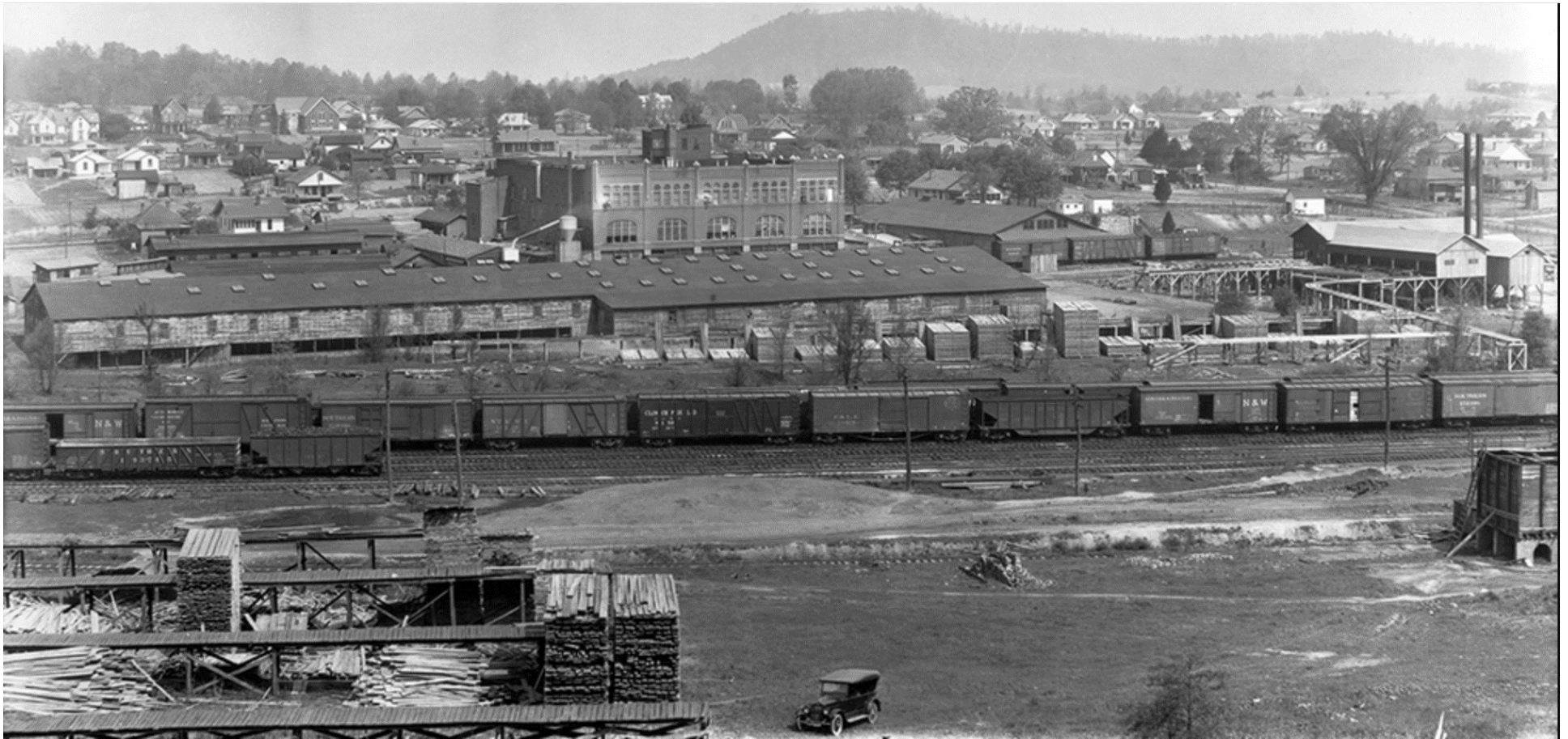


Figure 53. “McCorkle Lumber Company, Empire Furniture Company, Broadway Street and Southern Railway, Carolina, Clinchfield & Ohio, Carnegie”.²⁵⁷ Note, the Empire Chair Company had occupied by the “Wilder Block”, the multi-story building near the center of this photograph (see, for example, figures 40B, 41A, 43 and 46A) in 1917 and that the Empire Chair Company did not become the Empire Furniture Company until 1935. Consequently, it appears that the McCorkle Lumber Company occupied the facilities that had been built upon the site previously occupied by the Hotel Carnegie, on the right of this photograph, from 1925 to 1928. Also note the lumber yard in the lower left of this photograph appears to be the one associated with Unaka Flooring Company plant (which was previously owned by the Southern Bending Company); while the structure in the lower right of this photograph appears to belong to the Johnson City Shale Brick Company.



Figure 53B. A detail of a portion of the Empire Chair Company plant, adjacent to the Wilder block, during the period 1925 to 1928.



Figure 53C. A detail of the Empire Chair Company, which occupied the Wilder Block during the period when this photograph was taken, 1925 to 1928. The low building in the foreground appears to be part of the Empire Chair Company.²⁵⁸

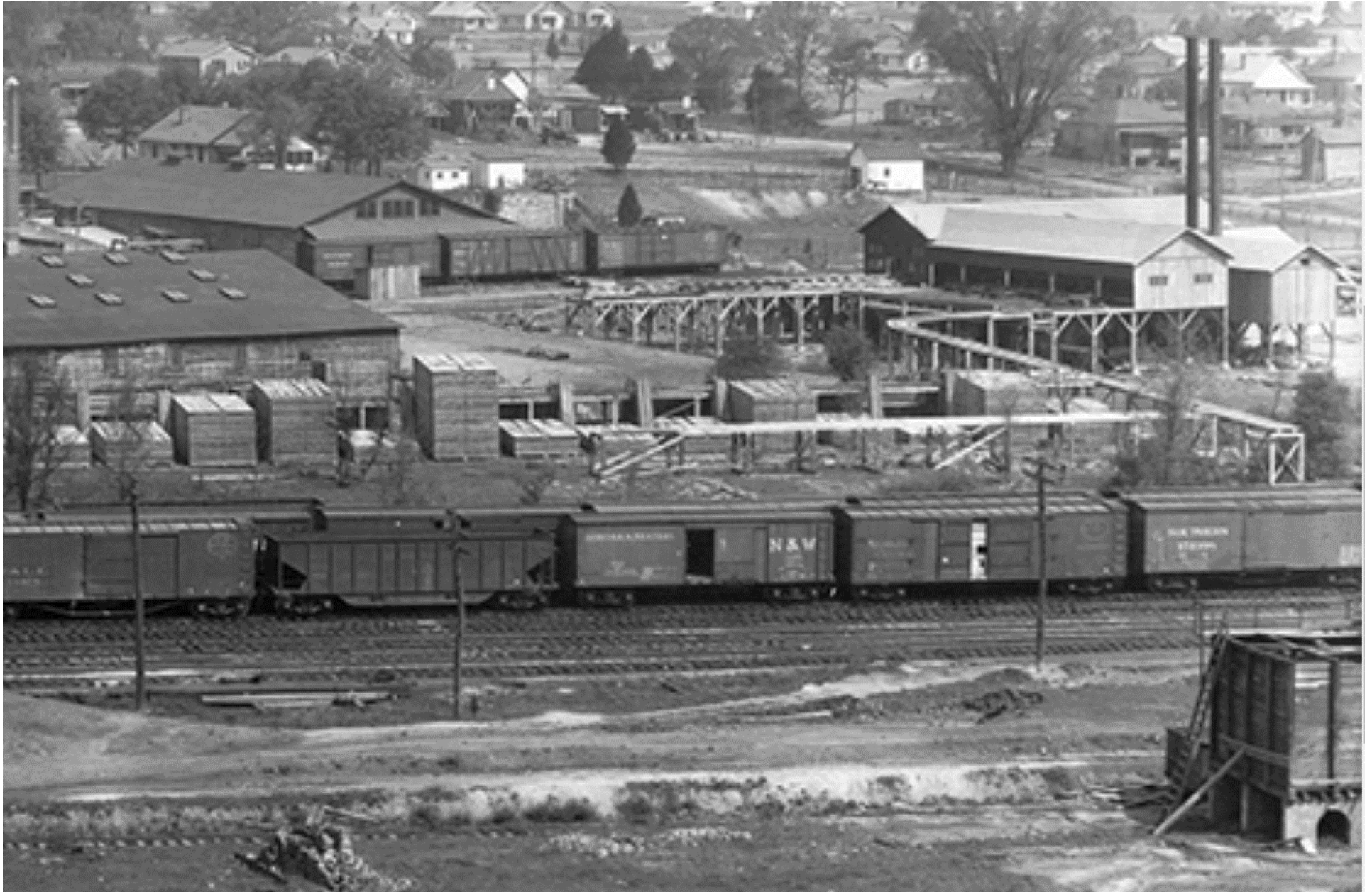


Figure 53D. A detail of McCorkle Lumber Company, taken between 1925 and 1928. Note the structure in the lower right of this photograph appears to belong to the Johnson City Shale Brick Company.



Figure 39A. A detail from a photograph taken in or after 1938. Note the building in front of the Wilder Block, which appears to be the same as that in Figure 53C.



Figure 54. This is the “Wilder Block” or “Wilder Building,” which housed the General Offices of the Carolina, Clinchfield & Ohio Railroad from 1908 until 1914. It appears that this photograph was taken shortly after this building was constructed in 1891 and 1892. This building was located west across Centre Street from the Carnegie Hotel.²⁵⁹ Behind the Wilder block, to the right, can be seen the Singhiser and Chandler block, which was destroyed by a fire in 1907.



Figure 54A. A detail of the “Wilder Block” or “Wilder Building.” It appears that this photograph was taken shortly after it was constructed in 1891 and 1892.

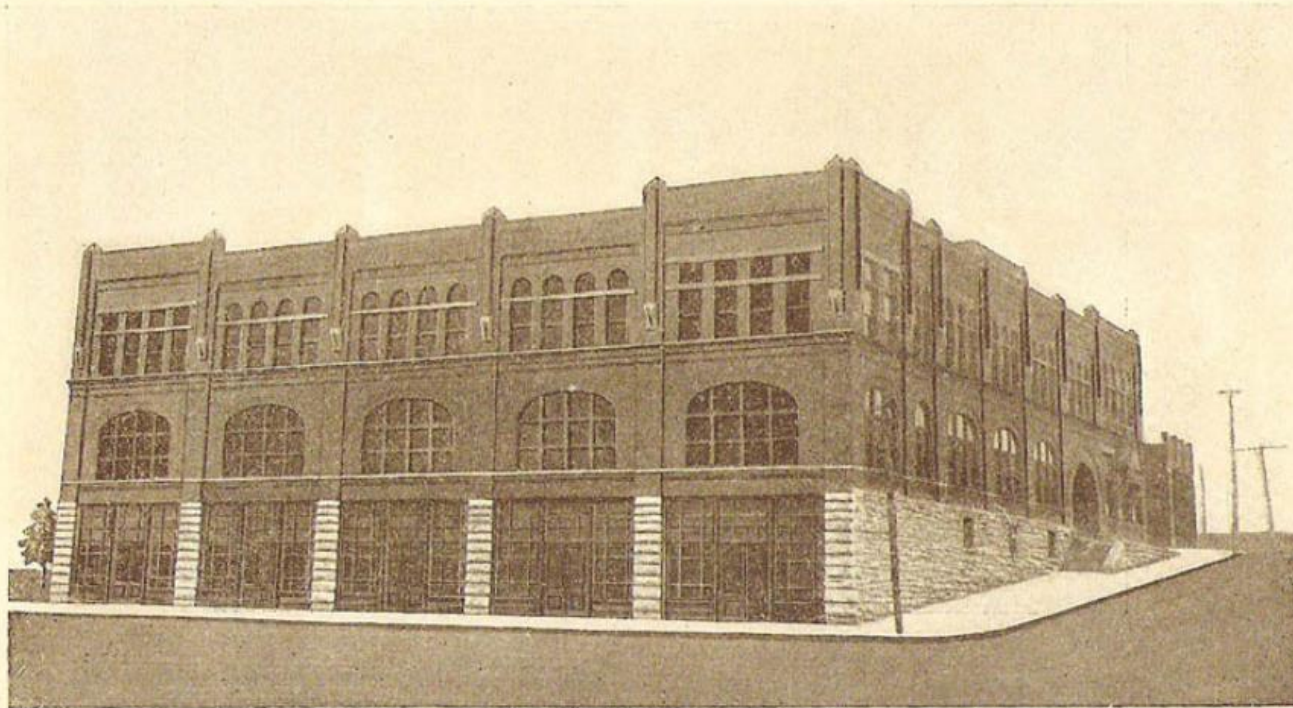


Figure 52A. A detail of the “Wilder Block” or “Wilder Building.” Behind the Wilder block, to the right, can be seen the Singhiser and Chandler block, which was destroyed by a fire in 1907.

EMPIRE CHAIR COMPANY

ELIZABETHTON, TENNESSEE

JOHNSON CITY, TENNESSEE



Ten years ago the Empire Chair Co., employed two men and two boys. Today they employ 180 people. Their factory has 150,000 feet of floor space. Material is handled by us, from the stump to the finished article. We have our own sawmill, of 15,000 feet daily capacity.

PAGE FIFTY-TWO

Here we make our own Quartered Oak; also much other material. All our Quartered Oak is White Oak, carefully selected, which means a beautiful, distinct flake or grain. All the lumber we use is air dried before going to the Dry Kilns—the only way to properly prepare furniture lumber.

Figure 55. A “1917 ad for Empire Chair Company”, note the lack of a railroad spur in front of the Wilder block.²⁶⁰

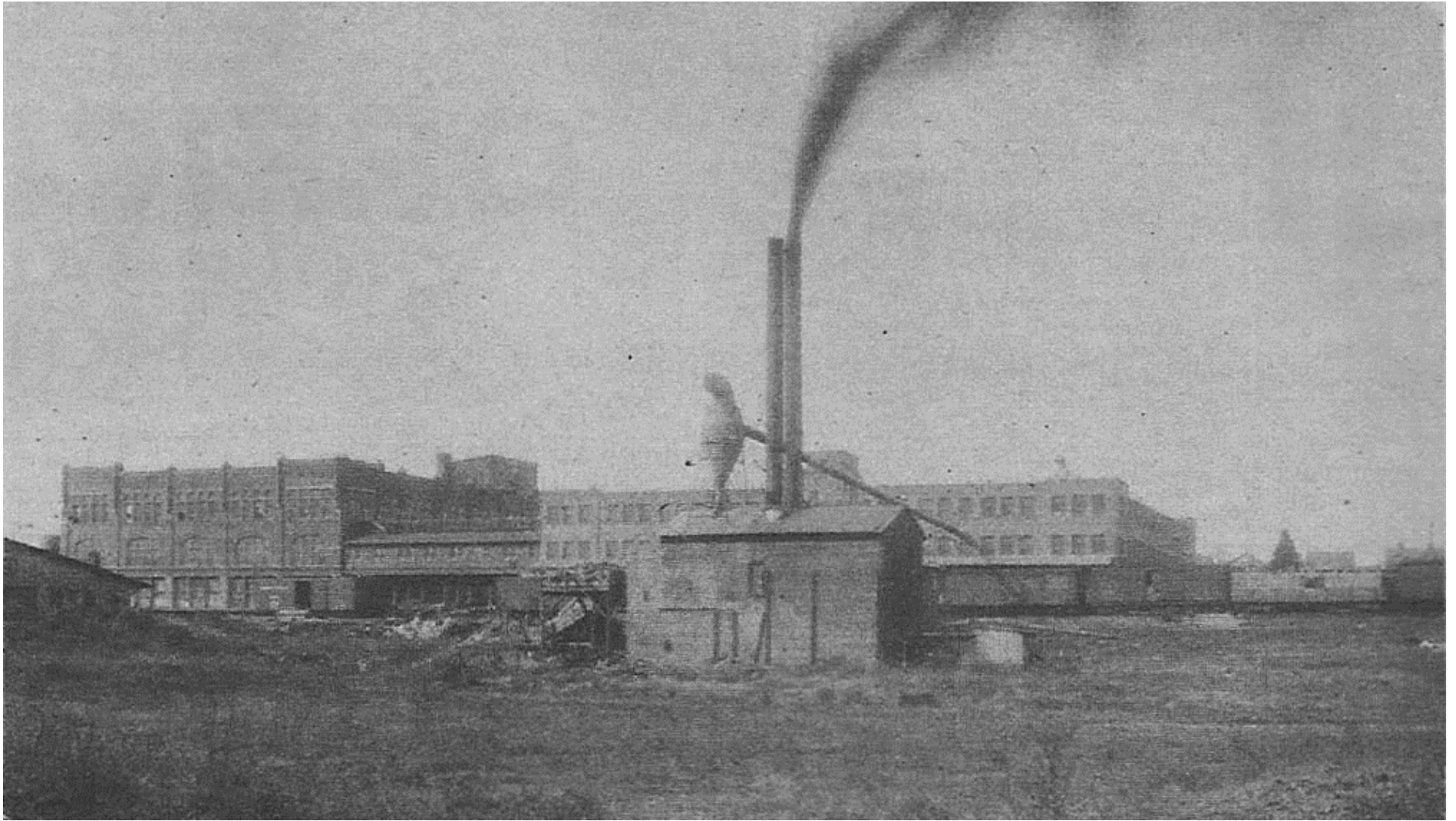


Figure 56. A “1930 photo of Empire Furniture”/Chair Company.²⁶¹ Note that the McCorkle Lumber Company plant appears to have been removed after 1928, and that the Empire Chair Company has expanded into the area that it had occupied, see figures 42F and 47. Also note the building on the left, which appears to have been part of the Johnson City Shale Brick Company. Also note what appears to be a boiler house (with a cyclone and two smokestacks) in the center of this photograph, which appears to be the same building depicted in Figure 57B, although significantly modified.



Figure 57. A “1950 aerial photo of Empire Furniture.” Although most of the railroad tracks in this area were on the opposite side of the stream in the left foreground, note the four boxcars sitting on the spur that runs in front of the Wilder block.²⁶² Also note the building on the left (in front of the Wilder Block), which appears to match that in Figure 53C.



Figure 57A. A detail of the Wilder block, which was occupied by the Empire Furniture Company at the time this photograph was taken in 1950.



Figure 57B. A detail of the buildings owned by the Empire furniture Company, which had been built on the land previously occupied by the Hotel Carnegie and the McCorkle Lumber Company. Note what appears to be a boiler house, see Figure 56.



Figure 57C. Another detail of the Empire Furniture Company complex in Carnegie, note that it appears that the street on the right is Broadway.



Figure 58. June 1999, Wilder block “CC&O Office Building in Johnson City”. Note that the railroad spur that previously ran to this building has been removed.²⁶³



Figure 59. A “recent” view of the south face of the Wilder block.²⁶⁴



Figure 60. A “recent” view of the west face of the Wilder block.²⁶⁵



Figure 61. A “recent” view of the east face of the Wilder block. The “Remaining portions of this building were demolished in 2008.”²⁶⁶



Figure 62. “Empire Furniture, Men Working”, undated.²⁶⁷



Figure 63. "Empire Furniture, Man Working", undated.²⁶⁸



Figure 64. "Empire Furniture, Men Working", undated.²⁶⁹

“Wilder Block”, 1889 – 2008

Tenants:

South and Western Offices, 1906 – 1908

Clinchfield Offices, 1908 – 1914

Empire Chair Company, 1916 – 1935

Empire Furniture Company/Corporation, 1935 to at least 2006

Demolished in 2008.

6.1. The Early Years of the Wilder Block, 1889 – 1906

13 June 1889. Johnson City Comet.

“Gen. Wilder has contracted to build a block of six storehouses in the Carnegie Addition. Bricks are now being made, and work will commence on the buildings early in the fall.”

31 October 1889. Johnson City Comet.

“The first block of buildings in the Carnegie addition, was commenced Monday. The block is Gen. T.J. Wilder’s, and will contain six store rooms on the corner of Second avenue and Center street. The rooms will be large and nicely furnished, and the buildings are to be heated with steam, and fitted with steam elevators. This is only the beginning of the buildings in the Carnegie addition...”

7 November 1889. Johnson City Comet.

“Still Stepping – Three Million Brick Contracted for. Wholesale Houses – New Buildings. – Is Johnson City going to Build? We answer the question by stating that contracts have been closed for three million brick by one man.

Gen. J.T. Wilder closed a contract with S.P. & J.J. Ivins, of Athens, last Friday for one million brick and the gentlemen will move their brick machine, one of the finest in the State, and capable of making 30,000 a day, to Johnson City at once and be ready to begin work as early in the spring as possible. The brick are to be used next summer.

P.C. Hoss & Co. have a contract for another million for the General and have their machine at work now making them in the Carnegie addition,

The other million is contracted for with a brick company of Florence, Ala. All these brick will be used in the Carnegie addition next year.

Gen. Wilder has contracted for the erection of a block of six store-rooms 100 x 140 feet and four stories high. Beaver & Hoffmaster, of Knoxville, are the architects. He will also build a brick warehouse 100 x 140, in the rear of the block near the line of the Three C’s road...”

14 November 1889. Johnson City Comet.

“The Carnegie company, with two thousand acres, has taken the lead. The streets in this addition are broad and are being permanently graded. The 3-C depot will be there – and all others eventually. The company is providing liberal things; will build a hundred thousand dollar hotel

next year; donate a large market square, and throw open an immense park, of natural forest. Already foundations are being laid for four story business blocks, and the brick are making. Gen. Wilder says that the company will give a site and water [to?] every brick machine that comes along, and make a contract with the proprietor for a million brick. The General added: 'We expect to lay twenty million brick next year.'

Some of the local speculators have done well..."

16 January 1890. Johnson City Comet.

"New Buildings... Kegley & Eversole are rapidly placing rock for the foundation of Gen. Wilder's block of six store houses in the Carnegie addition..."

6 February 1890. Johnson City Comet.

"Mr. W.C. Hoffmeister, of Knoxville, member of the firm of Beaver & Hoffmeister, architects, was in the city Monday. He spent Sunday in Elizabethton, the home of his childhood. Mr. Hoffmeister is very much pleased with Johnson City, and will likely establish a branch office here. While in the city Monday he closed a contract with General Wilder for one of his new blocks in Carnegie, and also some other work for Johnson City..." However, this contract appears to have fallen through.

6 February 1890. Johnson City Comet.

"Ground was broken Monday for the foundations of Gen. Wilder's block of 6 store rooms. The rock for the foundation are on the ground and the work will be pushed rapidly.

A large force are at work excavating for the Carnegie Land Company's general offices. The buildings will be opposite the hotel and will be a handsome structure 4 stories high and covering a space of 100 x 150."

24 April 1890. Johnson City Comet.

"Started with a Rush. A list of Factories, Hotels, Business and Dwelling Houses Completed and Under Construction Since January 1st, 1890. – Nearly a Million dollars Already Spent in Building... J.T. Wilder's Stone Block 60,000..."

17 July 1890. Johnson City Comet.

"Our New buildings. – 38 Business Houses and 75 Dwellings now in Course of Construction. – No Town in the State can Show a Growth so Great... The stone foundation for Gen. Wilder's block of six storerooms on Second avenue and North Main street is completed, and the brick work will be commenced in a few days..."

Singiser & Chandler are excavating for a block on the corner of Second avenue and Centre street. It is to be 50 x 140 feet, two stories and basement.

The foundation walls for Gen. Wilder's stone building, 100 x 140, four stories high, are being rapidly built, and the Kentucky sand stone for the three upper stories is arriving daily. It will be one of the handsomest as well as most expensive private buildings in the State. The Carnegie Land Company will have their general offices in it..."

24 September 1891. Johnson City Comet.

“The Wilder Building. – A Magnificent Structure – General Wilder’s Efforts. The outer walls of the great Wilder building in Carnegie will be completed in four days, immediately after which the roof will be put on.

For its size this building has been constructed most rapidly, and yet the workmanship is first class.

This building is almost as large as Hotel Carnegie and is as fine if not finer in many respects. The walls have been built of pressed brick, made by the Standard Press Brick Company of Chattanooga. These brick will compare favorably with those manufactured elsewhere in the United States. The building is made up as follows: Five commodious store rooms, one very large bank room, sixty-six office rooms and a neat hall 30 x 72 feet.

There is now in Carnegie office rooms and other structures sufficient to accommodate a population of 5000. In fact it is a new city of fine proportions. It is a pleasing spectacle to the eye of man, and all he has to do is to go up and possess it.

General Wilder has done more to build up our city than any other man and he deserves the praise of a united people. His efforts have not been made for nothing. The population will increase. Men of capital seeing these mighty works will know that our town is characterized by spirit and enterprise and they will not hesitate to invest. If our people will ever co-operate with Gen. Wilder, they can hope very soon to have a city of gratifying proportions and splendid quality.”

19 November 1891. Johnson City Comet.

“Work continues on the interior of Wilder’s large business block. The plastering is being done, and the structure will be finished ere one is aware.”

4 February 1892. Johnson City Comet.

“\$1,288,051. – These are the Startling figures will Tell of the Growth and Activity of the Plucky, Magnetic City – In 1891 – Complete List of Improvements during the Year... More than a million and a quarter of dollars have been expended in plants, business blocks, school houses, residences, street improvements, water works, railroads, etc... J.T. Wilder, block [\$] 60,000...”

25 February 1892. Johnson City Comet.

“Carnegie was decorated with flags of the union on Patriot’s day. An immense flag was suspended between the hotel and Gen. Wilder’s great building.”

17 March 1892. Johnson City Comet.

“Nearly Finished, is the great Wilder Building in Carnegie. Gen. Wilder’s great business building in Carnegie is nearly finished. Without doubt it is among the best constructed buildings in East Tennessee. The work is much neater than that of the Carnegie Hotel.

There are 65 rooms in this building, and there is not one but what is well lighted and well ventilated. All are elegantly finished and provided with every convenience.

The building is provided with electric lights, etc., on every floor.

It is divided into office, bank and business rooms.

The Carnegie Land company recently moved in, and are using three rooms in the eastern corner.

Quite a number of the rooms have been rented, and will soon be in use.

The rooms on the upper floor have yet to be plastered.

A large force of hands are kept constantly at work, and it will be only a short while until the structure is completed.”

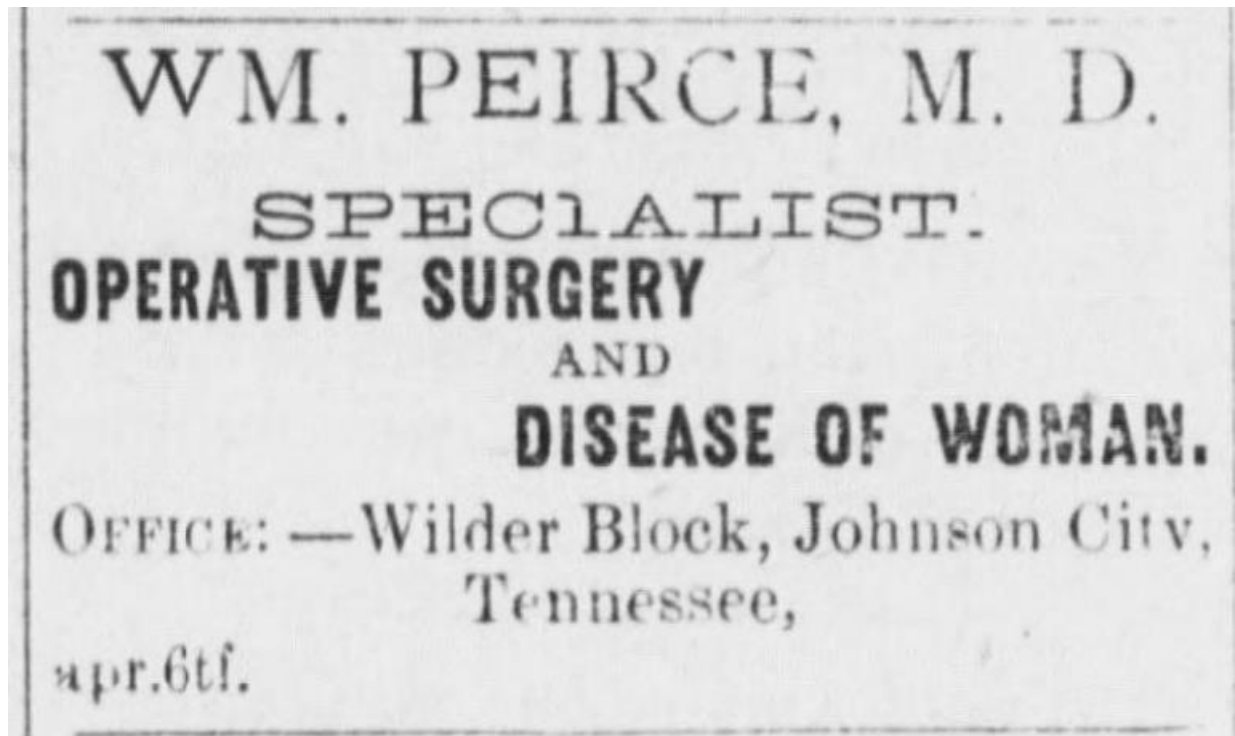


Figure 65. Johnson City Comet, 5 May 1892.

13 October 1892. Johnson City Comet.

“The Episcopal Minister. The Episcopal mission at this place has secured the services of Rev. Francis T. Berry, and he has taken charge of the work... The public are invited to attend the services every Sunday morning at 11 o’clock in the Wilder building at Carnegie.”

May 1897. The Sanborn Insurance Company Map lists the Wilder block as “Vacant”.

7 March 1901. Johnson City Comet.

“Will Move to Johnson City. The headquarters of the Virginia Iron, Coal and Coke Co. and the Virginia and Southwestern Railroad company will move to Johnson City. A prominent official of the companies was in the city last week and stated that arrangements were about completed to lease the large building in Carnegie, known as the Wilder block, for the general offices of the companies. This step is made necessary by the enormous rents charged at Bristol. The building selected here is a handsome three-story block 100 x 149 feet. The second and third floors are cut into splendid office rooms with plenty of light and ventilation. It is one of the handsomest and best finished buildings in East Tennessee and is admirably adapted to the purposes of companies. The four large store rooms on the first floor will make fine rooms for the general purchasing agent’s departments. Anyone at all familiar with the building and surroundings will see at once the wisdom of these people in selecting this site.”

April 1903. The Sanborn Insurance Company Map lists the Wilder block as “Vacant”.

2 February 1905. Johnson City Comet.

“Trustee’s Sale. – By virtue of a trust deed, securing payment of purchase money to S.J. Kirkpatrick, S.C. Williams and heirs of S.M. Arnell, deceased, executed to the undersigned as Trustee by Carnegie Mercantile Company, on December 16, 1904, registered T.D. Book, 17, page 523, at Jonesboro, default having been made in matter effecting insurance and keeping the building below recited insured, entitling to foreclosure, and such default having been declared, we will proceed to sell in foreclosure in front of Jobe’s Opera House in Johnson City, Tennessee, on Monday, March 6, 1905, within legal hours, the property so conveyed in trust, to-wit: Lots 12 (twelve), 13 (thirteen), 14 (fourteen), 15 (fifteen) and 16 (sixteen) in Block 5 (five), of Carnegie Land and Improvement Company’s addition to Johnson City, in ninth Civil District of Washington County, Tennessee, on which stands the building known as the Wilder block.

Terms of Sale:

Cash in hand, in bar of equity of redemption. The Banking and Trust Co., Trustee. This January 31, 1905.”

4 October 1906. Johnson City Comet.

“Wednesday Night’s Fire. The Hayes Department Store in Carnegie Destroyed. About ten o’clock Wednesday night fire was discovered in the department store of J.E. Hayes in Carnegie and it gained such headway before the department arrived that it was impossible to save anything. The store was located in the old Singhiser & Chandler block, erected during the boom but never used profitably until the last few years. It cost about \$15,000, and was sold by a building and loan association for about \$400. It later passed into the hands of Judge S.J. Kirkpatrick, the present owner, who carried \$3,0000 insurance upon it. Mr. Hayes carried about \$10,000 worth of goods and only \$6,500 insurance. He will re-enter business as soon as possible.

Johnson City’s only branch postoffice, Station A, was located in the building and all mail and supplies were destroyed.”

6.1.2. South and Western Offices, 1906 – 1908

4 October 1906. Johnson City Comet.

“Johnson City generally gets what it goes after. It wanted the general offices of the South and Western railroad company and went after them. As a result the Carnegie Hotel and the Wilder block have been leased for twenty-seven months by the company and will be occupied by the first of November or as soon as they can be put in proper repair. We congratulate Mr. Carter and associates upon the wisdom displayed in this decision and felicitate Johnson City upon its victory. It’s no trouble to show Johnson City’s advantages to wise people and win them.”

South and Western Offices Will Come to Johnson City

**Hotel Carnegie and Other Buildings Have Been Leased
and the Transfer Will be Made Within Next
Sixty Days—Means Much to Our City.**

The Bristol Herald of the 5th instant says: "It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter having been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted and otherwise repaired and will afford ample room for the use which will be made of them.

Figure 50 (reproduced here for convenience). Johnson City Comet, 4 October 1906.²⁷⁰

10 October 1906. The Coal Trade Journal.

"It was announced in Bristol this week semi-officially that the principal offices of the Clinchfield Coal Co., Dawson Coal & Coke Co., Virginia - Pocahontas Coal & Coke Co., Crane's Nest Coal & Coke Co., South & Western Ry. Co., and the other business of the George L. Carter and Thomas F. Ryan interests, would, within 26 days, be moved to Johnson City, a small town 25 miles south of Bristol. The reason assigned for leaving Bristol was their inability to obtain sufficient office room under one roof in Bristol. This will be a loss to Bristol of a pay-roll said to aggregate \$25,000 monthly, and about 200 men."²⁷¹

11 October 1906. Johnson City Comet.

“Official Announcement. – South and Western Officials Tell Something of the Plan. ‘After a consultation between Alfred Walter, president of the Seaboard Air Line railway and chairman of the board of directors of the South & Western, and George L. Carter, president of the latter and head of the George L. Carter syndicate, in Bristol last night it was officially announced that the headquarters of the South & Western would be moved to Johnson City within sixty days.

The Carnegie hotel and the Wilder block at Johnson City have been leased by the company and will be used as the general offices of the operating and engineering departments. Everything will be moved from Bristol and it is probable the officials who now reside here [Bristol] will move to Johnson City.

The George L. Carter syndicate, which is building the South & Western, is backed by George L. Carter and Thomas F. Ryan and is one of the largest corporations in this entire region. The construction of the South & Western railway from the coal fields of Virginia to the South Atlantic Seaboard will be completed within two years...

This location of the offices will mean much for Johnson City as the pay roll of the office force alone is said to exceed \$25,000 per month, and the city has been kept continually flooded with laborers and transients connected with the road for two years and much revenue has been derived therefrom.

One reason assigned for leaving Bristol is the lack of office room...

The Carter interests have taken options on nearly all of the available real estate at Johnson City and the immense yards, the largest on the road, will be constructed in the suburbs of the city and options for the same will be taken up as soon as possible, The road will enter the same depot with the Southern and ground has been secured for this purpose at what is considered a very reasonable price.’ – Bristol Correspondent in Knoxville Sentinel.”

18 October 1906. Johnson City Comet.

“Wilder Block Being Remodeled. There is no longer any doubt the offices of the S. & W. road being removed to this city. For awhile it looked like the Bristol people would offer sufficient inducements to keep them, but it was of no avail. The Wilder block is being remodeled and repaired and will be put in readiness for occupancy about the middle of November. An army of workmen are now busy on the big block and it will soon be alive with the officials and clerks of the big railroad that is building from the Ohio river to the ocean.”

6 December 1906. Johnson City Comet.

“South and Western Offices will be moved to Johnson City as Soon as Buildings are in Readiness. The offices of the South and Western railway and allied interests will be moved from Bristol to Johnson City before Christmas, if the buildings that are to be used at Johnson City can be prepared for occupancy by this time. Otherwise, the moving will not occur until about the first of the year.

The offices will be located in the Carnegie hotel building, and the Wilder block at Johnson City, and extensive repairs on these buildings have been in progress since the first announcement of the decision of the officials of the company to move the offices from Bristol.

It is said that no further trouble is being encountered by the employes of the company in securing homes at Johnson City, and they are finding desirable houses at reasonable rates.

The moving of the many offices will be a stupendous undertaking, and will require several days time. – Bristol Herald.”

30 May 1907. Johnson City Comet.

“Department Store Burned. The department store of J.E. Hayes was destroyed by fire Tuesday morning just before daylight. The building is located in Carnegie on the corner of Carter [Centre?] street and Second avenue, opposite Hotel Carnegie and just back of the Witder [Wilder] block now occupied by the S. & W. officials. There was nothing saved, the flames came pletily [completely] filled the building, when the fire was discovered and it was impossible to save anything. The Witder block was slightly damaged and fane [?] blue print roon [room] on the roof was totally destroyed. The building occupied by Hayes belonged to his sister, Mrs. E.G. Earnes and was insured for \$5,000. Mr. Hayes carried a large stock of goods and was doing a good business his loss was partly covered by \$5,000 insurance. The damage to the Witder block was fully covered by insurance. Warcerean and Co., contuctas had an office on the first floor of the Earnest building and lost about \$1,000 worth of furnature with no insurance.”

6.1.3. Clinchfield Offices, 1908 – 1914

7 January 1909. Johnson City Comet.

“Commercial Club Holds Big Meeting... Important Propositions... The Wilder block, which cost \$60,000, across the street from the hotel, was also offered to the trustees at \$20,000. This building has also been installed with steam heat and repaired throughout. The sale of this building would be sold subject to Mr. Carter’s lease...”

26 November 1914. Johnson City Comet.

“Offices May Move to Carter Building. – The Carter building, formerly occupied by the Love – Thomas Co., is being remodelled and made into a modern office building. The first and third floors are being divided into many office rooms. A new vault will be built and a fourth floor may be added. It is stated upon good authority that the building will be occupied by the C.C. & O. Railway as a general office building and the office building in Carnegie abandoned.”

6.1.4. Empire Chair Company, 1916 – 1935

30 June 1916. Carolina, Clinchfield and Ohio Railway, Sixth Annual Report...

Table 58. “List of New Industries Established during Year Ended June 30, 1916”²⁷²

Shipping Point	Name	Character
Johnson City	Empire Chair	Chairs

17 August 1916. Johnson City Comet.

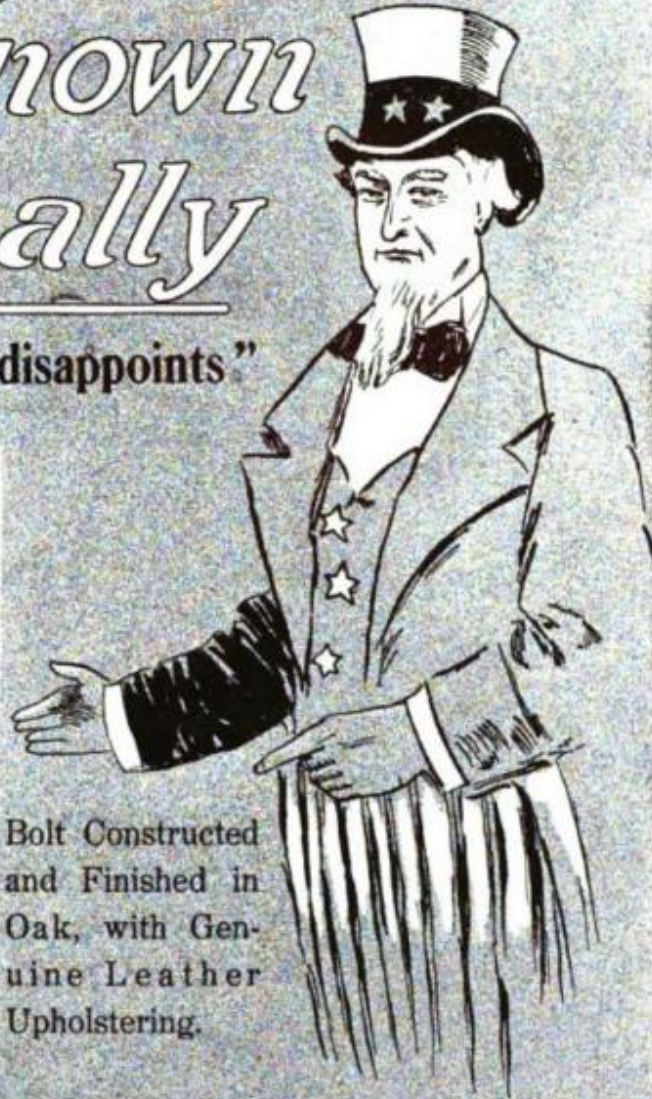
“Empire Chair Company Buys Greeneville Plant – Lee F. Miller, the attorney, who is president of the Empire Chair Co., (which company recently moved its general offices and finishing department from Elizabethton to Johnson City, and has established headquarters in the Wilder block in East Carnegie, which property the company has purchased and remodeled), returned from Greeneville yesterday evening, where he went to attend the sale of the manufacturing plant and assets of the Acme Chair Co., formerly the Greeneville Chair Co., which was sold at Greeneville yesterday by Receivers Kopcke and Shuford...”

Well Known Nationally

"The line that never disappoints"



Bolt Constructed
and Finished in
Oak, with Gen-
uine Leather
Upholstering.



DISTINCTIVE and ATTRACTIVE

CHAIRS and ROCKERS

See Display, second floor, head of stairs, **1319** Michigan Ave., CHICAGO

Representatives: C. F. CARRIER, FRED T. CARRIER

The "Empire" line will move quicker and will sell with less sales talk than any other line of Chairs for the money on the market.

EMPIRE CHAIR CO., Johnson City, Tenn.

Figure 66. *The Furniture Worker*, December 1917.²⁷³



By Empire Chair Company, Johnson City Tenn.

Figure 67. The Furniture Worker, December 1917.²⁷⁴

16 August 1917. Johnson City Comet.

“Tobacco Warehouse – The site has been purchased and the contract let for the construction of the big tobacco warehouse and four prizing rooms at Johnson City, and actual work of construction will begin immediately. The site embraces the entire block on First avenue, Addition, just across the street from the Empire Chair Co., plant formerly the ‘Wilder Building’.”

The author did not locate any additional information on this enterprise.

15 December 1917. The St. Louis Lumberman.

“Johnson City – The Empire Chair Co. has increased their capital stock from \$150,000 to \$200,000.”²⁷⁵

Comfort Plays a Big Part

IT IS HARD to estimate the skill required in making good, first-class high-grade chairs.

There is so much to take into consideration. Comfort plays a big part. There is one thing certain—if a customer tries a chair out by sitting in it, and finds that it is uncomfortable—goodbye sale!

The Empire Chair Company has made a study of chair comfort. Just lead a customer to an Empire chair and tell her to sit down, and you will stand a mighty fine chance of making a sale.

Empire Chairs are made by experts. To stock them is to satisfy completely your chair patronage.



Empire Chair Co.
JOHNSON CITY -- -- TENNESSEE

Rocker No. 588

Figure 68. The Furniture Journal, July 1922.²⁷⁶

1922. Southern Lumberman's Directory of American Lumber Consuming Factories.

“Johnson City... Pop 8,500...

Empire Chair Co... Red and white oak, poplar...”²⁷⁷

6 March 1926. American Lumberman.

“Johnson City – Empire Chair Co., increasing capital from \$300,000 to \$400,000.”²⁷⁸

30 September 1926. Manufacturers Record.

“Tenn., Johnson City – The Empire Chair Co., R.N. Parker, Pres., reported has plans for an additional plant to manufacture dining-room furniture: one 4-story, 80 x 150 ft., one 3-story, 80 x 150 ft. buildings, with ware houses, sheds, office, etc.; brick construction throughout; install modern machinery; estimated cost \$200,000; Don Beeson, Archt.”²⁷⁹

11 November 1926. Manufacturers Record.

“Tenn., Johnson City – Empire Chair Co., R.N. Parker, Pres. And Mgr., let contract to Geo. McClure, Johnson City, for foundation and brick work for \$100,000 plant; other contracts not let; 2 buildings, including kilns and steam plant, mill construction; install complete furniture manufacturing unit; bids not fixed; D.R. Beeson, Archt., and Geo. Smith, Constr. Engr., both Johnson City; has plants at both Johnson City and Elizabethton.”²⁸⁰

December 1926. Wood Working Industries.

“Johnson City, Tenn. – January first the Empire Chair Company will begin manufacturing high grade dining room furniture. More than 200 people will be employed and the company contemplates a production of about \$1,000,000 annually. The new plant, although operated as a separate unit, will be a part of the Empire Chair Company, one of the most active and leading industries for the past several years in this city and Elizabethton. More than 350 workers are employed at the two plants at Johnson City and Elizabethton. With the new furniture plant, the total number of employees will be somewhat more than 550 and the investment will be increased from \$500,000 to more than \$700,000. Three years ago the company’s production amounted to approximately one-half million dollars. Today, their production amounts to more than one million dollars per year.”²⁸¹

1931. Industrial Woodworking: A Magazine for Woodworking Production Executives.

“Plant Output Gains. The output of the Empire Chair Co., Johnson City, Tenn. (manufacturers of high grade furniture) has been steadily increasing. The plant is now operating at capacity with more than 400 men employed.”²⁸²

1932. Tennessee: A History, 1673 – 1932.

“Empire Chair Company is the largest manufacturing concern in Johnson City today. It operates three plants, two in Johnson City, and the original plant at Elizabethton. Over four hundred and fifty persons are steadily employed by the company whose plants have a capacity of two and a half million a year. Present officers of the company are: Robert N. Parker, president, general manager, and treasurer; George T. Wofford, vice-president; Miss M.A. Ross, secretary. The directors include all the officers, and in addition, Mr. Glenn Setzer, chairman of the board, and Mr. James A. Summers.

Robert N. Parker was born in Massachusetts, in 1880. He grew up in a manufacturing section, and early became interested in the manufacture of chairs... Finally, in 1920, he came to Johnson City, taking over the Empire Chair Company which he now controls. As executive head of the enterprise, he has been largely responsible for its present success. Products of the company include office chairs, general household chairs, and dining room suites, in particular.”²⁸³

1932? – 1935. Labor Relations Reference Manual.

“Empire Chair Company which was liquidated in the bankruptcy court, and its plant surrendered to holders of defaulted bonds. For three years [1932? – 1935] the plant was idle contributing to the distress of the unemployed in Johnson City...”²⁸⁴

6.1.5. Empire Furniture Company/Corporation, 1935 to at least 2006

1932? – 1939. “N.L.R.B. v. Empire Furniture Corporation... November 8, 1939... On petition to enforce Board Order...”

The National Labor Relations Board seeks, by its petition, enforcement against the respondent of its order of January 7, 1939, directing the reinstatement of three of its employees and the payment of wages to a fourth who does not desire reinstatement, upon findings made by the Board that the respondent had been guilty of unfair labor practices including the discharge of employees for union activities... The first order of the Board was challenged by a petition of the respondent to set it aside, filed on May 9, 1938...

The respondent is engaged in the manufacture of furniture at Johnson City, Tennessee. It receives the major part of its raw materials from without the State through instrumentalities of interstate commerce, and ships much of its finished product into other States. While some question of jurisdiction was in the beginning suggested, attack upon its exercise is no longer pressed, and we conclude that the Board appropriately exercised jurisdiction.

In view of the issue and findings it seems important to consider the origin of the respondent’s enterprise. It is successor to the Empire Chair Company which was liquidated in the bankruptcy court, and its plant surrendered to holders of defaulted bonds. For three years [1932? – 1935] the plant was idle contributing to the distress of the unemployed in Johnson City. Through the activity of one Shumate, a retired banker and at present Secretary-Treasurer of the respondent, a new enterprise was started in the old plant through cooperation of private enterprise with community effort. The Johnson City Chamber of Commerce and the municipality gave substantial aid to it, in order to ease the local relief load; the Chamber of Commerce loaning the new corporation \$17,500 to help it meet conditions for the approval of a loan by the Reconstruction Finance Corporation of \$175,000, and the City aiding by remitting back taxes and lowering current assessments.

The respondent began operations in June of 1935. Up to the time the present labor controversy began it had made substantial progress, exhibiting its product at the markets in Chicago and in New York, but manufacturing principally only as orders were received. Its R. F. C. loan has been only slightly reduced. It employs a maximum of 350 men, and up to April, 1937, its relations with its employees appear to have been unattended with any difficulty or complaint on their part. Late in April one Barnett, not long in respondent’s employ, communicated with the Textile Workers’ Organizing Committee, an affiliate of the Committee for Industrial Organization, and requested that the employees of the plant be organized into a union. A representative came to Johnson City and began the work of organization. It proceeded rapidly, – it is contended that approximately 300 men signed union cards.

It appears from the record that several of the respondent’s foremen were hostile to unionization. While these foremen were entrusted with complete responsibility for hiring and discharging men, it by no means follows from this circumstance that an irrebuttable or even a reasonable inference arises that a lay-off or discharge was for union activity when the annual labor turn-over was 200 and there is cumulative evidence contra. We must search the record for something more substantial and definite to sustain the findings.

Our first question is whether the record substantially supports a finding by the Board that the respondent refused to agree to bargain collectively with the union if it were designated as bargaining agency of the employees in an election. The Board concluded that the respondent was full apprised from the beginning of the activity among its employees toward self-organization; that its foremen denounced the union; threatened its members with the loss of jobs; that its officer Shumate declared an intention to close the plant if the union succeeded in organizing; that the respondent declined to recognize the union as representative without an election and expressly refused to bargain with it if designated at an election.

We explore the record for support. Winebarger, a foreman, had expressed to an employee the view that unions had never done anything to help him; that strikes had resulted from the activities of organizers in which men had been killed, and that he had seen where the C.I.O. had caused much trouble; and Leonard, another foreman, had given it as his opinion that the C.I.O. were a 'a bunch of communists fit to stir up trouble'. There is no evidence that these observations reflected the view of respondent's management, that the foremen were expected to, intended, or did permit their personal views upon organization to influence them in the hiring or discharging of employees. We fail to find evidence of Shumate's declared intention to close the plant if the union succeeded in organizing. The testimony relied upon for this finding is that of the employee Morrison who was warned by Shumate that he must not solicit union members on company property or abuse those who failed to respond. He testified that Shumate said to him 'When it comes to where we can't do anything, we will lock the gate and leave out of here.' While this statement was vigorously and categorically denied, nevertheless, if it was made it fails to support the finding. When industry for whatever cause can no longer function, the locking of the gate is beyond the exercise of volition on the part of management.

The record supports the finding that the respondent declined to recognize the union as representative of its employees without an election. We do not understand this to be a condemned labor practice when the management desires reasonable proof of majority unionization and its selection of a bargaining agency. The executive officers requested proof that those who professed to represent the men had authority so to do. They offered to produce, though they did not produce, membership cards. This would have told the respondent nothing since it had no record of employee signatures with which to compare the cards. The respondent welcomed an election. Its repeated request for an election is not controverted. It wished to have it held forthwith but the union representatives urged upon it the necessity of time within which to perfect necessary machinery for holding it.

Finally the Board finds that the respondent refused to agree to bargain with the union even if at an election it were designated as the employee bargaining agency. This finding not only flies in the face of evidence produced by the respondent through witnesses whose credibility is not impeached, but is in direct conflict with the evidence of its own representative... There is no basis for the finding that the respondent would refuse to bargain with the representatives of a union certified to it as the bargaining agency of its employees as the result, of an election.

The men who were ordered to be reinstated were Buchanon, a hand sander, Barnett and Hicks, while the respondent was ordered to pay Morrison a minimum amount of wages. Buchanon was laid off; he was a member of the union and had solicited others to join. There was nothing unusual in this laying off of Buchanon. His work was that of a common laborer and when there was not sufficient work the number of sanders was from time to time reduced. The Board draws the inference first that Buchanon was not laid off but discharged, and second that his discharge was for union activity, from the fact that there was a practice at the plant to pay off discharged men immediately, but to pay off those who, of their own accord leave, at the next pay day. It ignores, however, the evidence that Buchanon demanded his pay at once and it was for that reason

given to him. By building one inference upon another, and by the simple expedient of rejecting controverting evidence destructive of both as not entitled to credence even though unimpeached, the Board arrived at its finding. That Buchanon was discharged and for union activity is further sought to be supported by his hearsay statement that after his discharge two additional sanders were immediately employed. To recite this evidence is sufficiently to characterize it...

Barnett was engaged in assembling table bases by means of an instrument called a 'horse clamp'. The method proved unsatisfactory and was discarded. Other men were assigned to assembling by a new method, Barnett being meanwhile laid off. He concluded that he was discharged and the next day called for his tools and drew his pay. His conclusion that he was discharged is not otherwise supported and there is no evidence associating the discharge, if he was discharged, to union activities. Bernie Morrison was likewise laid off because there was no further work. There is credible evidence not impeached that Morrison's work had fallen off greatly in quality and it also appears that he secreted certain patterns. The only evidence that could possibly connect his discharge to union activity is solicitation of members during working hours, and abuse of fellow workers. If so connected his discharge was not in pursuance of an unfair labor practice... the statute does not interfere with the employer's right to discharge for violation of contract and that discharge is not an unfair labor practice merely because the discharged employee is a member of a union.

Hicks was discharged for violating a company rule against running to the time clock. A serious accident to an employee resulting from this practice and a demand by the company carrying group insurance that it cease, led to the adoption of the rule. While compliance may not always have been vigilantly enforced, the rule existed and there is cumulative evidence not only from his superiors but from his coworkers that Hicks violated the rule and so incurred the penalty of discharge threatened for its violation. There is no support for the finding of the Board that the discharge of Hicks was due to union activities.

The petition of the board for enforcement of its order must be denied because its findings of unfair labor practices are unsupported by substantial evidence. Sensible of the great social purpose of the National Labor Relations Act... courts have gone far to uphold rulings of the administrative agency charged with its enforcement, doubtless in the belief that over-zealousness must in time yield to expertness in weighing evidence and that time and responsibility must develop a judicial approach to disputed issues in a tribunal which, though administrative, exercises to such large extent the high judicial function. It may not be amiss – indeed, it may be in the highest public interest to observe that the beneficent purposes of the Act will not, be effectuated by decisions such as that presently reviewed.

Petition denied.”²⁸⁵

30 July 1968. Interstate Commerce Commission Reports: Motor Carrier Cases.

“Empire Furniture Company, of Johnson City, Tenn., manufactures dining room, living room, and occasional furniture. Its gross annual sales is approximately \$2 million or 3.9 million pounds. The furniture is shipped crated to customers located throughout the United States. The company is served by both motor and rail carriers. Shipper complains that general-commodity motor carriers return shipments because they cannot effect interline arrangements and that where such carriers can make delivery they refuse to provide reasonable pickups. Because of the lack of satisfactory service, shipper has been forced to acquire and operate 4 vehicles in private carriage which shipper intends to discontinue if the authority sought herein is granted.”²⁸⁶

Empire Furniture Corporation

Manufacturers of

DINING — BED — AND LIVING
ROOM FURNITURE

Johnson City, Tennessee

Figure 69. The Buccaneer, 1941.²⁸⁷ Added 24 September 2024.

Table 59. “Directory of Industries Inspected, with No. Employees, December 1, 1916, to December 31st, 1917... Washington County – Johnson City”.²⁸⁸

Style of Firm	Business	No. Employees			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Chair Mfrs.	28	24	1	

Table 60. “Directory of Industries Inspected, with No. Employees... Washington Co. – Johnson City”, for the Year Ending 31 December 1918.²⁸⁹

Style of Firm	Business	No. Employees			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Chair Mfrs.	13			

Table 61. “Directory of Industries Inspected, with No. Employees... Washington County – Johnson City”, for the Year Ending 31 December 1919.²⁹⁰

Style of Firm	Business	No. Employees			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Mfrs. Chairs	39	29		

Table 62. “Directory of Industries Inspected, with No. Employees... Washington County – Johnson City”, for the Year Ending 31 December 1920.²⁹¹

Style of Firm	Business	No. Employees			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Chair Mfg., Plant 2	38	17		
Empire Chair Co.	Table Mfrs.	45			

Table 63. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.²⁹²

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Chair Mfg.	79	8		

Table 64. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1925.²⁹³

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Mfg. Chairs	90	12		

Table 65. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1926.²⁹⁴

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Mfg. Chairs	109	9		

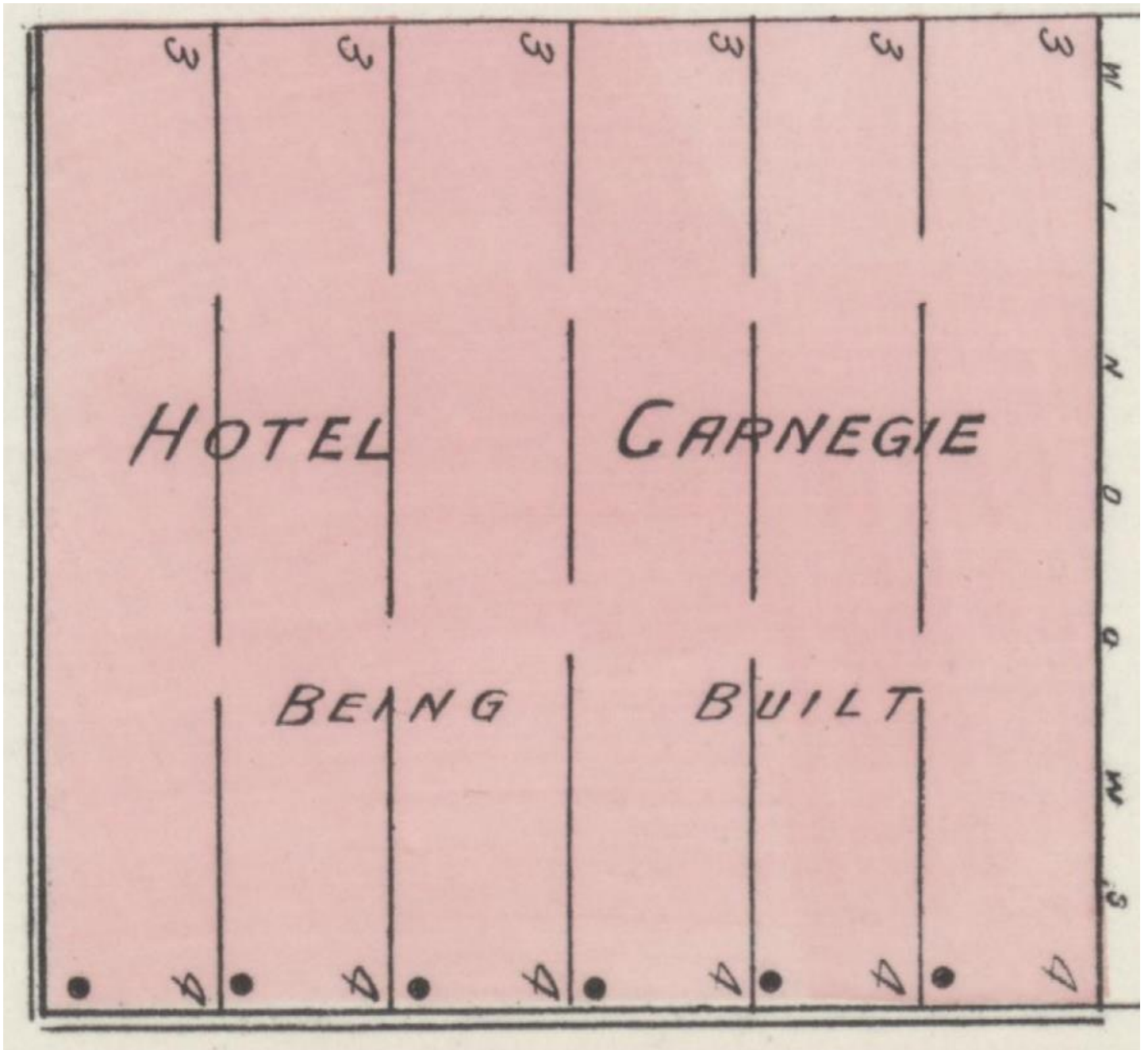
Table 66. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1929.²⁹⁵

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Chair Co.	Mfg. Chairs and Furniture	319	22		

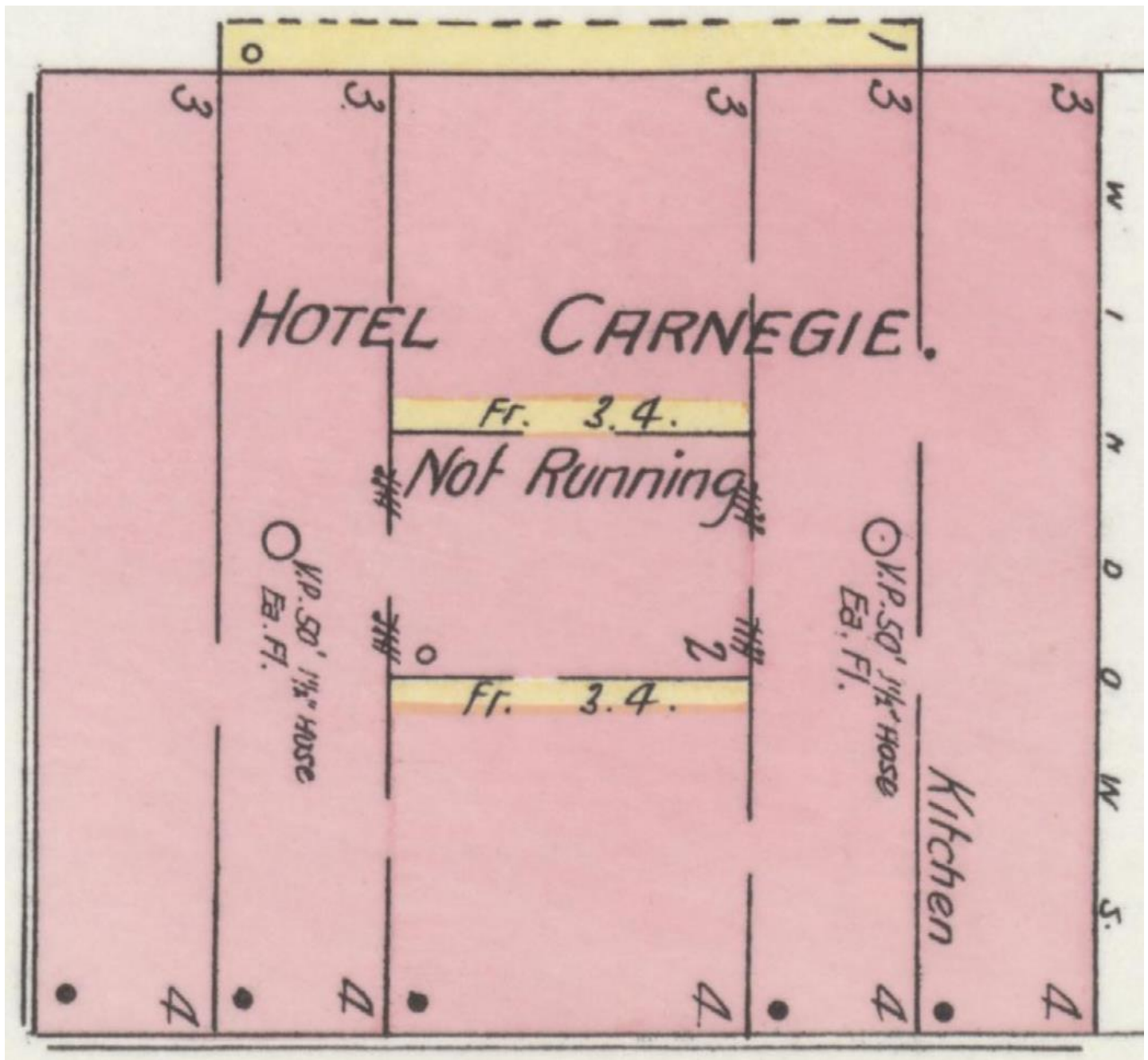
Table 67. “Location and List of Industries Inspected 1935... Washington County – Johnson City”, for the Year Ending 31 December 1935.²⁹⁶

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Empire Furniture Corp.	Mfg. Furniture	287	3		

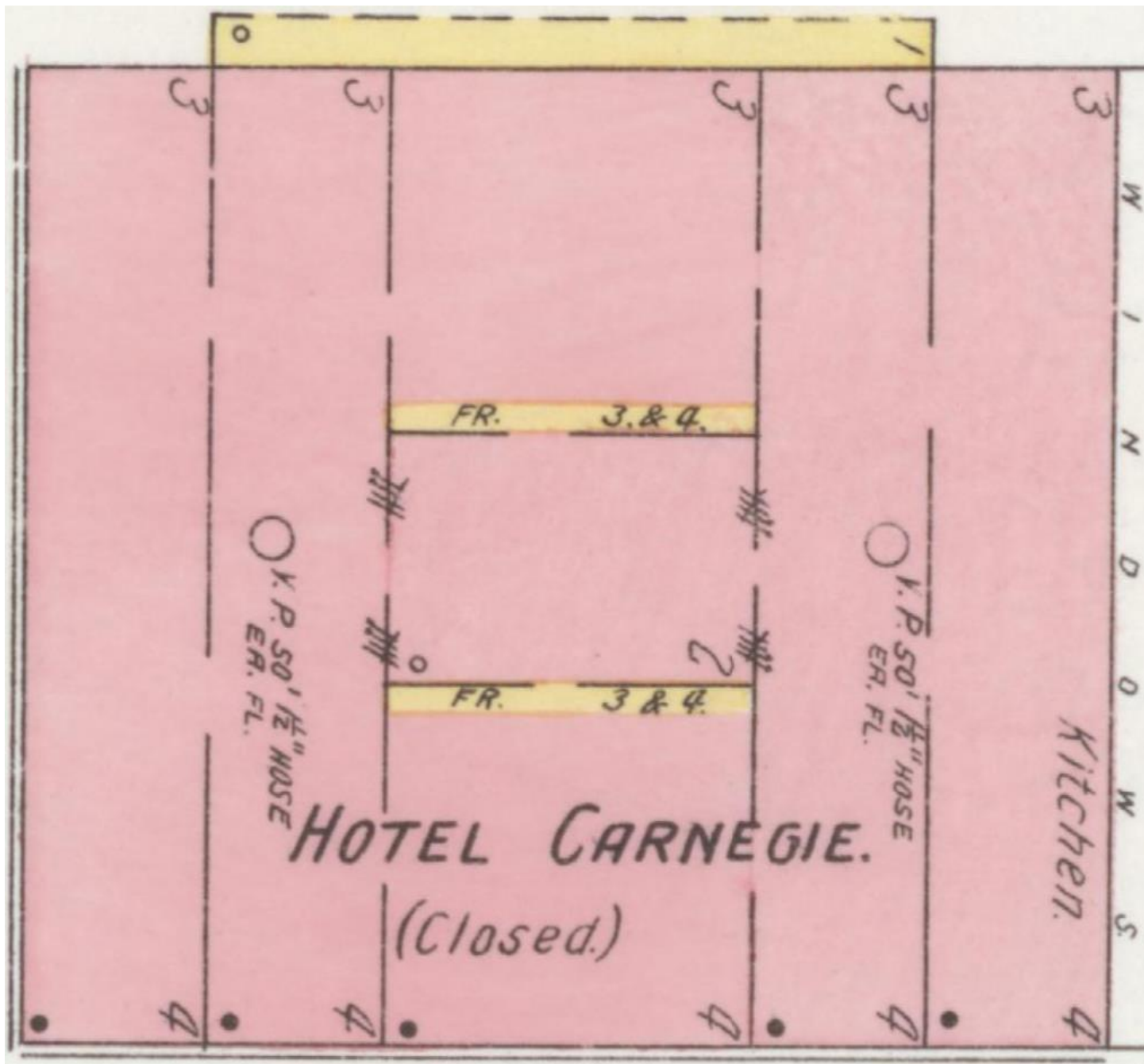
6.2. Hotel Carnegie, 1889 – 1910



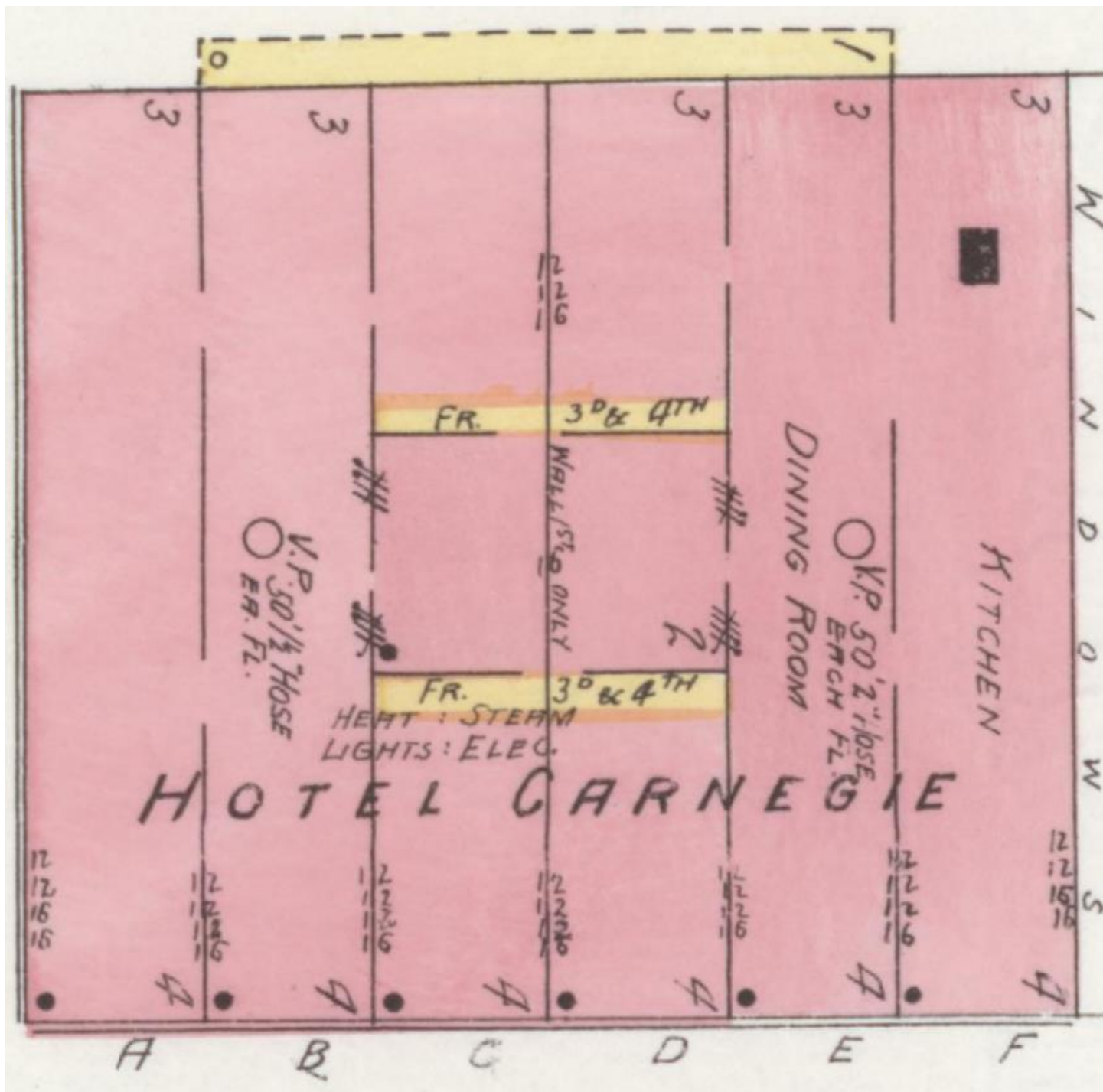
Map 20B. A detail of "Hotel Carnegie" "Being Built", in January 1891.



Map 21B. A detail of “Hotel Carnegie” “Not Running”, in May 1897. Note that the number of internal divisions had been reduced from six to five, with the middle one having twice the width of the other four.



Map 22B. A detail of "Hotel Carnegie (Closed)", in April 1903.



Map 23B. A detail of "Hotel Carnegie", in December 1908. Note the location of the dining room, see Figure 74. Apparently, the reception area was in the middle section, see Figure 73. The Hotel Carnegie was destroyed by a fire in April 1910. In 1925, the McCorkle Lumber Company built a facility on the same land.



Figure 51C. A detail of the south side of the Hotel Carnegie. This image appears to date to between 1905 and 1910.



Figure 52B. A detail of the south side of the Hotel Carnegie. This image predates 1907.



Figure 70. "Hotel Carnegie" by 1906, north side.²⁹⁷

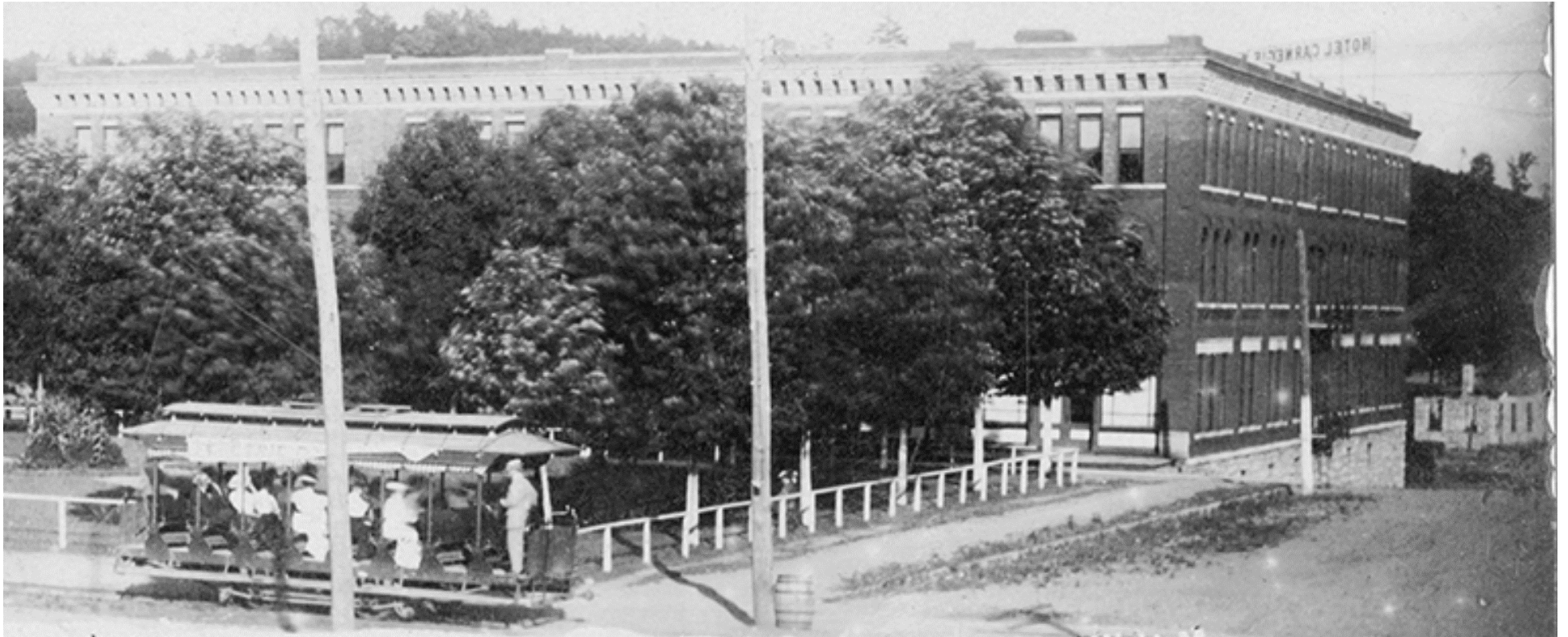
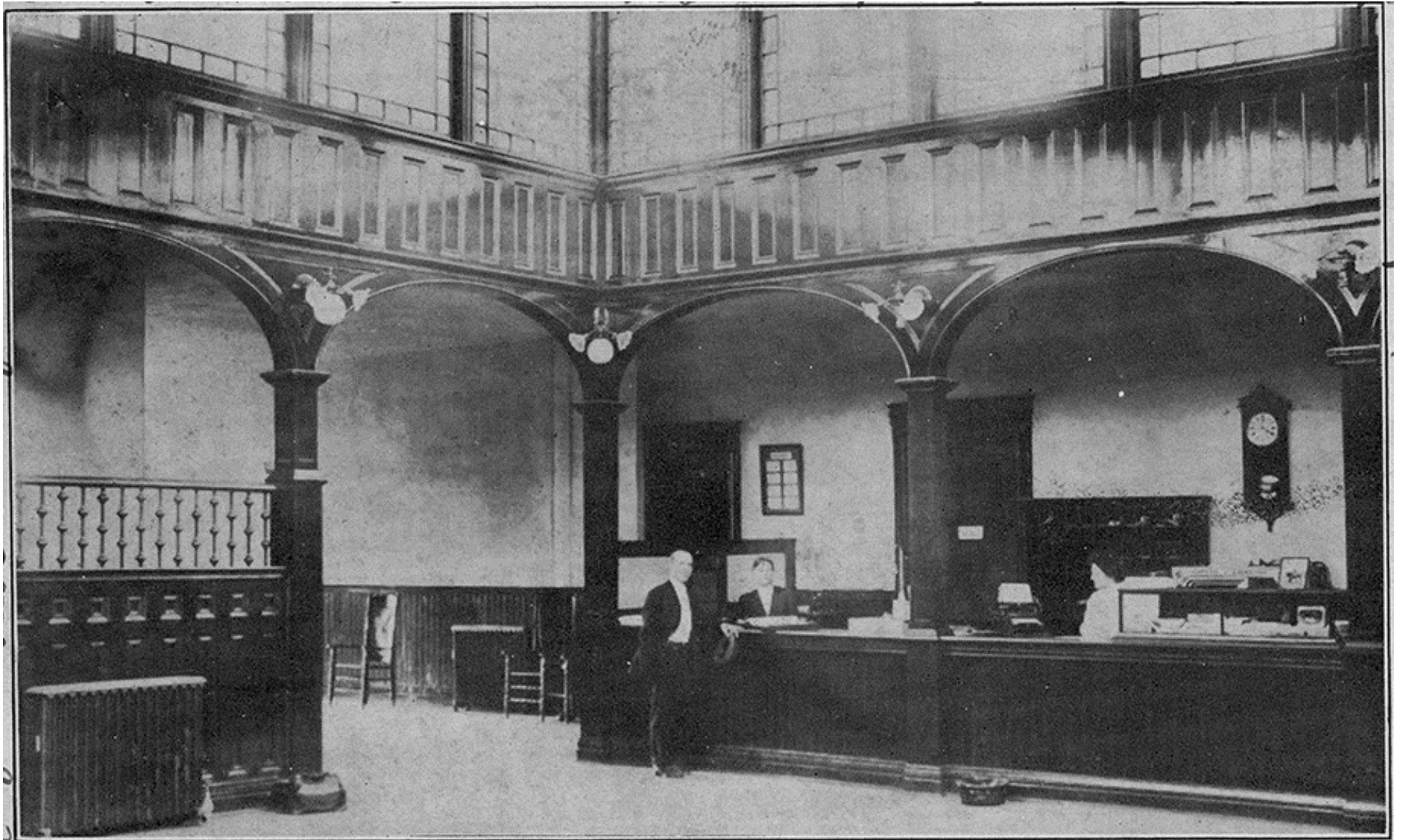


Figure 71. The north side of the "Hotel Carnegie" by 1908. Note the "Hotel Carnegie" sign in the top right of this image. Also note the unfinished C.C. & C./S. & W. depot on the right and the open-air trolley car in the foreground. This card was postmarked 26 August 1908.²⁹⁸



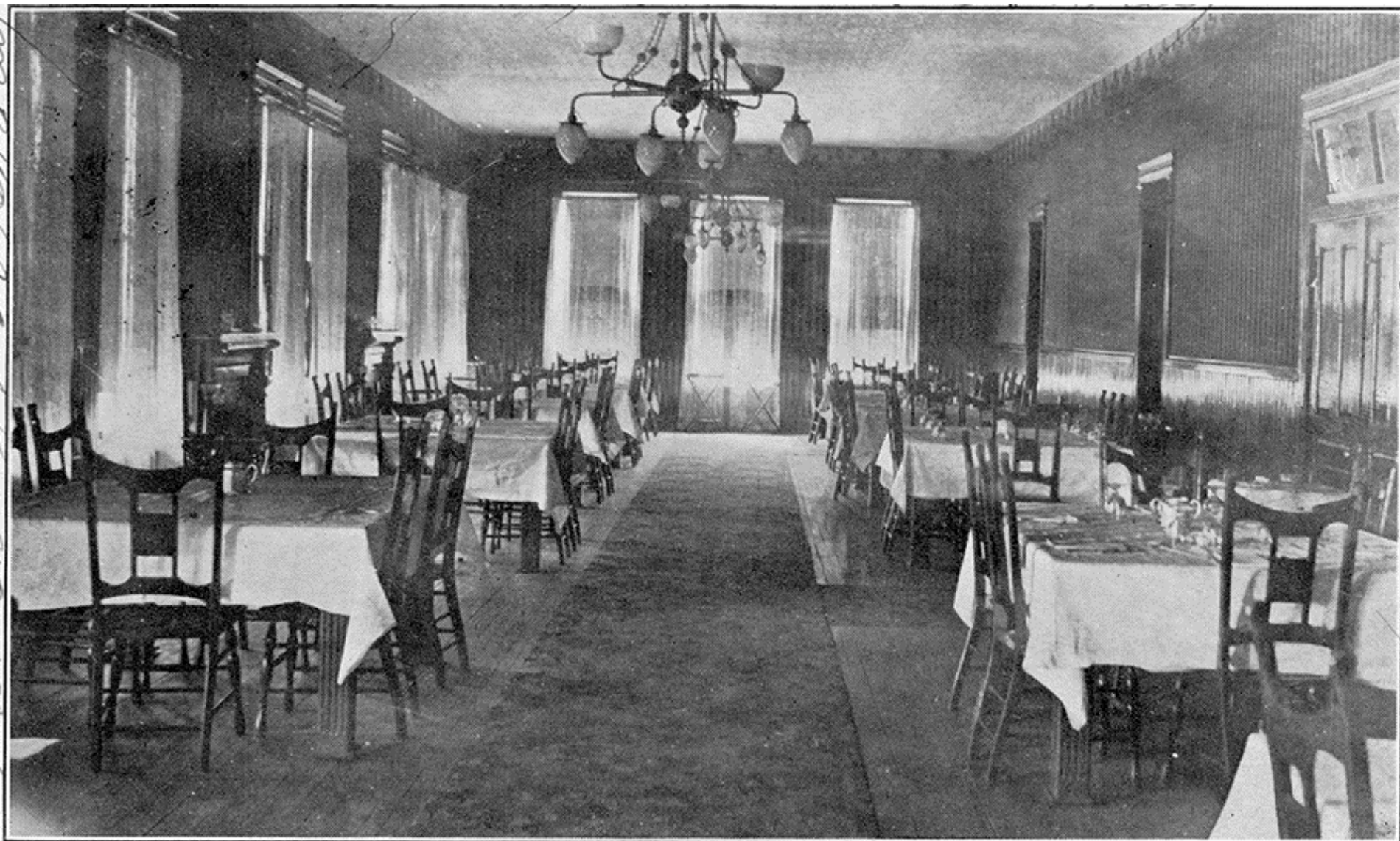
HOTEL CARNEGIE, (FRONT VIEW) JOHNSON CITY, TENN.

Figure 72. "Hotel Carnegie: Front View," by December 1908. Also note the ruins of the C.C. & C./S. & W. buildings in the right background.²⁹⁹



PARTIAL VIEW OF OFFICE AND LOBBY, HOTEL CARNEGIE

Figure 73. "Partial View of Office and Lobby, Hotel Carnegie," by 1907. This card was postmarked 28 August 1907.³⁰⁰



PARTIAL VIEW OF MAIN DINING ROOM, HOTEL CARNEGIE

11/17/07 Mall

Figure 74. "Partial View of Main Dining Room: Hotel Carnegie," dated 17 November 1907.³⁰¹

6.2. Hotel Carnegie, 1889 – 1910

31 October 1889. Johnson City Comet.

“The first block of buildings in the Carnegie addition, was commenced Monday. The block is Gen. T.J. Wilder’s... This is only the beginning of the buildings in the Carnegie addition. Early in the Spring work will be commenced on the big hotel to cost \$100,000, and several other store houses are contracted for. All buildings are to be of brick, and finished in modern style.”

7 November 1889. Johnson City Comet.

“Still Stepping – Three Million Brick Contracted for. Wholesale Houses – New Buildings. – Is Johnson City going to build? We answer the question by stating that contracts have been closed for three million brick by one man...”

A large hotel covering an entire block 300 x 400, and to cost \$100,000 will be commenced in the early spring. The stock has all been subscribed and it will be one of the finest buildings in the South...”

14 November 1889. Johnson City Comet.

“The Carnegie company, with two thousand acres, has taken the lead. The streets in this addition are broad and are being permanently graded. The 3-C depot will be there – and all others eventually. The company is providing liberal things; will build a hundred thousand dollar hotel next year; donate a large market square, and throw open an immense park, of natural forest. Already foundations are being laid for four story business blocks, and the brick are making. Gen. Wilder says that the company will give a site and water [to?] every brick machine that comes along, and make a contract with the proprietor for a million brick. The General added: ‘We expect to lay twenty million brick next year.’

Some of the local speculators have done well...”

2 January 1890. Johnson City Comet.

“1890. – Some of the Things Started and Done on the First Day... A large force was put to work yesterday excavating for the \$100,000 hotel on the corner of 2nd avenue and Center street. The building will be brick and will be 150 x 140 feet and four stories high. The brick are already contracted for and will be delivered at once on the ground. The hotel will be pushed to completion as fast as possible, and will be a model when completed...”

16 January 1890. Johnson City Comet.

“New Buildings... The work of excavating for the large hotel in the Carnegie addition is about completed. The brick work will commence as soon as the material can be gotten.”

6 February 1890. Johnson City Comet.

“In Carnegie Addition... Work on the streets still continues and all the streets are being graded full width now... The excavating for the hotel is completed and masons are at work building the rock retaining wall at the rear of the building. The brick work on the hotel will be commenced

next week and hurried to completed. It is the intention of the company to have the hotel ready for occupancy in the summer.”

27 February 1890. Johnson City Comet.

“Will Stay at Home. The order for the casting and iron work for the new hotel and office building for the Carnegie Land Company, has been given to the Johnson City Foundry and Machine Works. The order is a large one and Chattanooga and Knoxville companies had bids in but Johnson City never gets left. The company here does as good work and does it cheaper than any company in the State. It is not out of place to state here that there are only one or two larger and better equipped shops in the State than the Johnson City Foundry and machine works.”

24 April 1890. Johnson City Comet.

“Started with a Rush. A list of Factories, Hotels, Business and Dwelling Houses Completed and Under Construction Since January 1st, 1890. – Nearly a Million dollars Already Spent in Building... Hotel Carnegie. \$150,000...”

1 May 1890. Johnson City Comet.

“Some of the stone foundations have been finished on the Carnegie Hotel and the brick masons have been spreading themselves this week. The contractors say they will lay 150,000 brick a week. Some idea of the size of the building can be had when it is known that it will require three months to complete the brick work even at that rate.”

19 June 1890. Johnson City Comet.

“The Carnegie Hotel. The work on the Carnegie Hotel is being pushed as rapidly as circumstances will allow. It is an immense building and consequently seems to progress slowly. The walls are up to the second floor and the joists are all laid. The iron columns will be raised this week and the brick work will be resumed and finished within sixty days. The inside finish is already contracted for and will be shipped in a few weeks.”

19 June 1890. Johnson City Comet.

“Lloyd, Mathes & Co. have an advertisement in this issue for wood. They are running a large brick-yard in the Carnegie addition and are making brick for the Carnegie hotel. This job alone will take nearly 2,000,000 brick, but they will make them and have the brick work done by the middle of August.”

17 July 1890. Johnson City Comet.

“Our New buildings. – 38 Business Houses and 75 Dwellings now in Course of Construction. – No Town in the State can Show a Growth so Great... “Work on the Carnegie Hotel is progressing. The walls are about ready for the third floor timbers, and the building is beginning to assume immense proportions. A glance at it now will enable visitors to form an idea of what it will be when finished...”

17 July 1890. Johnson City Comet.

“Wanted. Six good brick-layers. Wages \$3.50 per day. None but sober men need apply. M.F. Shaffer & Co. Carnegie Hotel Building, Johnson City, Tenn.”

20 November 1890. Johnson City Comet.

“Hotel Carnegie. The Carnegie Hotel is rapidly nearing completion. The masons laid the last brick on the chimney tops yesterday and the roofers completed their work this morning. The plumbers and electricians are busy arranging for steam, water, lights and calls, and an army of men are lathing and otherwise getting ready for the plasterers. Every effort will be made to have the building ready by the first of January. The wood-work is all worked out ready to be applied and there will be no further delay.”

15 January 1891. Johnson City Comet.

“Nearing Completion. This week the plasterers will finish their work on hotel Carnegie. The carpenters have been following closely with the finish and have a large portion finished. At the present rate, with no more delay in getting flooring for the first floor, the wood work inside will be completed by March 1st. The carpets for all the rooms are made and the furniture purchased for all the departments, including china and silverware, and as soon as the carpenters get out of the way it will be fitted up for business.”

30 April 1891. Johnson City Comet.

“Like a Mighty Giant – Johnson City Stalks on to the Goal in Spite of Hard Times... During all these hard times Johnson City has done more good and permanent work than any other town, large or small, in the South. Notwithstanding the bad weather, work has steadily advanced on all the plants and factories, until now, we are on the threshold of prosperity’s open door. In the next sixty days we will have completed one of the most magnificent, and capacious hotels in the State. We refer to the Carnegie hotel, which is being erected by the Carnegie Land Company in their addition to Johnson City. This hotel, which is built of re-pressed brick and trimmed with white stone, is four stories high, 150 long by 140 wide and contains 120 bed rooms 13 x 24 in dimensions besides offices, parlors, dining rooms, etc., all having monstrous proportions, and is supplied with all modern improvements, such as water, gas and electricity. It will be furnished at considerable cost – the carpeting and furniture alone costing over \$21,000. The same company has now nearing completion a blast furnace of 125 ton capacity, which in point of material and architecture, is without equal in the South. The Carnegie Land Company improve their property and construct their edifices as if they were building for eternity. It has almost every street in it cut down and filled up to uniform grade, and every dollar that it realizes from the sale of real estate it turns around immediately and put it back in the shape of improvements. The operations of this company should inspire the utmost confidence in all as to the future of Johnson City...”

4 June 1891. Johnson City Comet.

“The Hotel Carnegie, a handsome brick and stone structure of four stories, will be open by the end of this month. It is thoroughly equipped and will be one of the finest hotels in the central

South, certainly the best in East Tennessee. The 100 bedrooms are large and lofty, all lighted from the outside, and the parlors, offices, etc., are spacious and comfortable.”

8 July 1891. “Hotel Carnegie – The Finest Hotel in East Tennessee Completed – To be opened by the 20th – Mr. Farr of Pennsylvania as Landlord – Johnson City Comet – July 8, 1891

Hotel Carnegie is Finished.

This great palatial hotel has no equal in East Tennessee and for convenience has no superior in the South. It was built and furnished at a cost of about \$125,000. The best of material was used throughout the structure, the interior finish being antique oak.

It is lighted by immense plate glass windows and there is not a room that does not receive a copious supply.

In all there are about one hundred and twenty-five rooms in the three stories which constitute the hotel proper. Below these, and fronting on First Avenue, are six large rooms built for storage of materials. These are in the basement, which has been constructed of heavy stone. Back of these rooms are others to be used in connection with the hotel, as general delivery of supplies, engine room, laundry, etc.

There is not to be found a single accommodation known to hotels with which the magnificent structure is not provided.

It has an electric passenger elevator in addition to its numerous and easy stairways. It has spacious hallways, and is provided with ventilation sufficient to insure the highest degree of health and comfort.

Its dining rooms are large and finely finished. Its parlors and reception rooms are peerless in the finish and furnishings. Its billiard, library, bath and other rooms have the polish of perfection. The office room, which is in the center, receives light by day from a great opening over its head. The roof is overlaid with pitch and gravel and furnishes a splendid observatory. From here one can see quite a distance up and down the valley and across the mountains miles away.

The verandas are features worthy of note. On the side fronting the railroad there are two verandas extending out from the first and second floors of the hotel proper. Each of these is a hundred feet in length and is provided with wrought iron balustrades. Fronting Second Avenue is a larger veranda by way of which the hotel is entered. Along the sides of the structure are some two or three smaller verandas extending each from the terminus of a hallway.

The hotel is provided with first class water and electric systems. It has also a system for lighting by gas. It will be heated by radiation from a furnace beneath. The building has been laid throughout with very fine carpet, every room has been elegantly furnished and there is not a feature that is lacking in completeness.

Hotel Carnegie will be open for guests about the 20th of this month. The property has been leased to R.N. Farr, a man with liberal experience in the art of conducting hotels. Mr. Farr is a native of Pennsylvania, but comes here from Lincolnton, North Carolina, where he has recently been conducting a hotel. He will be here the latter part of this week to make preparations for the opening.

There is nothing out except a part of the parlor furniture, which will be here this week.

Thus on or about the 20th there will be opened in Johnson City the finest hotel in East Tennessee, suited alike to commercial travelers, tourists, and pleasure seekers.”³⁰²

30 July 1891. Johnson City Comet.

“Mr. Farr, the lessee of the Carnegie Hotel, has arrived and is making preparations to open that magnificent hotel as soon as possible.”

Hotel Carnegie,
Largest, finest finished and best equipped Hotel in East
Tennessee.
Large Sample Rooms.
Electric cars to all depots and every part of the city.
Has Every Modern Convenience.
Rates, \$2.50 per day and upwards. Address
R. W. FARR, Manager.
SPECIAL INDUCEMENTS TO TRAVELING MEN.

Figure 75. Johnson City Comet, 24 September 1891.

27 September 1891. The Hotel Carnegie “was opened for business on Sept. 27, 1891. In the daily Comet of that date we find this statement in reference to the hotel:

‘The auspicious beginning of this magnificent enterprise marks a new and important epoch in our history. It is a proud and enduring moment to far-seeing business genius and tact, the energy, the hopefulness, and the confidence and courage of capital which have transformed, in a few brief years, a struggling, unsightly village to a gem city, bristling with life and activity and full of incentives and aspirations for the future.’”³⁰³

15 October 1891. Johnson City Comet.

“The Hotel is a handsome structure, built of Chattanooga pressed brick, with stone trimmings, finished throughout in the most liberal and elegant manner with solid oak wood work, with grand stair cases of the same material. Every feature of the construction of this great hotel is of the most solid, substantial and elegant character. Every room is well lighted, and not one in the house is less than eighteen feet square, most of them eighteen by twenty four. My first entrance was in the kitchen, where I was met by a silver plate on the ranges which bore the legend ‘T.A. Snow, Chattanooga.’ It is the most elaborate and completely equipped kitchens I have ever had the pleasure of inspecting. ‘Here,’ said the General, ‘I made my beginning. I planned the kitchen, the heart of the establishment, and built the hotel to it.’ I could take up a page of the Times in describing this house and yet not more than fairly describe it.”

17 December 1891. Johnson City Comet.

“A Comet representative stepped into the new grocery store of Robert Mason while in Carnegie yesterday. Mr. Mason has a most complete stock of the very best brand of groceries, and his store is neat and attractive. He occupies a corner room under Hotel Carnegie.”

NEW GROCERY STORE.
In the Corner Room, Hotel Carnegie Block, First Avenue.

I have just opened a large and well selected stock of **FRESH GROCERIES** and **CONFECTIONS** in the Hotel Carnegie Block, corner of Center Street and First Avenue, and will be pleased to have you call and see what I have. I have made a specialty of securing a fine assortment of Goods in my line for the Holiday Trade, in the way of

Fruits, Nuts, Fancy Candles, Etc.

Store within ten steps of electric car line. Call and see me. Prices Reasonable.

R. B. MASON.

Figure 76. Johnson City Comet, 31 December 1891.

4 February 1892. Johnson City Comet.

“\$1,288,051. – These are the Startling figures will Tell of the Growth and Activity of the Plucky, Magnetic City – In 1891 – Complete List of Improvements during the Year... More than a million and a quarter of dollars have been expended in plants, business blocks, school houses, residences, street improvements, water works, railroads, etc... Hotel Carnegie, 1st avenue [\$]100,000...”

Carnegie Barber Shop.

HOTEL CARNEGIE

Lewis Buchanan - - - Proprietor.

Everything new, clean and first-class, Experienced artists in attendance, Hot and cold baths at all hours. Give us a call. 408-1f

Figure 77. Johnson City Comet, 4 August 1892.

26 October 1893. Johnson City Comet.

“Hotel Carnegie Closed. Hotel Carnegie, which has been one of the best kept hotels in the State, closed temporarily last week on account of business being so light. Mr. R.W. Farr, who has managed the hotel for the past two years, has had a great many difficulties to overcome, but in spite of all that everything under his under his management has been kept first-class up to the day it closed. It is to be hoped that business will so improve that its doors will be open to the public very soon again, and that Mr. Farr will be its manager as no better hotel man could be found in the South.”

6 February 1896. Johnson City Comet.

“The New College. The prospect of Johnson City getting the big college is very flattering indeed. The citizens are taking kindly to the proposition of the synodical committee, and will no doubt be able to make the best offer along the line.

For 50 years the boards have been trying to solve the problem presented by Washington and Tusculum colleges which are rival Presbyterian institutions so close together that the church thinks that it is a waste of money to endow both of them.

The bursting of the boom of Johnson City was at last presented a solution for it. Carnegie hotel for \$30,000 which cost \$96,000. At a meeting last Friday night a committee...”

14 May 1896. Johnson City Comet.

“We have information from a thoroughly trustworthy source to the effect that Hotel Carnegie will soon be thrown open to the public. Parties are in the city now with this object in view.”

May 1897. The Sanborn Insurance Company Map lists the Hotel Carnegie as “Not Running”.

14 April 1898. Johnson City Comet.

“Open Up Hotel Carnegie. It is a pity to see such a fine building as the Carnegie Hotel standing idle. Why not some good, live hotel man lease it for the coming season, and by advertising it thoroughly there is no doubt but what it could be filled to overflowing with Southern visitors. This has been a very mild winter, and the indications are that yellow fever germs have not been destroyed, which will be the means of driving more people to the mountains than was ever known before. Johnson City is situated in one of the most healthful climates to be found anywhere, and the Carnegie Hotel is one of the finest in State and would accommodate one hundred and fifty people with great ease.”

22 December 1898. Johnson City Comet.

“The Carnegie Hotel was also sold and the Travelers Insurance Company bought it at \$25,000.”

14 June 1900. Johnson City Comet.

“Several car loads of furniture are being shipped from Hotel Carnegie to points in Alabama.”

21 February 1901. Johnson City Comet.

“The effort to get Hotel Carnegie opened to entertain the board of managers of the Soldiers’ Home, has been abandoned, as the necessary repairs could not be made in time.”

7 March 1901. Johnson City Comet.

“Big Boarding House. Wm. E. Uptegrove & Bro., contemplate leasing Hotel Carnegie and fit it up for a private boarding house for their employees who do not live in the city. The building was constructed for a hotel in 1891 but has never been used as such. For several years it has been rented as a tenement house. The building is well suited to the purpose for which Messrs. Uptegrove & Bro. desire to use it and they will probably lease it.”

3 April 1902. Johnson City Comet.

“Brick Plants... The East Tennessee Brick Co. is moving its plant to a lot near the furnace and has it about ready to put in operation.

M.H. Campbell & Co. are going to make some hand-made brick on the old yard below Hotel Carnegie and are now getting ready to do so.”

April 1903. The Sanborn Insurance Company Map lists the Hotel Carnegie as “Closed”.

30 April 1903. Johnson City Comet.

“It has been suggested that Hotel Carnegie be purchased by the city and used as a high school or college. It is understood that the building and contents that cost \$100,000 can be had for \$17,000 or \$20,000. If Johnson City will buy the building for this purpose there is scarcely any doubt that Washington county will levy a special tax to help maintain it as a county high school. There is no doubt about the necessity for such a school here. We must have a college. A great many of our people can not send their children to boarding schools for financial reasons. Here is a solution of the problem. What do you think?

3 September 1903. Johnson City Comet.

“Hotel Carnegie Sold. Hotel Carnegie has been sold to T.A. Gillespie, of this city, and Dr. J.F. Spence, of Knoxville. The purchasers will incorporate it as a sanitarium and put the stock on the market in a short time. About \$20,000 will be expended repairing and remodeling the building and beautifying the grounds.”

18 August 1904. Johnson City Comet.

“Hotel Carnegie Sold. The Home College Company of Chicago is the fortunate purchaser of this splendid property. Dr. John Fletcher Spence has just consummated a sale of the entire

property and the Appalachian Sanitarium will soon be known as the Home College Company. This company is incorporated under the laws of the state of Illinois, with its main office at 355 Dearborn street, Chicago. Dr. John Mersitte Driver is president, Prof. Hamilton White secretary and general manager, and Hon. W.H. Culver treasurer..."

6 October 1904. Johnson City Comet.

"Capitalists Pleased. The Chicago capitalists, Messrs. Northrup & Lampson, who purchased the Carnegie hotel, were here last week and looked at the building. They were much pleased with their purchase and will spend several thousand dollars repapering and otherwise over hauling the building. They will make it a summer and winter resort, and hope to have it filled with guests the year round."

3 November 1904. Johnson City Comet.

"Will Improve Hotel Carnegie. The new owners of Hotel Carnegie will greatly improve the building. Mr. Northrup has contracted for a steam heating system and lighting plant. The building will be re-papered, and Will Smith, having secured the contract for a large part of the work, will begin the task Monday.

The street car line will be extended to the building. A track force will be put to work Monday extending the line. Mr. Northrup is also negotiating for the adjacent building and the estate of the Carnegie Development Company. S.H. Pouder has been employed as assistant manager of the hotel."

17 November 1904. Johnson City Comet.

"Street Car Extension. The Johnson City Traction Co. is rapidly extending its line to Hotel Carnegie and the S. & W. depot. It will be completed next week if the weather is favorable. When the cars begin running to the depot the S. & W. trains will not stop at the Market street crossing for passengers and persons desiring to take trains will have to go to Carnegie. The street car line will be connected with the S. & W. sidings and will be thus enabled to transfer freight from the depot to the Soldiers Home or to other parts of the city. The street car line will be laid out Second Avenue to the hotel instead of on First Avenue as heretofore stated."

8 December 1904. Johnson City Comet.

"Extension Completed. The Johnson City Traction company has completed the extension of its line to Hotel Carnegie. The completion was celebrated with appropriate ceremonies, and the last spike was driven by H.H. Kirkpatrick, manager of the company, and C.S. Northrup, manager of the Hotel company. A regular schedule will be inaugurated Saturday."

15 December 1904. Johnson City Comet.

"Auspiciously Opened. Responding to an invitation the Comet joined a party of business men last Saturday morning who were the guests of the Johnson City Traction company, the occasion being the opening of the line from New street to Hotel Carnegie. A special car was operated and the entire trip was successfully made under the most favorable circumstances, and without destroying any bottled goods. Manager Kirkpatrick was in charge of the party. Mr.

Northrup, manager of the hotel and the Big Store, was on board and took pleasure in showing the party through the hotel and the department store. Many improvements are being made in the hotel and it is being thoroughly renovated and retouched. With the car line open and 15 minute trips being made to the hotel and the store are virtually in town and will do a good business.”

12 January 1905. Johnson City Comet.

“The Bubble Burst. – Promoter Northrup’s Plans Flashed in the Pan. For some weeks there has been a good deal of old time activity around the Carnegie Hotel. Dr. Spence came back from Chicago some time ago and announced that he had sold the hotel to Chicago capitalists. Later there came Mr. C.S. Northrup, who took charge as a manager for the owners. He began to do great things at once. The hotel was over hauled and a great many things were added and others promised. Mr. Northrup organized the Carnegie Mercantile Co. under the laws of the District of Columbia, and it was soon announced that a large department store was to be established. The Singhiser & Chandler building and the Wilder block were contracted for and a substantial payment made. The lands and lots of the Carnegie Development Co. were obtained and for awhile all looked well. It looked like the good old days were returning and all would have been well if – aye, there’s the rub. All this enterprise is expensive and pay day came, then the trouble started. The consolidation for the hotel failed, and Dr. Spence filed a bill in chancery attaching the property for his debt and asking that the sale be set aside. Pending action upon this bill Northrup was arrested and taken to Knoxville charged with passing worthless securities. He gave bond and had the hotel deeded back to Dr. Spence by the Chicago parties. The bond given by Northrup was attacked, as it consisted only of Carnegie Mercantile Co. stock, and last Monday he was re-arrested and is now in Knoxville. It is possibly that matters may be adjusted amicably if certain sums of money are paid. Dr. Spence returned from Knoxville Thursday afternoon and said Northrup was still in the hands of the sheriff.

On last Saturday afternoon the Carnegie Mercantile Co. gave a deed of trust for 15 days to protect creditors to the amount of about \$8,000. S.A. Byrd was appointed trustee. The stock will invoice enough to cover the debts if it can be sold advantageously.

Mr. Northrup insists that all matters will be straightened up in a short time. He bought some valuable property and if he could have held on would have realized a handsome profit. The true story of the transaction would read like a romance and the Comet will give it to the public as soon as it can be published.”

20 July 1905. Johnson City Comet.

“Carnegie Hotel Leased. Trigg & White, real estate dealers of Bristol, have leased Hotel Carnegie for Mr. Geo. L. Carter as headquarters for the Carolina Construction Co. Mr. Carter will use a number of the large rooms on the first floor for supplies and the upper rooms for offices. The hotel will be kept open under the present management or a new one. The terms of the lease are such that if the building should be sold to other parties the occupants are to have six months’ notice to vacate.”

21 September 1905. Johnson City Comet.

“C.S. Northrup Convicted. C.S. Northrup, the Chicago man who came to Johnson City in connection with the sale of Hotel Carnegie some months ago, has been convicted of obtaining money under false pretense and sentenced to the penitentiary for five years by the Knox county

criminal court. The case has been appealed to the supreme court and will be passed upon by that tribunal in a few days. In the meantime the bondsmen have given Northrup up and he is in jail. The specific charge against him is that he obtained Dr. J.F. Spence's endorsement to a note for \$4,000 and obtained the money upon it for the purpose of using the funds to pay lien notes on the hotel due the Travelers' Insurance Company and used the money for other purposes. The warrant was sworn out by Dr. Spence after repeated efforts has been made to settle the matter otherwise."

**South and Western Offices
Will Come to Johnson City**

Hotel Carnegie and Other Buildings Have Been Leased
and the Transfer Will be Made Within Next
Sixty Days—Means Much to Our City.

The Bristol Herald of the 5th instant says: "It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter having been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted and otherwise repaired and will afford ample room for the use which will be made of them.

Figure 50 (reproduced here for convenience). Johnson City Comet, 4 October 1906.³⁰⁴

4 October 1906. Johnson City Comet.

“Johnson City generally gets what it goes after. It wanted the general offices of the South and Western railroad company and went after them. As a result the Carnegie Hotel and the Wilder block have been leased for twenty-seven months by the company and will be occupied by the first of November or as soon as they can be put in proper repair. We congratulate Mr. Carter and associates upon the wisdom displayed in this decision and felicitate Johnson City upon its victory. It’s no trouble to show Johnson City’s advantages to wise people and win them.”

10 October 1906. The Coal Trade Journal.

“It was announced in Bristol this week semi-officially that the principal offices of the Clinchfield Coal Co., Dawson Coal & Coke Co., Virginia – Pocahontas Coal & Coke Co., Crane’s Nest Coal & Coke Co., South & Western Ry. Co., and the other business of the George L. Carter and Thomas F. Ryan interests, would, within 26 days, be moved to Johnson City, a small town 25 miles south of Bristol. The reason assigned for leaving Bristol was their inability to obtain sufficient office room under one roof in Bristol. This will be a loss to Bristol of a pay-roll said to aggregate \$25,000 monthly, and about 200 men.”³⁰⁵

11 October 1906. Johnson City Comet.

“Official Announcement. – South and Western Officials Tell Something of the Plan. ‘After a consultation between Alfred Walter, president of the Seaboard Air Line railway and chairman of the board of directors of the South & Western, and George L. Carter, president of the latter and head of the George L. Carter syndicate, in Bristol last night it was officially announced that the headquarters of the South & Western would be moved to Johnson City within sixty days.

The Carnegie hotel and the Wilder block at Johnson City have been leased by the company and will be used as the general offices of the operating and engineering departments. Everything will be moved from Bristol and it is probable the officials who now reside here [Bristol] will move to Johnson City.

The George L. Carter syndicate, which is building the South & Western, is backed by George L. Carter and Thomas F. Ryan and is one of the largest corporations in this entire region. The construction of the South & Western railway from the coal fields of Virginia to the South Atlantic Seaboard will be completed within two years...

This location of the offices will mean much for Johnson City as the pay roll of the office force alone is said to exceed \$25,000 per month, and the city has been kept continually flooded with laborers and transients connected with the road for two years and much revenue has been derived therefrom.

One reason assigned for leaving Bristol is the lack of office room...

The Carter interests have taken options on nearly all of the available real estate at Johnson City and the immense yards, the largest on the road, will be constructed in the suburbs of the city and options for the same will be taken up as soon as possible, The road will enter the same depot with the Southern and ground has been secured for this purpose at what is considered a very reasonable price.’ – Bristol Correspondent in Knoxville Sentinel.”

6 December 1906. The Johnson City Comet.

“Beginning about the 15th of December or as soon thereafter as the offices of the S & W are located in the Wilder block, the Johnson City Traction Company will operate street cars between the Arlington hotel and Hotel Carnegie every 15 minutes until mid-night. This is done for the accommodation of the S. & W. employes and the guests of Hotel Carnegie.”

27 December 1906. The Johnson City Comet.

“Hotel Carnegie Opened. Hotel Carnegie was thrown open to the public Christmas day with W.H. Martin as Mine Host. The building has been thoroughly renovated, repapered, rewired and heated throughout by steam. The plumbing has been overhauled and many private baths installed. Mr. Martin has had much valuable experience in public hostelrys and will operate this hotel to the satisfaction of the public. Hotel Carnegie is not particularly an ornate building, but it is a solid pile of comfort and convenience and is modern throughout with its new equipment.

Beginning January 1st street cars will be operated upon a 15-minute schedule between the Southern depot and the hotel until midnight. Practically all the employees of the S. & W. offices will board at the hotel and it starts out with a nice patronage.”

7 February 1907. Johnson City Comet.

“Western Union Enlarging. The Western Union in this city is spreading out. An office has been opened in the Carnegie Hotel with Miss Dixie Bryan as branch manager. The office up town is still under the management of Holland Russell, with W.I. Ray and Billie McClain as assistants.

Hereafter the office will be open until 9 o’clock every night and on Sunday from 8 to 10 a.m. and 3 to 5 p.m. This is quite an improvement upon the old conditions and shows how we grow.”

23 July 1908. Johnson City Comet.

“Carnegie Hotel Sold. It is currently reported here that Dr. J.F. Spence has sold the Carnegie Hotel to Chicago parties and after January 1, 1909, it will be used as a sanitarium.”

31 December 1908. Johnson City Comet.

“Bright Prospects for Girls’ Academy Here – Last Friday at the meeting of the board of directors of the Washington and Tusculum Colleges at Greeneville, a committee of three gentlemen from the Commercial Club, appointed by the president, S.C. Williams, E.F. Young and Jas. A. Summers were in attendance, also J.E. Brading, a member of the board of directors, looking after the interest of Johnson City to try and persuade the board to move their college to Johnson City, and made them a most generous offer in connection with the Carnegie Hotel property.

The committee and their proposition were favorably received and much encouragement was given them. Members of the board, whom it was thought would be unfavorable to such a proposition, were heartily in favor of it. The meeting and results were very satisfactory, and if we keep our ear to the ground we are likely to hear something drop now very soon.

This Carnegie Hotel property was to be sold to the International Institute of Chicago for a tuberculosis sanitarium, but on account of the opposition of the Commercial Club, the scheme was abandoned. The banks which hold the mortgages on this property are anxious to realize on same

and have offered the property to this college for a very low figure. If it is accepted a female college will be established in the immediate future. Here's hoping."

7 January 1909. Johnson City Comet.

"Commercial Club Holds Big Meeting... Important Propositions... Improvements in Carnegie Hotel.

Since the hotel was opened Mr. George L. Carter had installed at large expense a modern steam heating plant, and radiators were placed in all the rooms, besides many other improvements. The furniture, fixtures, flatware and linen all go with the building, and we believe Mr. Carter will give material help toward assisting the club in locating a school in this property.

The Wilder block, which cost \$60,000, across the street from the hotel, was also offered to the trustees at \$20,000..."

14 January 1909. Johnson City Comet.

"Mass Meeting Monday Night in Courthouse... to Raise Funds Necessary to Offer Hotel Carnegie to Washington – Tusculum Colleges. – On Monday night a good sized crowd of leading business men assembled in the court house, pursuant to a call issued by the Commercial Club for the purpose of giving citizens at large an opportunity to discuss the proposed city charter and to make donations to a fund to be given as a bonus to establish a college in Johnson City.

Mr. S.C. Williams, chairman of the Commercial Club's committee, which waited on the board of trustees of Washington and Tusculum Colleges, which met in Greeneville some three weeks past, offering to them the Carnegie hotel property, which cost \$125,000, for \$36,000 and to supplement this offer with a subscription of \$15,000, as stated in our last week's issue of this paper.

A subscription was taken up, in which fifty per cent was to be paid in cash, and the balance in equal installments running for five years. Seven thousand five hundred dollars was subscribed, and Mr. M.L. Fox assured the people that he would secure an additional \$2,500, making a total of \$10,000 subscribed at the meeting.

A committee was appointed to raise an additional \$5,000. It is believed that this can be easily accomplished, as many of the leading and wealthier business men of the city were absent..."

24 February 1910. Johnson City Comet.

"Carnegie Hotel to be Apartment House – The trustee holding control of the Carnegie hotel property has decided to convert the building into about twenty apartments and put a new roof on it. It is believed that with the improvements in contemplation, Johnson City will have the best apartment house anywhere in East Tennessee, if not the entire state."

7 April 1910. Johnson City Comet.

"Hundreds View the Wreck – Carnegie Hotel's Sunday Visitors – Hotel Totally Destroyed – After nineteen Years of Great Show and Little Usefulness this Landmark is Razed – From Sunday's Daily. Hotel Carnegie is no more. This magnificent hotel erected during the boom days by Gen. J.T. Wilder at a cost of \$150,000 is now in ashes.

About two years ago this building was put in order for the use of the C.C. & O. Office force and some of the officers of the kindred corporations. During the last year it was vacated by the

officials and for several months was occupied by several families as a sort of club, each pro rating the cost of mantance [maintenance].

It was finally decided by the owners of the building, the East Tennessee National Bank, of Knoxville, and other banks, to convert the building into an apartment house and it was vacated for that purpose. Everybody had moved out except M.L. Fox and family, who represented Trustee Fisher and they were preparing to vacate at once.

Shortly after midnight Mr. Fox returned to the hotel from the residence of S.E.N. Moore and discovered odors of smoke emanating from the basement. An alarm was turned in as a matter of precaution and he and others began to look for the fire. Mr. Fox in answer to a telephone query, said that there was some smoke in the basement but that no fire had been discovered, and no danger of a conflagration was feared.

Several alarms were turned in and the whole town was aroused. It seems that the fire permeated the building between the floors and in partitions and had such headway that when it finally broke out the building was doomed.

As the Comet goes to press at 3 o'clock the ruddy glare of the flames can be read in the sky for miles.

It is impossible to save the building and no effort is being made to do so, but the office building across the street occupied by the C.C. & O. officials will be unharmed.

This hotel was opened for business on Sept. 27, 1891. In the daily Comet of that date we find this statement in reference to the hotel:

'The auspicious beginning of this magnificent enterprise marks a new and important epoch in our history. It is a proud and enduring moment to far-seeing business genius and tact, the energy, the hopefulness, and the confidence and courage of capital which have transformed, in a few brief years, a struggling, unsightly village to a gem city, bristling with life and activity and full of incentives and aspirations for the future.'

It was opened and operated for some months by R.W. Farr, of Philadelphia, now the popular manager of the Imperial hotel at Knoxville."

6.3. McCorkle Lumber Company, 1924 – 1928
At the intersection of First Avenua and Centre Street

This enterprise was served by a spur of the Southern Railway.

In 1924 and 1925, this McCorkle Lumber Company facility was built on land previously occupied by the Hotel Carnegie. After 1928, this land was acquired and cleared by the Empire Chair/Furniture Company, after which a new facility was built upon it.



Figure 53E. A detail of what appears to be the McCorkle Lumber Company, which, from 1925 to 1928, occupied the land previously occupied by the Hotel Carnegie. Note the structure in the lower right of this image, which appears to be part of the Johnson City Shale Brick Company.



Figure 53F. A detail of what appears to be the saw mill and lumber yard of the McCorkle Lumber Company, 1925 – 1928.

6.3. McCorkle Lumber Company, 1924 – 1928

December 1924. Packages.

“Johnson City, Tenn. – McCorkle Lumber Co., M.R. [“Merton Raymond”] McCorkle, president, contemplates building plant, installing band mill and other machinery for the manufacture of dimension stock; will probably move plant from Nora, Va.”³⁰⁶

4 December 1924. Manufacturers Record.

“Tenn., Johnson City – McCorkle Lumber Corp., M.C. McCorkle, Pres. – Mgr., will erect 30 x 100 ft. building, composition roof, estimated cost \$15,000; move plant from Nora, Va.; install engine and hog; daily output 30,000 ft. hardwood lumber.”³⁰⁷

12 December 1924. The Lumber Manufacturer & Dealer.

“Business Changes and New Enterprises... Johnson City – McCorkle Lumber Co. recently began.”³⁰⁸

10 December 1924. Lumber World Review.

“Johnson City, Tenn. – McCorkle Lumber Co. will erect band mill and lumber manufacturing plant.”³⁰⁹

15 October 1925. The New York Lumber Trade Journal.

“Mart McCorkle, of the McCorkle Lumber Company of Johnson City, Tenn., was a visitor in Baltimore October 2 and called on some of the hardwood men. He stated that the business of his company was increasing steadily and that conditions in the trade had appreciably improved.”³¹⁰

February 1928. The Lumber Manufacturer & Dealer.

“Johnson City – McCorkle Lumber Corp. removed to Swords Creek, Va.”³¹¹

Note that the Empire Chair Company expanded into the are occupied by the McCorkle Lumber Company, after it departed in 1928.

7. The Area Occupied by the Carnegie Iron Furnace and Related Enterprises,
1887 to no later than 1938

Carnegie Iron Furnace, 1887 – 1905
Located near the intersection of Steel Street and First Avenue.

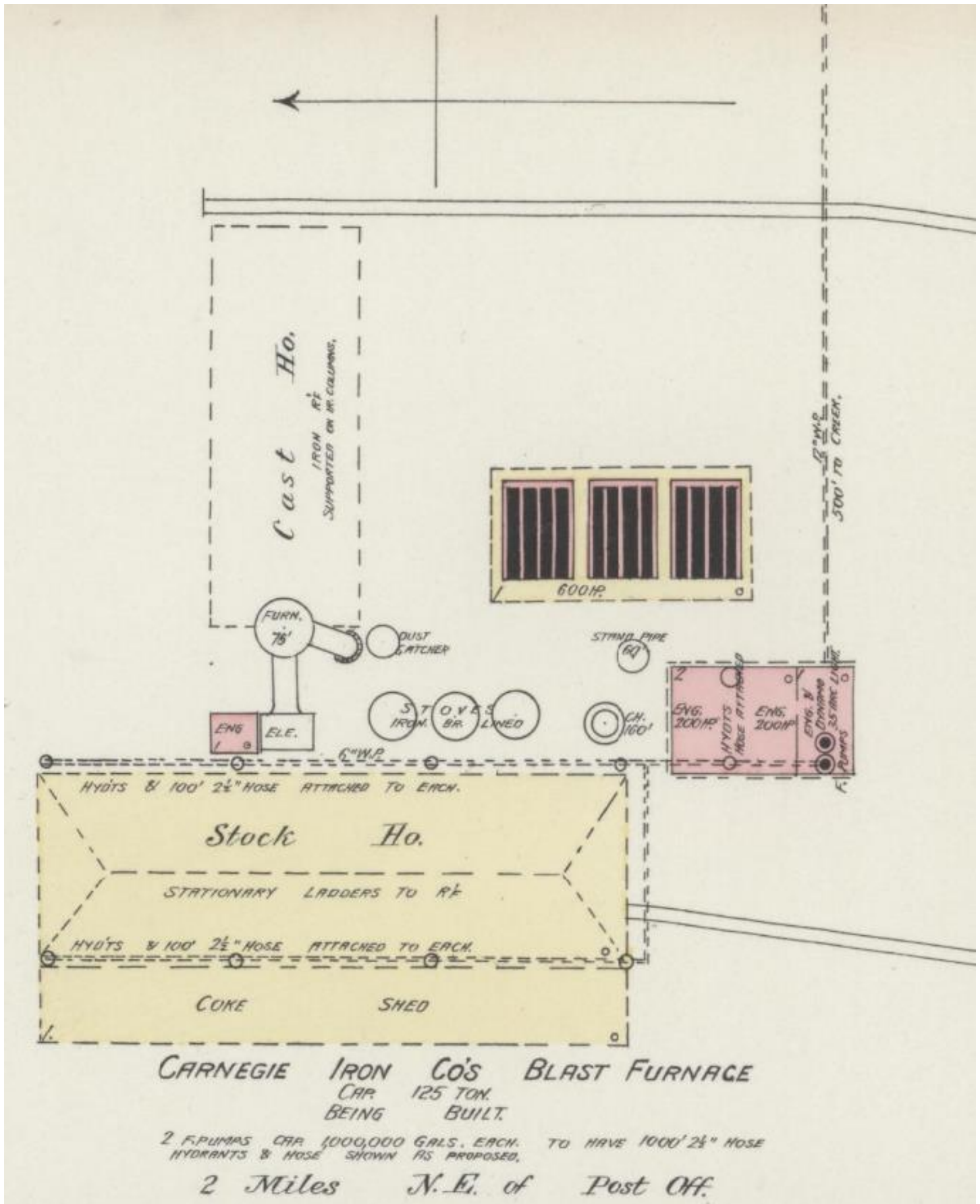
Owners:

Carnegie Iron Company, 1887 – 1895
Wilberforce Sully, 1895 – 1898
George Abingdon Parker (of England), 1898 – 1899
Virginia Iron, Coal and Coke Company, 1899 – 1905

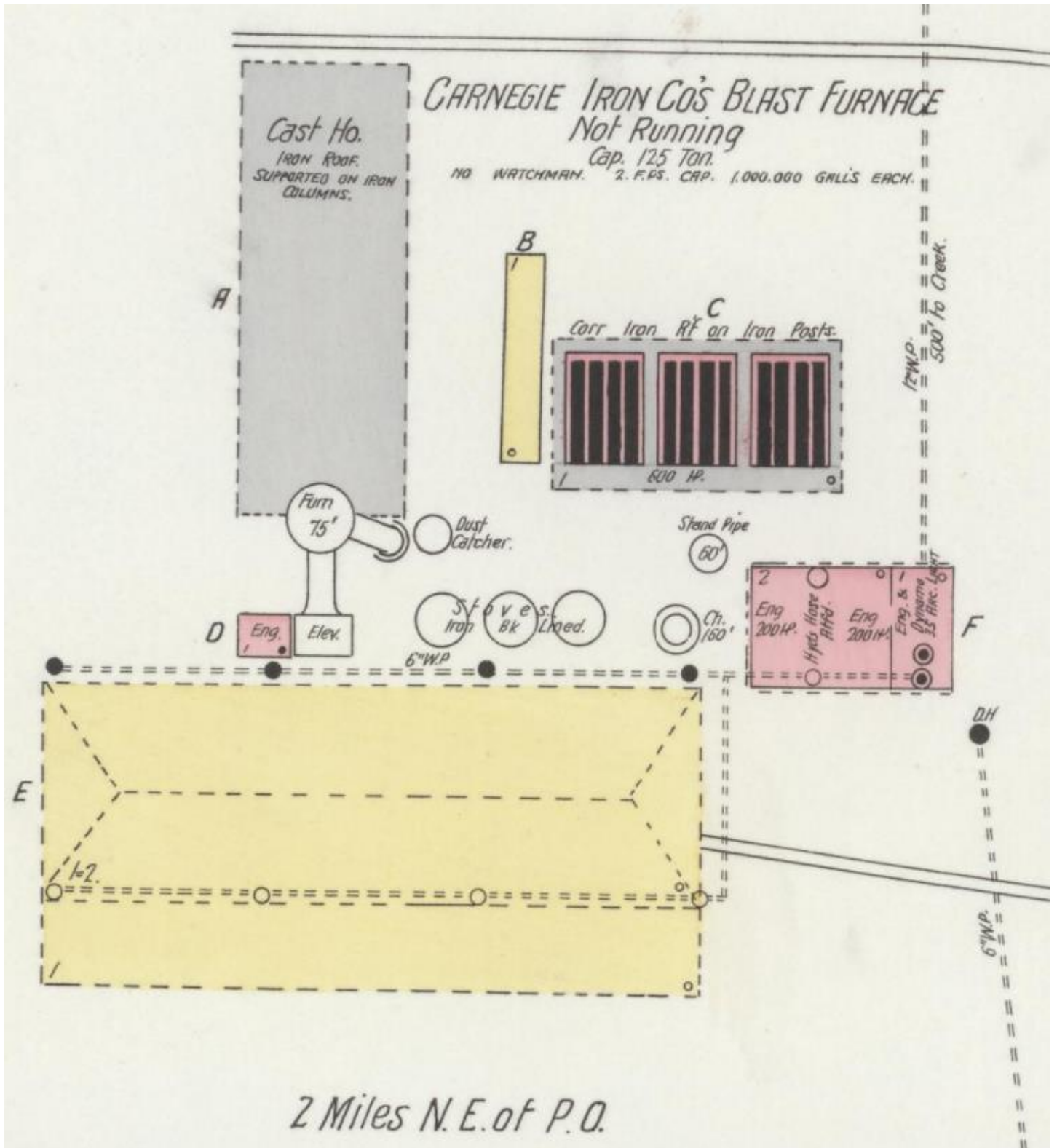
Cranberry Iron Furnace, Operated by the Cranberry Iron & Coal Company,
1901 (leased; purchased 1905) to at least 1929

This enterprise was served by the E.T.V. & G./Southern by 1892, the E.T. & W.N.C. in 1899, and the South & Western/Clinchfield by 1903.

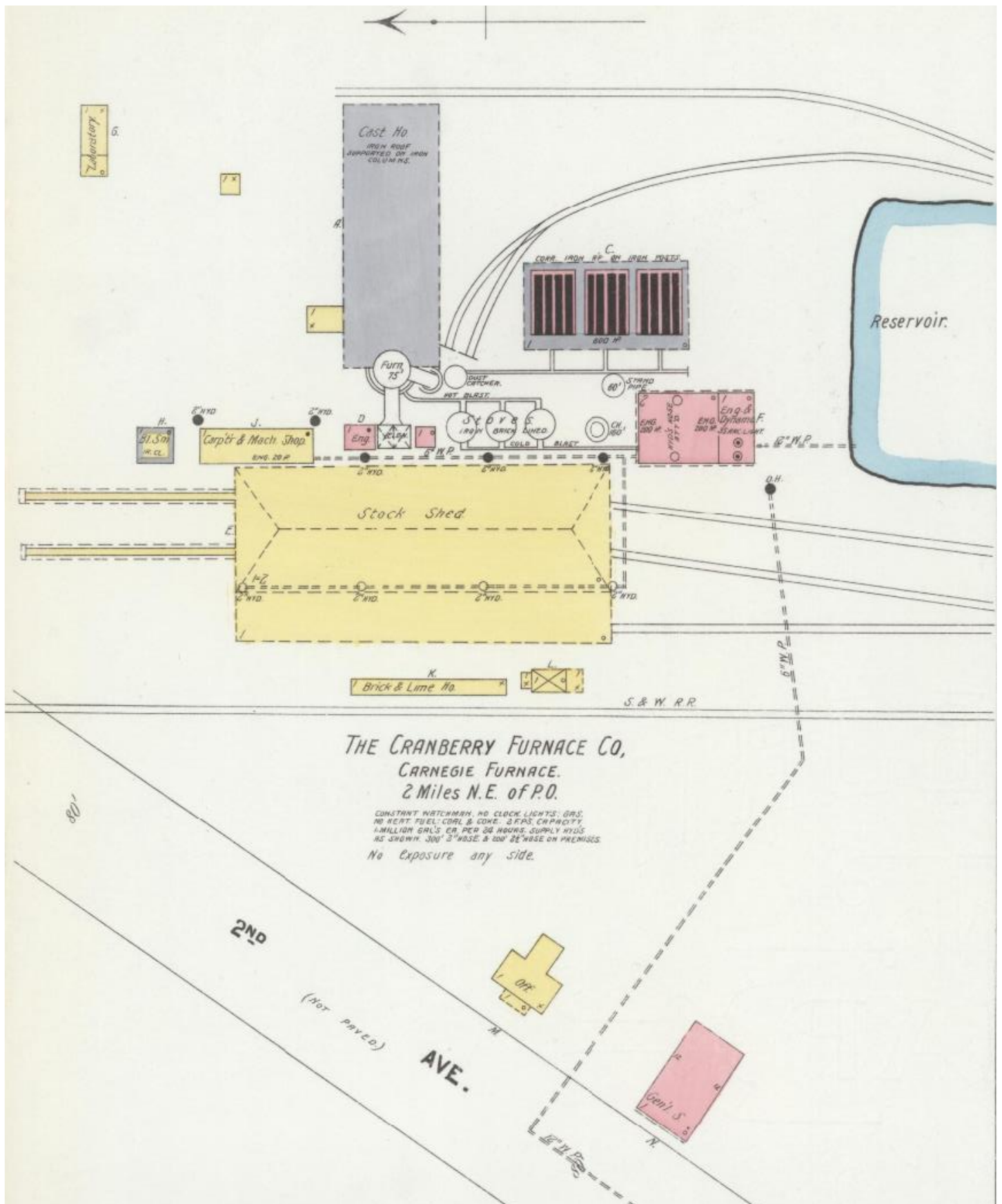
Apparently, the bulk of this plant was demolished by 1938



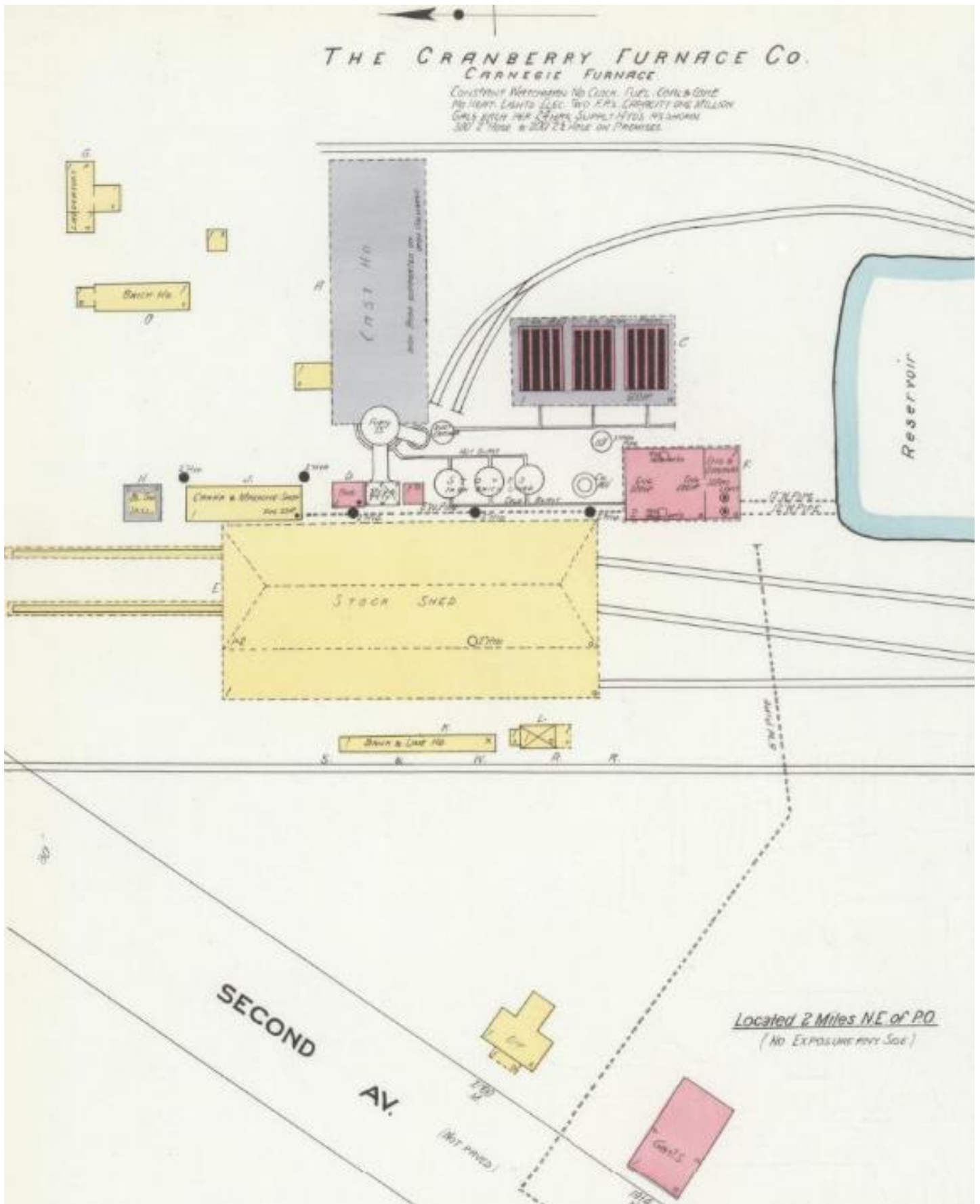
Map 25. "Cranberry Iron Co.'s Blast Furnace... Cap. 125 ton. Being Built", January 1891.³¹²



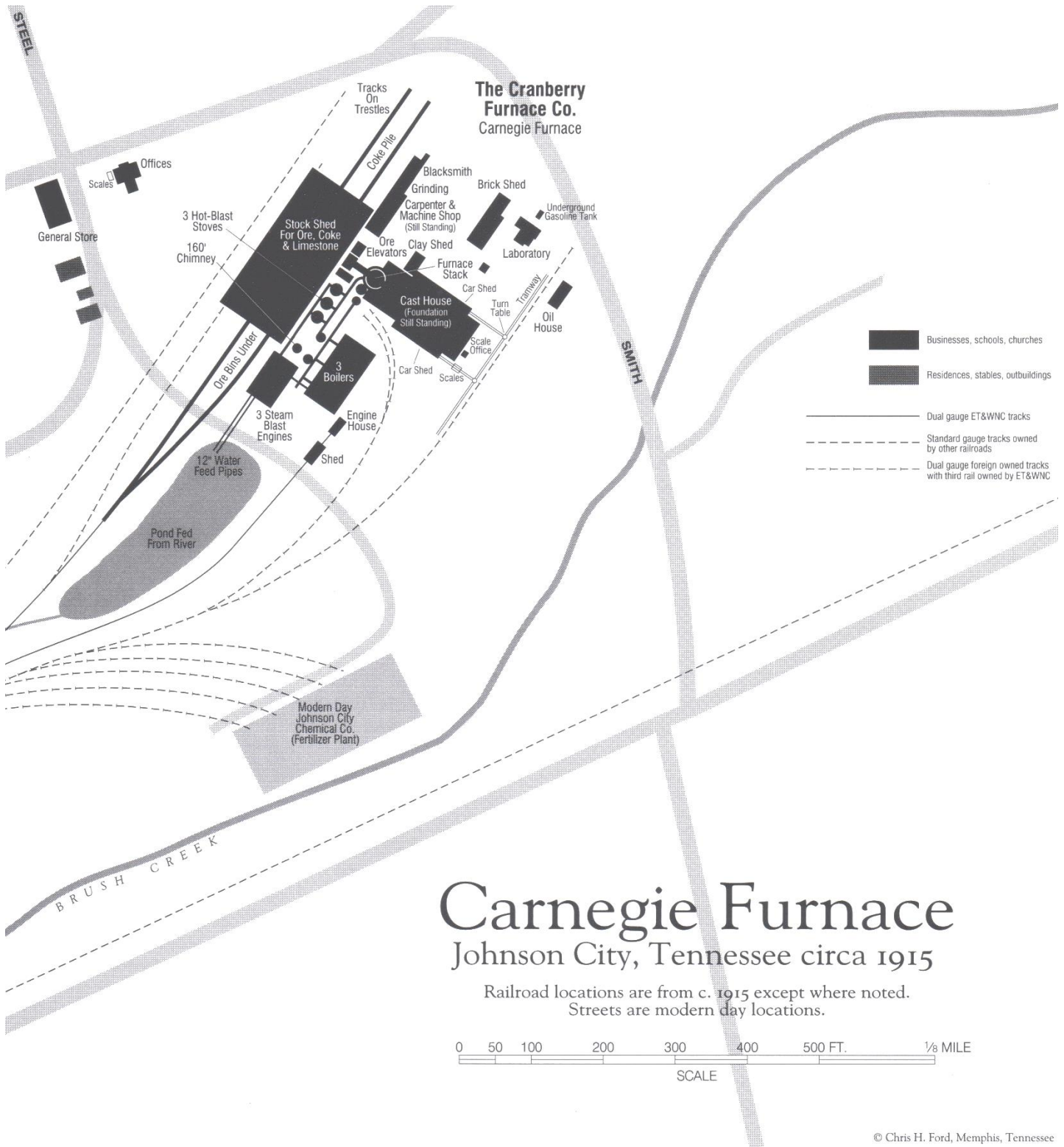
Map 26. "Carnegie Iron Co.'s Blast Furnace... Not Running," May 1897.³¹³



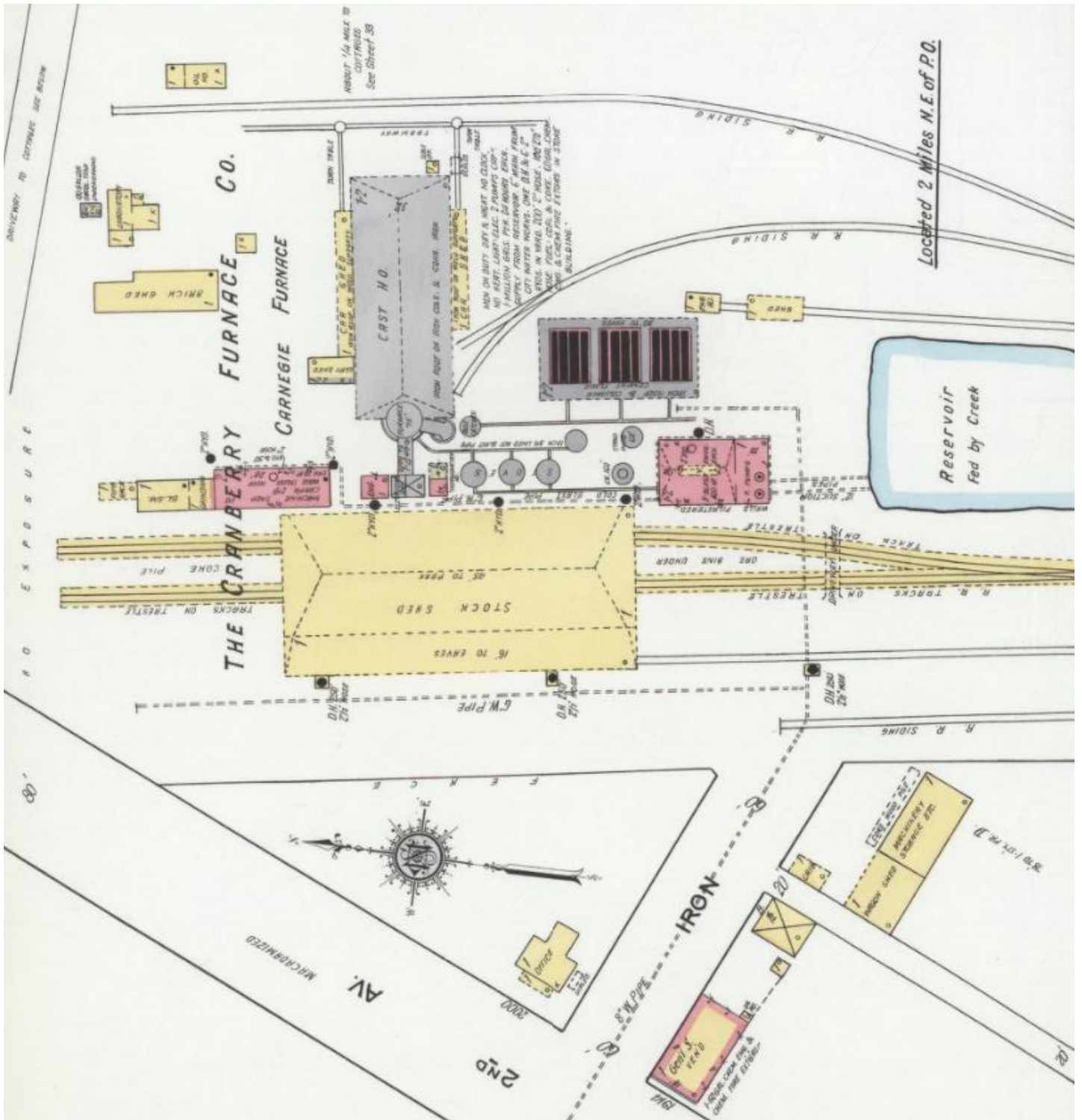
Map 27. "The Cranberry Furnace Co., Carnegie Furnace," April 1903.
 Note the General Store at the bottom of the map.³¹⁴



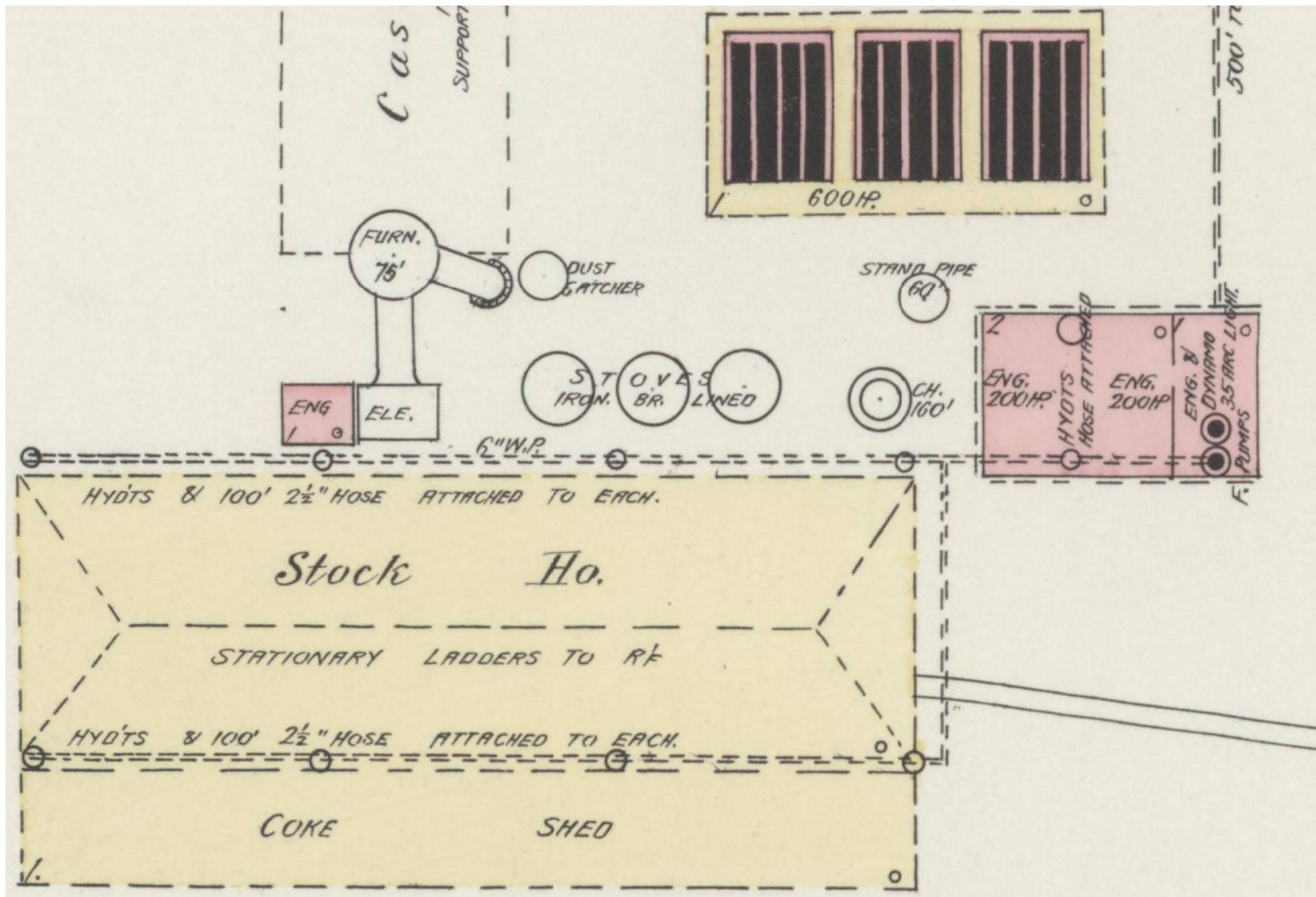
Map 28. "The Cranberry Furnace Co., Carnegie Furnace," December 1908.³¹⁵



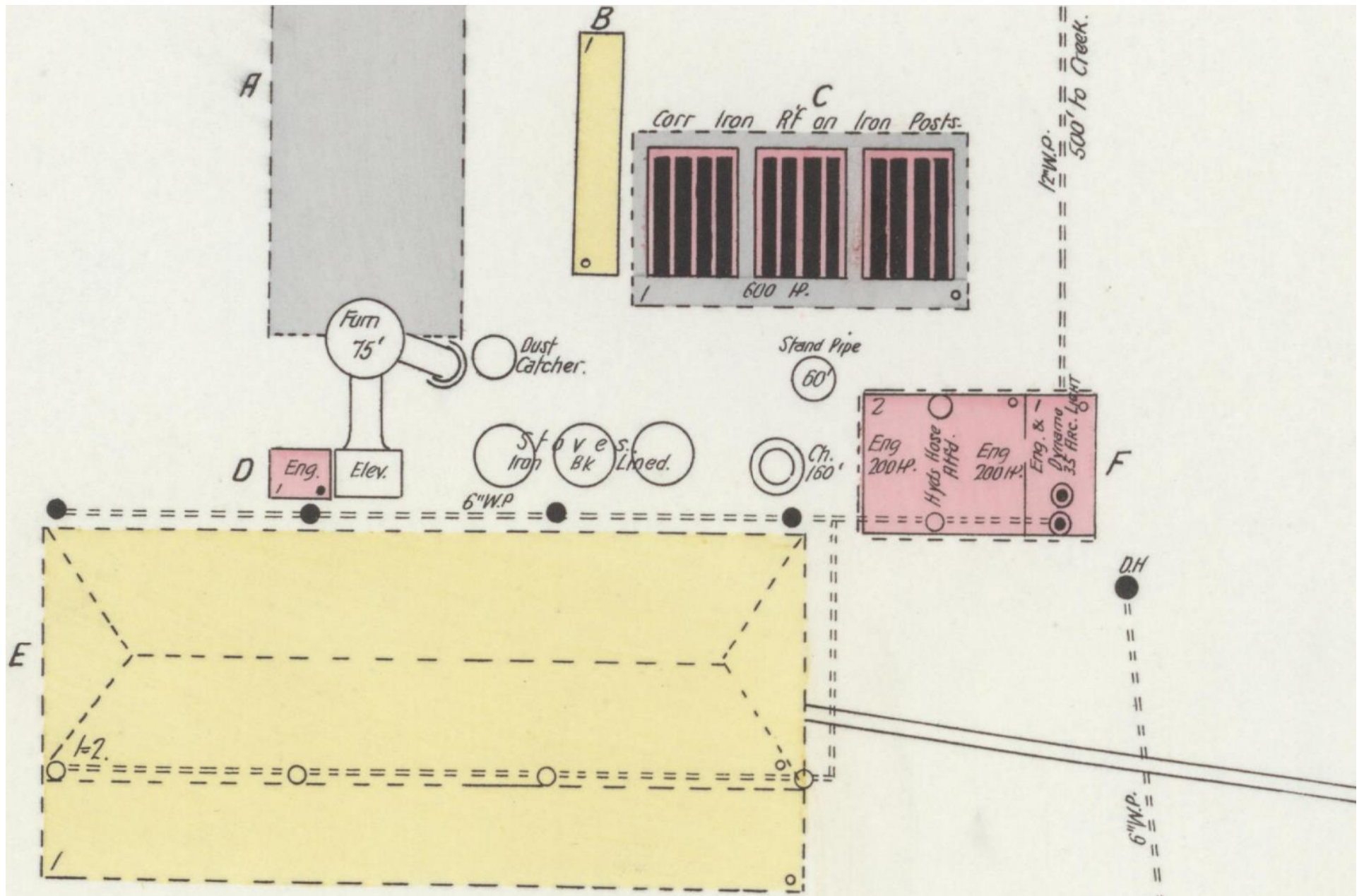
Map 30. "Carnegie Furnace, Johnson City, Tennessee circa 1915", by Chris Ford. This map labels the buildings and their contents in a helpful way.³¹⁷



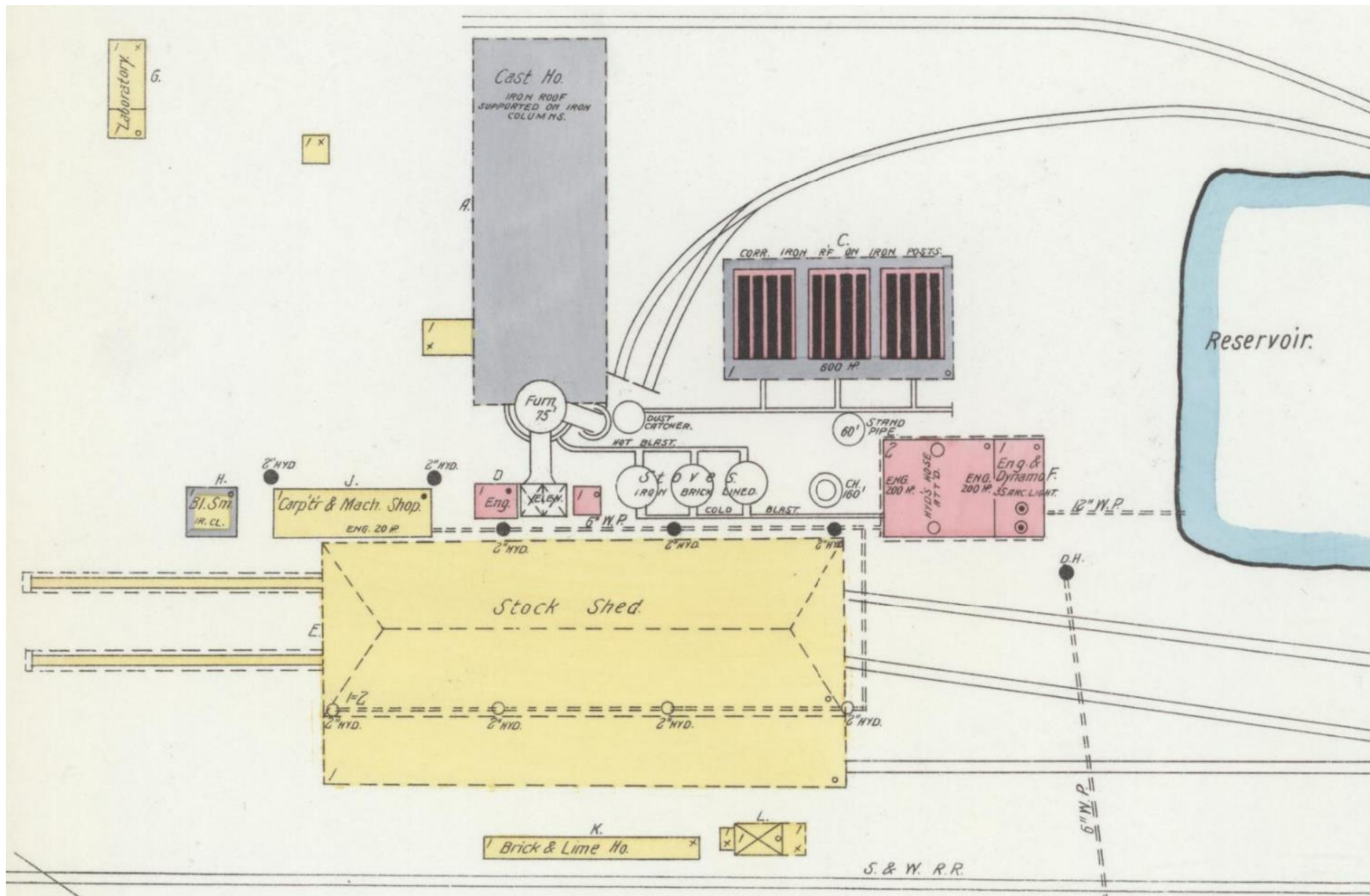
Map 31. "The Cranberry Furnace Co., Carnegie Furnace," February 1920.³¹⁸



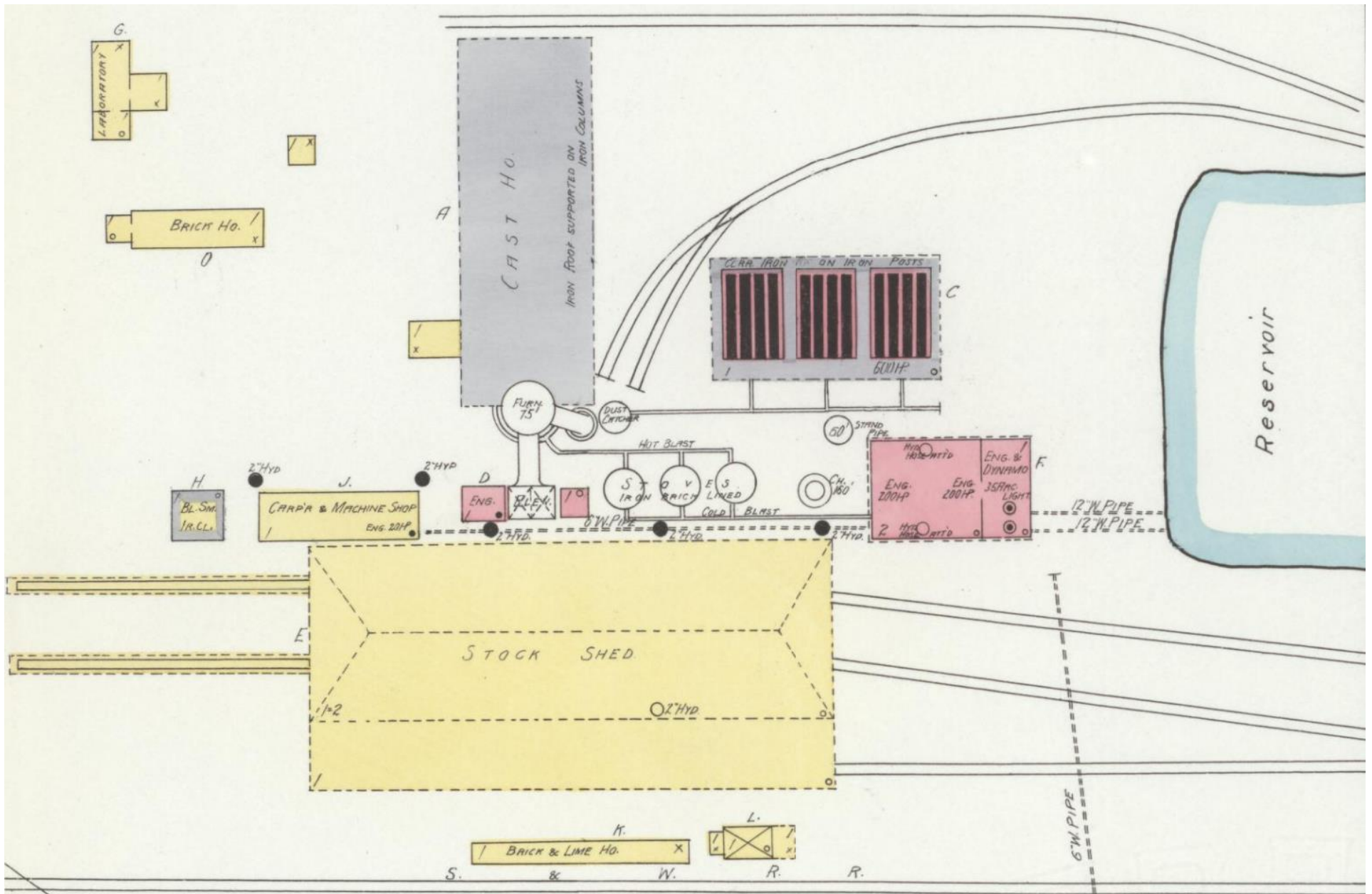
Map 25A. A detail of the "Carnegie Iron Co's Blast Furnace", in January 1891.



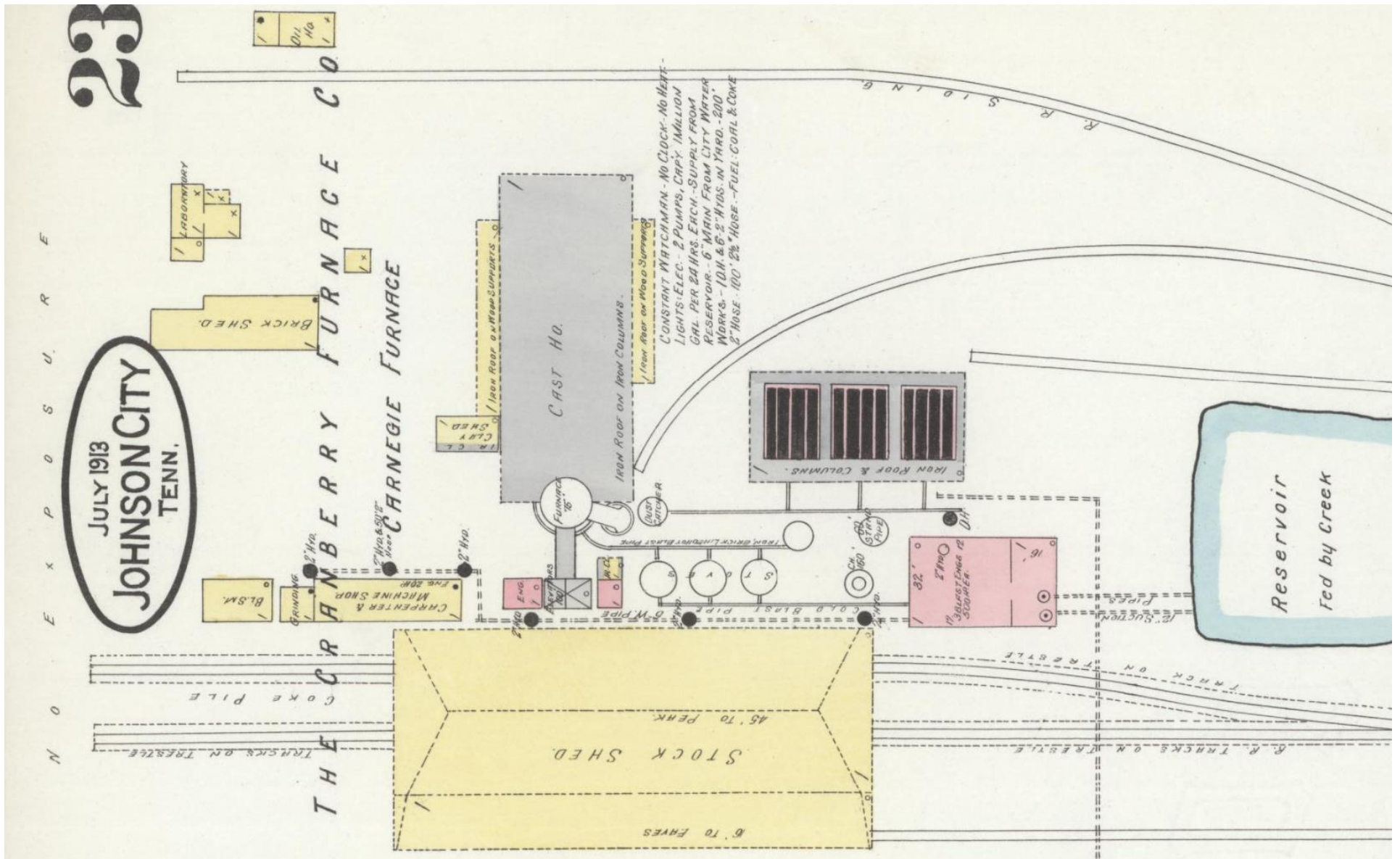
Map 26A. A detail of the “Carnegie Iron Co’s Blast Furnace” “Not Running”, May 1897. Except for the addition of building “B”, there does not appear to be any major changes since 1891.



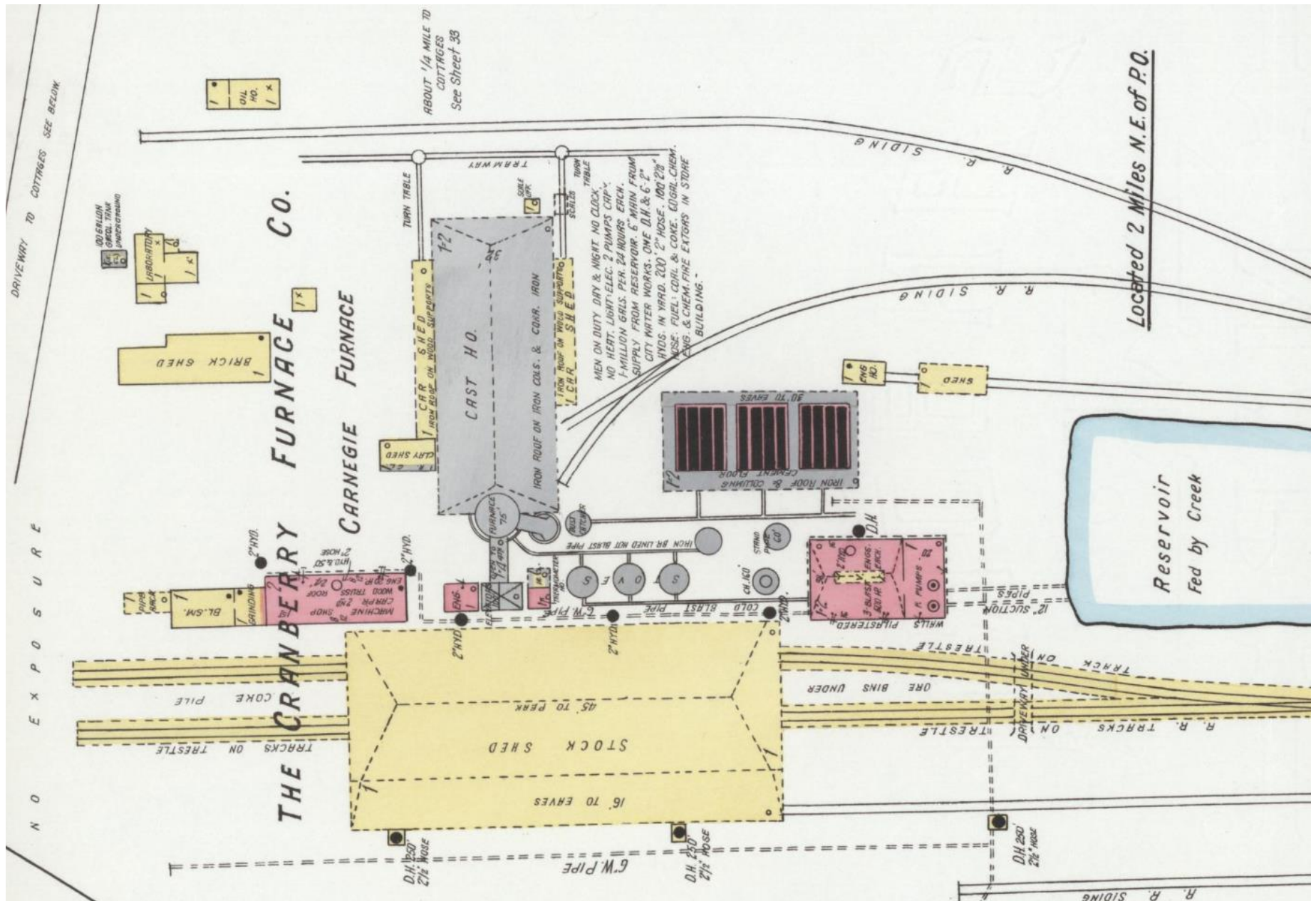
Map 27A. A detail of “The Cranberry Furnace Co, Carnegie Furnace”, in April 1903. Note the removal of building “B”, which was depicted on the May 1897 edition of this map, and the addition of a “Laboratory”, black smith facility, a “Carp’t'r & Machine Shop”, a “Brick & Lime Ho.” and three other small undesignated buildings, as well as railroad spur to the “Cast Ho.” and trackage and trestles in and around the “Stock Shed.”



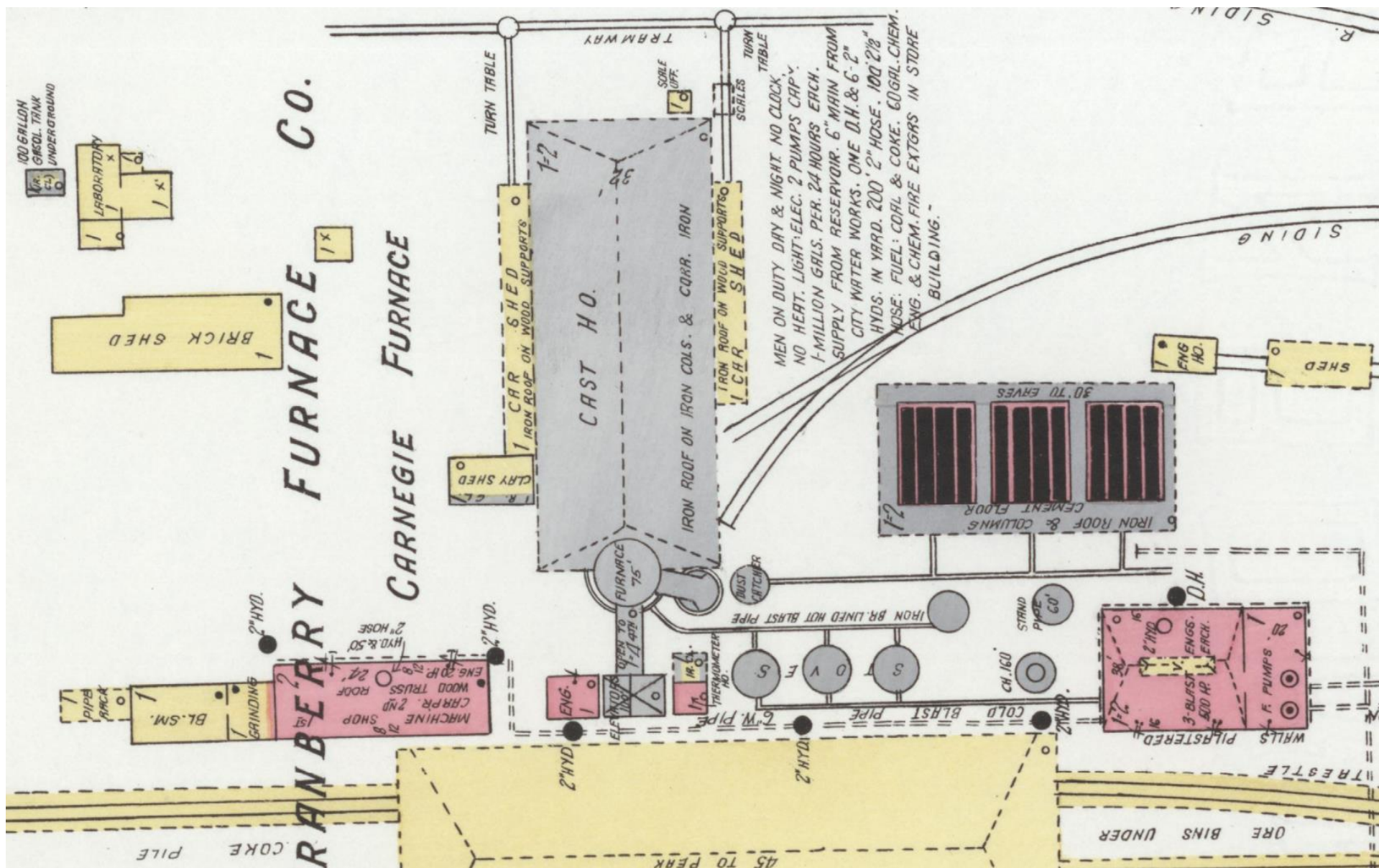
Map 28A. A detail of "The Cranberry Furnace Co. Carnegie Furnace", in December 1908. Note the addition of the "Brick Ho." and an expansion of the "Laboratory".



Map 29A. A detail of “The Cranberry Furnace Co Carnegie Furnace”, in July 1913. Note that an “Oil Ho.” has been added and that a “Grinding” room has been added to the “Carpenter & Machine Shop”; also, the number of blast engines has been increased from two to three. Additionally, minor changes appear to have been made to the “Brick Shed”, the “Laboratory”, and the “Cast Ho.” It also appears that changes have been made to the trackage in the plant. Finally, note that the “Brick & Lime Ho.” has been removed and a fourth stove added.



Map 31A. A detail of “The Cranberry Furnace Co. Carnegie Furnace”, in February 1920. Interesting changes include the addition of a tramway around the “Cast Ho.” and an “Eng. Ho.” and shed on the tracks by the reservoir.



Map 31B. Another detail of “The Cranberry Furnace Co. Carnegie Furnace”, in February 1920.

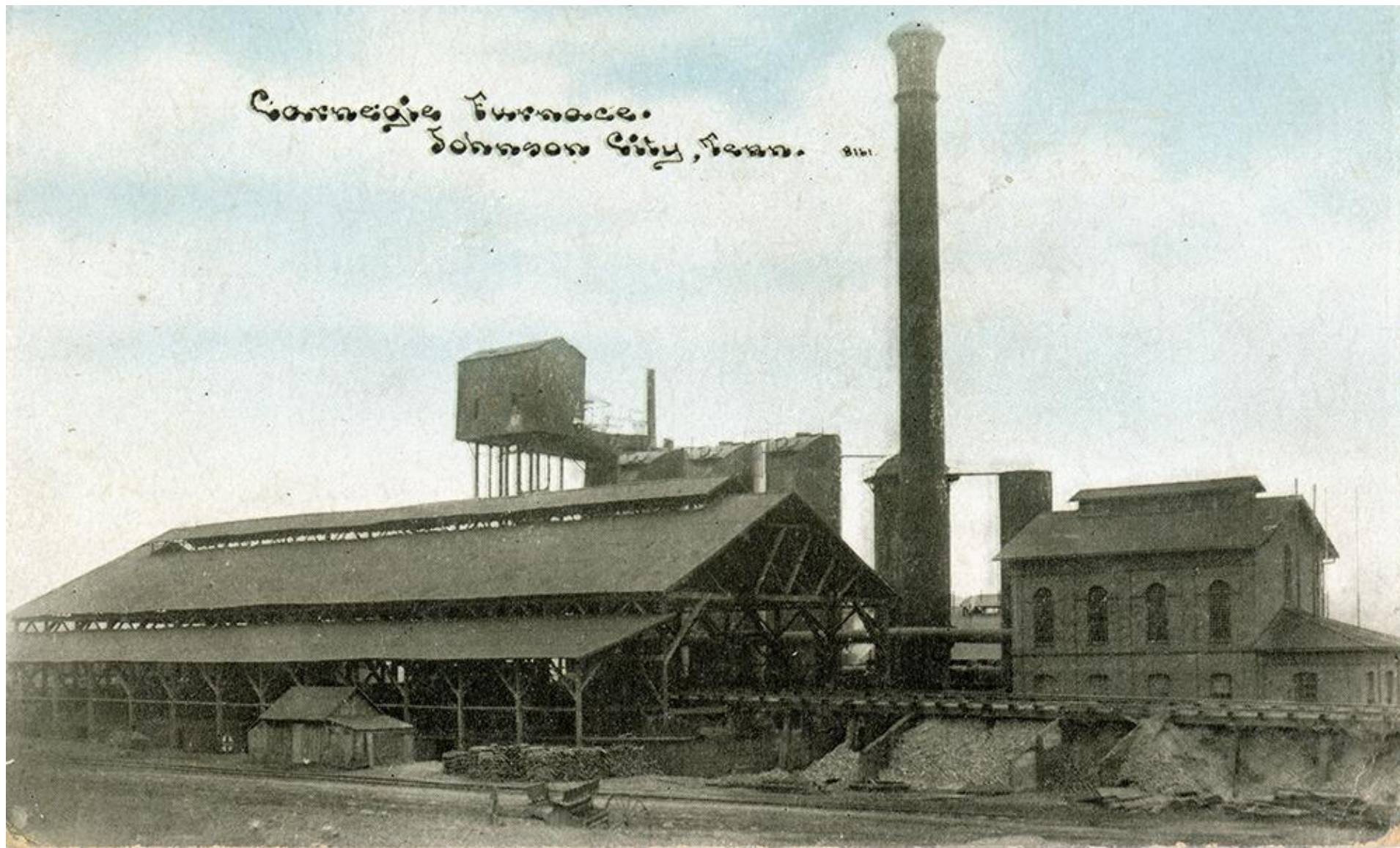


Figure 78. "Carnegie Furnace," postmarked 29 July 1913 (?). On the left of the image is the "Stock Shed" with two railroad tracks on trestles entering it from the right (note what appears to be a Santa Fe boxcar in the "Stock Shed"). The small building to the right of the boxcar appears on Map 27A (note that the "Brick & Lime Ho." is not present). The structure above the "Stock Shed" appears to be the elevator to the 75' tall furnace. The silo-shaped structures to the right of the elevator appear to be the four "Iron Brick Lined" "Stoves". The next structure to the right appears to be the 160' tall chimney, with a 60' "Standpipe" to its right. Finally, the brick structure on the right appears to be the one that held the "steam blast engines".³¹⁹

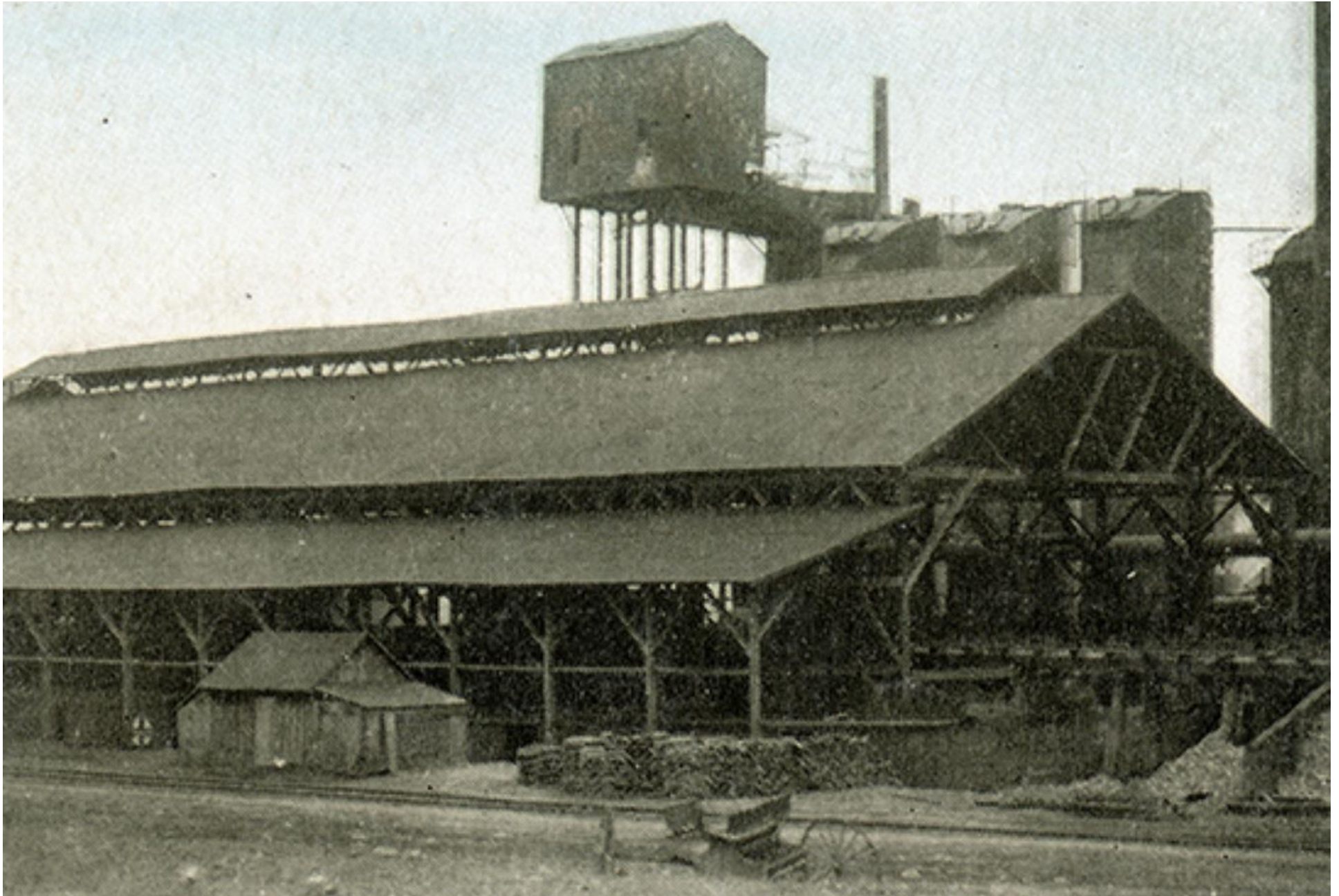


Figure 78A. A detail of the “Stock Shed”, elevator, and four “Iron Brick Lined” “Stoves”. Note what appears to be a carriage in the foreground.

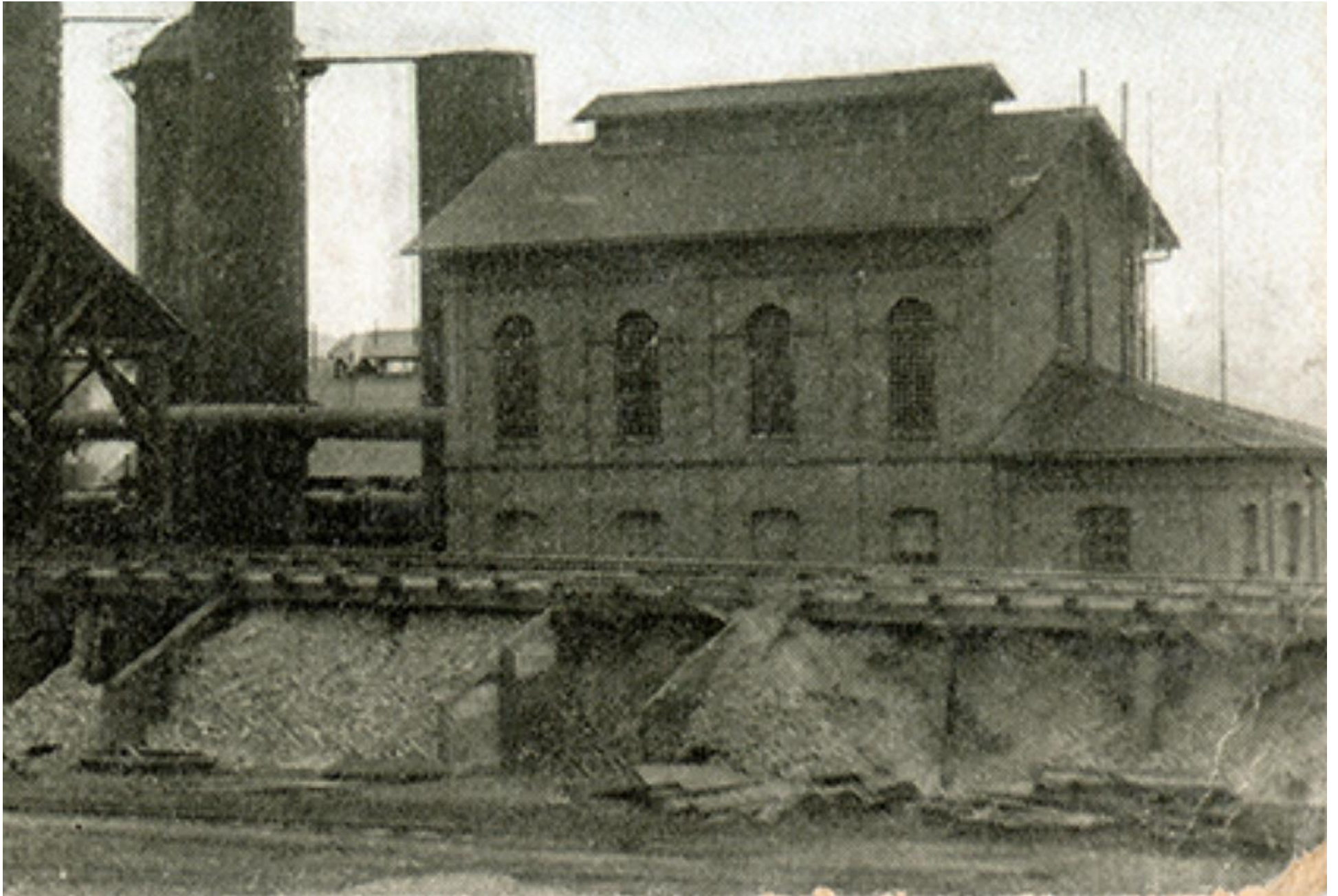


Figure 78B. A detail of the brick structure that held the two or three “steam blast engines” and the 60’ “Standpipe”, to its left.



CRANBERRY FURNACE, BY NIGHT, JOHNSON CITY, TENN.

Figure 79. “Cranberry Furnace, by Night, Johnson City, Tenn.”, by May 1925. The structure to the right is the boiler house.³²⁰



Figure 80. "View of Carnegie Furnace, built in 1890 and operated by the Cranberry Iron Company until 1929. This photograph is undated, but was probably taken at some point in the 1920s." Note the Southern Railway mainline in the foreground.³²¹



Figure 80A. A detail of the Cranberry Furnace. This image depicts, from left to right, the two-track trestle leading into the “Stock Shed”, the engine house (containing two or three “Steam Blast Engines”, the 160’ chimney, the boiler house (with three boilers) with the ore elevator to the top of the 75’ furnace behind it, the “Cast Ho.” with what appears to be a “Car Shed” (added between 1908 and 1909) beside it, the “Laboratory”, and the small “Oil Ho.” Note that the “Eng. Ho.” and “Shed”, depicted on Map 31B (dated February 1920) have been removed. Also note what appears to be a relatively new section of corrugated roofing on the “Cast Ho.” This photograph appears to post-date Figure 82, which dated to 1919 – 1921. Revised 24 September 2024.



Figure 80B. A detail of the “Stock Shed” and the engine house (containing two or three “Steam Blast Engines”). Note the dual-gauge track and flatcar on the right side of this image.



Figure 80C. A detail of the boiler house (with three boilers) with the ore elevator to the top of the 75' furnace behind it.



Figure 80D. A detail of the “Cast Ho.,” with what appears to be a “Car Shed” (added between 1908 and 1909) beside it, and the “Laboratory” (on the right side of this image). Note that the “Cast Ho.” is open to the elements at its base and appears to have a relatively new section of corrugated roofing. Revised 24 September 2024.

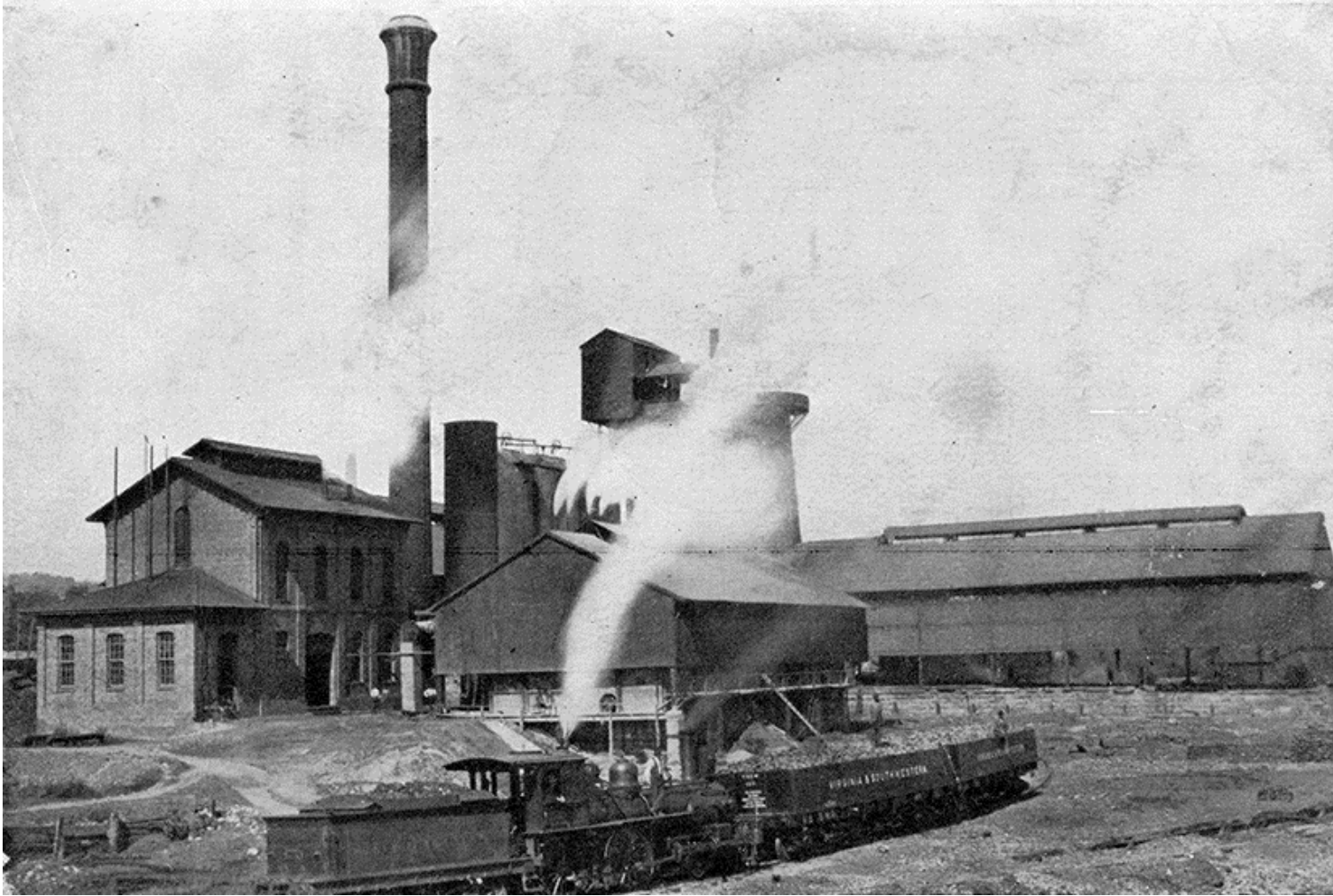


Figure 81. Historian L. Thomas Roberts captioned this photograph: “Cranberry Furnace. Originally named the Carnegie Furnace, it processed iron ore brought on the narrow-gauge railroad from Cranberry, North Carolina. Built in 1890 by Harry Hargraves, who was hired from Alabama, the new furnace was constructed just east of the Carnegie Addition, two miles from downtown Johnson City. Hundreds of Italian immigrant and black workers were hired to build the furnace. There were so many Italians coming in by train every day that the Comet noted that ‘in a few weeks a stranger coming to Johnson City will think he is in Italy.’ When finished, the furnace was believed to be the second Bessemer-type blast furnace constructed in Tennessee (behind Chattanooga in 1887) and potentially in the entire South.”³²² The author has not identified the locomotive, perhaps it was one of the switch engines owned by the furnace company. The first gondola is marked “Virginia & Southwestern”. Given the lack of the “Car Shed” by the “Cast Ho.” and a fourth stove, this photograph appears to have been taken before 1908.



Figure 81A. A detail of the engine house, containing two or three “Steam Blast Engines”.

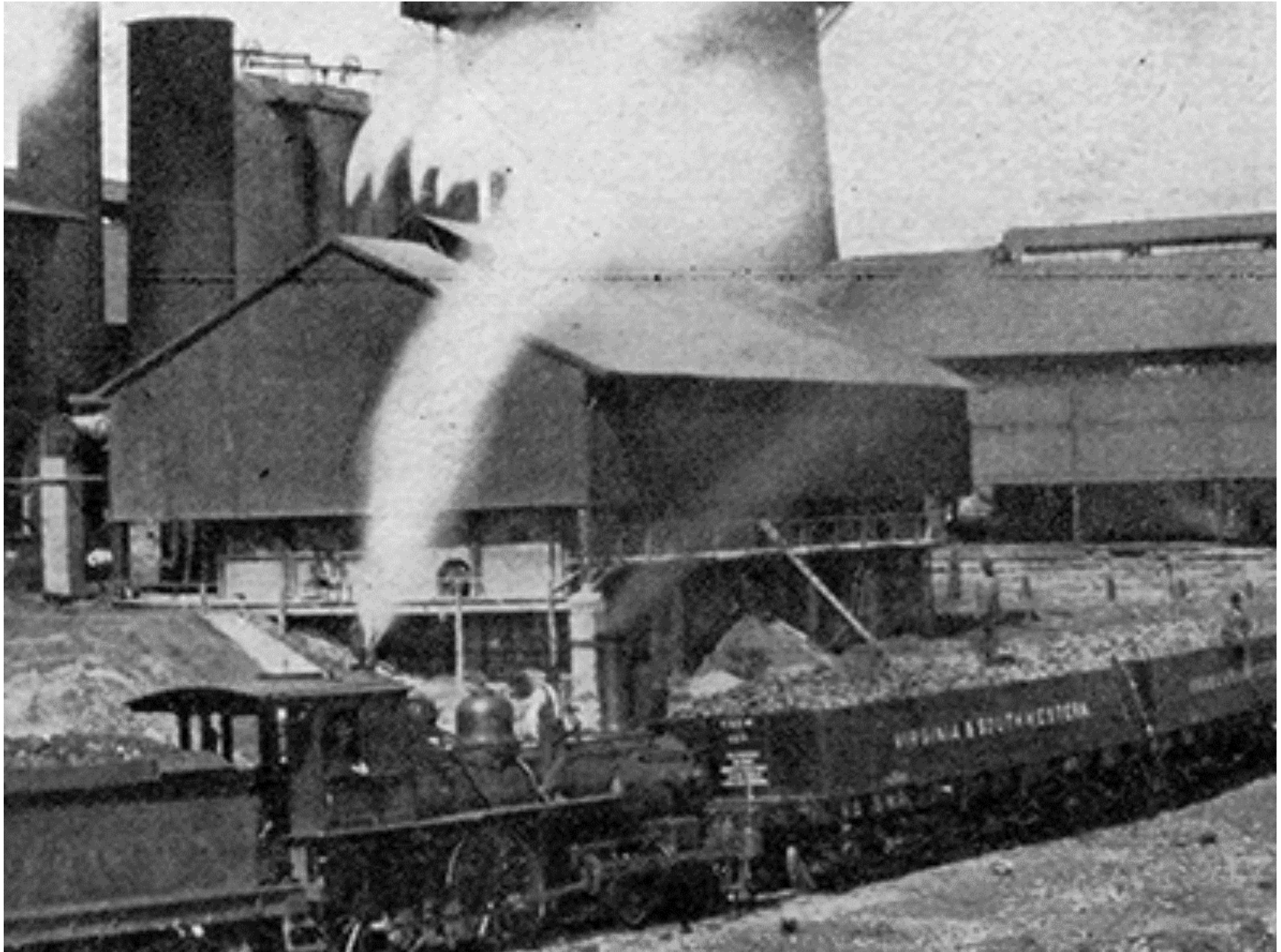


Figure 81B. A detail of the boiler house (with three boilers).



Figure 81C. A detail of the "Cast Ho."

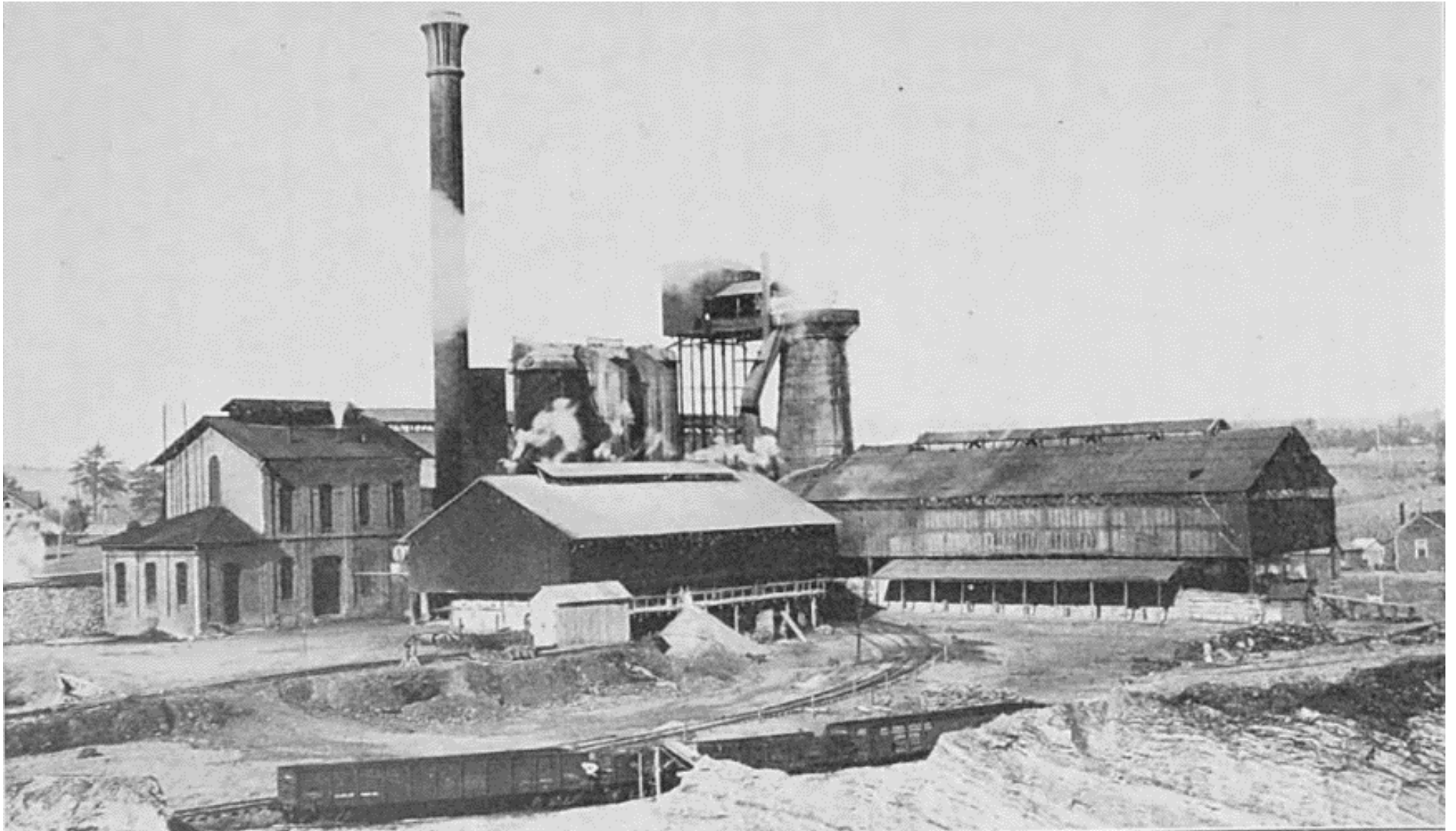


Figure 82. “Cranberry Furnace, Johnson City, Tenn.”. Note the building in the left background, which may be the office or general store of the Cranberry Furnace. Note that the dual-gauge spur to the building containing the three “Steam Blast Engines” has been removed, and that a building has been added in front of the boiler house, which appears to be the “Engine Ho.” that was built between 1913 and 1920. Also note the uniform color of the roof of the “Cast Ho.”, as well as the increased amount of dirt fill in the foreground in comparison with that depicted in Figure 80. A report prepared by the Division of Geology, State of Tennessee, indicates that this photograph was taken between 1919 and 1921. Additionally, this photograph appears to predate that in Figure 80 and post-date that in Figure 81.³²³ Revised 24 September 2024.



Figure 82A. A detail of the boiler house, the “Eng. Ho.,” the chimney, the elevator and the furnace.³²⁴ Revised 24 September 2024.



Figure 82B. A detail of the “Cast Ho.” Note that the small building against the right side of the “Cast Ho.” appears to be the “Scale Off.” with the tramway nearby. Two “Car Shed” or “shed roofs have been added to the stock [should read “Cast”] house... to cover the trams that carried ingots out to the railroad spur track”, see Map 31B.³²⁵

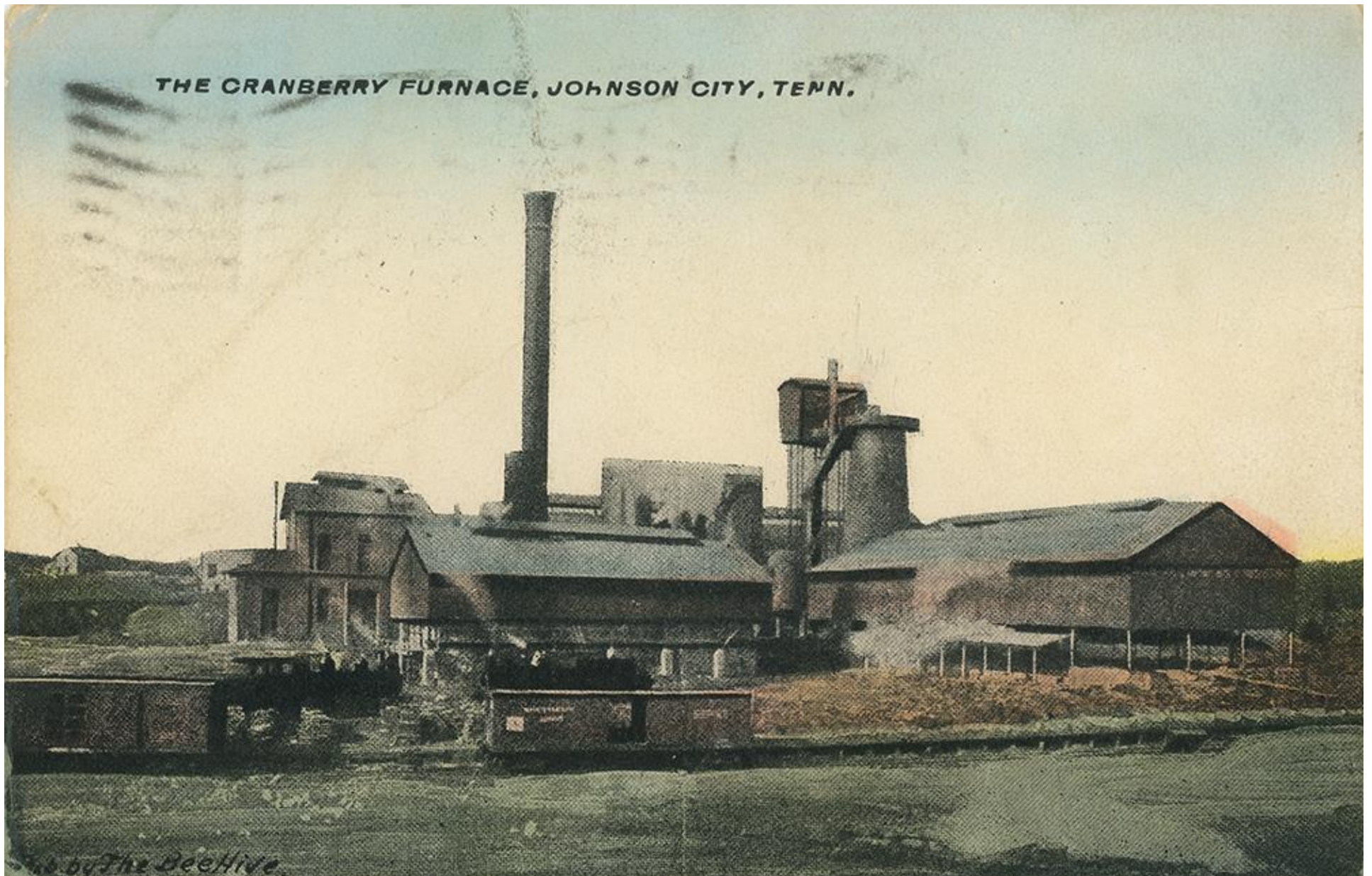


Figure 83. "The Cranberry Furnace in Johnson City is in full blast, c. 1908. Two locomotives are busy switching in the yard, between the two boxcars and the boiler house... The downcomer and dust catcher are clearly visible in this view. Burr Harrison Collection, Archives of Appalachia, East Tennessee State University." Note the buildings in the left background, which may be the office and/or general store of the Cranberry Furnace.³²⁶

Carnegie Iron Furnace, 1887 – 1901

Owners:

Carnegie Iron Company, 1887 – 1895

Wilberforce Sully, 1895 – 1898

George Abingdon Parker (of England), 1898 – 1899

Virginia Iron, Coal and Coke Company, 1899 – 1905

Cranberry Iron Furnace,

Operated by the Cranberry Iron & Coal Company, 1901 (leased; purchased 1905) to at least 1929

Demolished by 1938

See also: “Iron Furnace Operations,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., Fall/Winter 2003 (Volume 14, Number 2), pages 7 – 24, which contains scale drawings of the standard design to which the Carnegie Furnace was built.

7.1. Carnegie Iron Furnace, 1887 – 1901

“Carnegie (Cranberry) Furnace”, by John R. Waite.

“John T. Wilder was a young family man with a growing millwright business in Greensburg, Indiana when the Civil War broke out. He quickly enlisted in the First Independent Battery of Artillery, an Indiana volunteer unit, and was elected captain the next day. Within two months, he was promoted to lieutenant colonel of the 17th Indiana Volunteer Infantry which would later be known as ‘The Lightning Brigade’.

Over the next three years, Wilder led troops in over two hundred engagements and moved up through the ranks, eventually reaching the rank of Brigadier General. He resigned his commission in October 1864, as a result of a health problem. Wilder’s wartime service provided him with connections and knowledge that would serve him well over the next half century.

In 1866, Wilder moved his family from Indiana to Chattanooga, Tennessee. A year later he and two associates, Major W.A. Rockwood and Captain H.S. Chamberlain, organized the Roane Iron Company. They constructed the first coke-fired blast furnace in the south, at Rockwood in Roane County between Chattanooga and Knoxville. Wilder then established the Roane Rolling Mills Company in Chattanooga for the manufacturing of railroad rails.

Wilder expanded his mining and manufacturing interests over the next few years, becoming one of the south’s leading industrialists. In the early 1870s, Wilder purchased seven thousand acres on the crest and slopes of Roan Mountain. He constructed a twenty room hotel of spruce logs, on top of the mountain, and named it ‘Cloudland.’ When the East Tennessee & Western North Carolina Railroad reached the area, in the early 1880s, Wilder saw the opportunity to expand his tourist business. A much bigger Cloudland Hotel was completed in 1885. He also built the Roan Mountain Inn, next to the ET&WNC depot, at the Village of Roan Mountain.

While developing his Roan Mountain properties, Wilder moved his home to Johnson City and began promoting it as a potential center of the iron and steel industry in the South. The mountains to the east had abundant deposits of iron ore. The Pocahontas coal fields were nearby

and the area had plenty of the necessary limestone. By 1888 there were plans for five blast furnaces and two steel mills in the area.

A key element in Wilder's plan for developing Johnson City was the improvement of transportation in the area. On September 30, 1886, Wilder chartered the Charleston, Cincinnati, & Chicago Railroad Company, commonly referred to as the 3-Cs Railroad. He planned to build a 621-mile line from Charleston, South Carolina to Ironton, Ohio, on the Ohio River, and then down the Ohio River to Cincinnati. Johnson City would serve both as the headquarters and as a division point on the railroad.

Wilder began lining up financial backing for the estimated \$21 million cost of the 3-Cs line. The most substantial funding source was the London-based Baring Brothers Bank. Construction was started at three different locations. One crew worked south from Ashland, Kentucky, across the Ohio River from Ironton. Another crew worked north from Camden, South Carolina, toward Marion, North Carolina. Track construction crews worked south from Johnson City toward Erwin, Tennessee, and north from Johnson City toward Dante, Virginia. The company also constructed shops and a freight station in the Carnegie section of Johnson City and began construction of a depot near the junction of Broadway Street and the parallel tracks of the CC&C and the East Tennessee, Virginia & Georgia Railroads.

Wilder organized the Watauga Improvement Company on November 30, 1888 to develop an industrial and residential section along the CC&C and ETV&G Railroads. The development was a mile northeast of downtown Johnson City. Within a few months it was renamed the Carnegie Land & Improvement Company, apparently in an effort to attract financial backing from northern industrialist, Andrew Carnegie. Carnegie quickly became a boomtown with a variety of businesses and a hotel.

In January 1890, Wilder traveled to Birmingham, Alabama to find a contractor to build a blast furnace. The person he hired was Harry Hargraves, who had constructed over thirty furnaces throughout the south. The new furnace was to be built just east of the Carnegie Addition, two miles from downtown Johnson City.

Hargraves apparently built the furnace to a relatively new design by James L. White, a consulting engineer whose plans were used to construct dozens of similar iron furnaces across the country. Drawings of White-designed furnaces appeared in several 1890 issues of the trade journal, *Iron Age*. The furnace would produce Bessemer Iron; however, the complex did not include a Bessemer converter. The iron from the Johnson City furnace would be sold and shipped to other steel mills, where it would be converted to Bessemer steel...

The Carnegie Furnace was apparently the second Bessemer-type blast furnace to be constructed in Tennessee, and possibly the South. The first was constructed at Chattanooga in 1887. Over the next few years, Bessemer furnaces were built all across the South, especially at Birmingham, Alabama."³²⁷

6 October 1883. Plateau Gazette and East Tennessee News.

"Extensive iron works are to be established in Johnson City in a few months."

28 October 1886. Johnson City Comet.

"Our New Railroad. Attention is called to the full report of the railroad meeting at Charleston on the 30th inst. From present indications it is only a matter of a few months until you can take the train in Johnson City and go to Charleston or Cincinnati without change. Then look out for Johnson's. When the coal of Kentucky and the iron ore of North Carolina meet, Johnson

City will be the Birmingham of Tennessee. Land has already been purchased on which to erect furnaces as soon as coal can be brought here at reasonable rates. It is a well known fact that the Cranberry iron makes the finest Bessemer steel and the Kentucky coal has also been found to be best suited for that purpose..."

3 February 1887. Johnson City Comet.

"Time to Wake Up. Gen. Wilder was met at the depot yesterday and in reply to how matters were progressing up at Johnson's said that the prospect for blast furnaces, foundries, etc., were so good that there could be no doubt of a number being established in that section... Tribune."

26 February 1887. American Machinist.

"Land has been purchased at Johnson City, Tenn., on which, it is reported, an iron furnace is to be built."³²⁸

24 November 1887. Johnson City Comet.

"Real Estate Transfer. Real Estate has been lively the past week. The sales have been large and numerous. Mr. F.A. Stratton, of Chattanooga, has been the largest purchaser. He bought 38 acres from Gen. Wilder, formerly the Hoss property, and has had it laid off in lots. The General reserved 12 acres for a blast furnace. Mr. Stratton will grade the streets, lay sewers, build sidewalks and otherwise improve the property. He has contracted to have 300 shade trees set out, two in front of each lot..."

11 July 1889. Johnson City Comet.

"What is to be and Will be. – A forecast of Some of the Things in Store for Johnson City. Washington, July 7. – Gen. J.T. Wilder has just returned here from New York, where he has been in the interest of various Johnson City enterprises. While East he perfected arrangements for five blast furnaces to be located at Johnson City with capacities of 150 tons each..."

Gen. Wilder returned from New York yesterday. When asked about the above article he said that it was perhaps a little premature, but arrangements had been made and the matter practically settled to erect four blast furnaces and the fifth one almost assured. He feels sure that three of them will be built within the next year..."

7 November 1889. Johnson City Comet.

"Still Stepping – Three Million Brick Contracted for. Wholesale Houses – New Buildings... It is also definitely settled that the ground will be broken early in the year for a 100-ton iron furnace. It is the intention of the company to have the furnace ready for operation by the time the Three C's road reaches the coal fields. A second furnace of the same capacity will be commenced during next year..."

12 December 1889. Johnson City Comet.

"Blast Furnace and Hotel. Gen. Wilder telegraphed yesterday from New York, that every dollar of stock for the hotel and blast furnace had been subscribed. This means work will

commence on both as soon as the brick can be made. Plans for the hotel are now being revised and the building will be more attractive.

Full particulars regarding the furnace site, capacity, etc., will be given when Gen. Wilder returns.”

26 December 1889. Johnson City Comet.

“Gen. J.T. Wilder returned Saturday night from New York. He comes authorized to begin work at once on a blast furnace, and hotel and is now contracting for brick to do the work.”

2 January 1890. Johnson City Comet.

“1890. – Some of the Things Started and Done on the First Day... Gen. J.T. Wilder left yesterday for Birmingham to contract for the erection of the furnace, lately mentioned, and has ordered Mr. Cantwell to put a force of men at work next Monday grading 40 acres of land to erect the furnace upon. Within 30 days the furnace will be under way, if they are not disappointed in getting brick.”

16 January 1890. Johnson City Comet.

“Gen. J.T. Wilder returned Monday night from a trip to Alabama. The Comet called upon him Tuesday afternoon and found him, as president of the Carnegie Land Company, signing a number of deeds to property that had been sold during his absence amounting to \$17,300. He stated that arrangements have been completed to begin the furnace, and that the land will be laid out and graded at once. His business in Alabama was to get a practical furnace builder to take charge of the work as he could not give it his personal attention.”

13 February 1890. Johnson City Comet.

“Work Commenced – On a 125-ton Blast Furnace – The First Built in East Tennessee. The Comet has repeatedly said that Johnson City would be a great iron manufacturing city and that blast furnaces would soon make night for the Queen City of the Central South, a thing in name only. As evidence of our faith in this belief we have had a heading engraved from an original design and a blast furnace is one of the principal features.

The predictions of the Comet and the fondest hopes of our citizens are rapidly being realized. The Carnegie Iron Company under the guidance of Gen. J.T. Wilder, its president, and the man who built the first blast furnace in the South, has commenced a 125-ton blast furnace in Johnson City, and as soon as it is finished a companion of the same dimensions will be commenced.

Everybody in Johnson City is happy to-day, and the Comet shares in the good feeling. We don't mean that there is to be a meeting of the directors and locate the site of the furnace or elect officers, or that there has been a telegram received that the company will build at once, but that work has actually begun. Mr. Harry Hargrave, of Birmingham, has the contract and he is here and has about completed the draughting and Monday masons will begin work. Preliminary work is now going on and an office building for the superintendent of construction was begun today, and will be completed in ten days. A number of small dwellings have been let to contract and will be put up at once near the furnace for the workmen. Mr. Hargrave will occupy the Hampton dwelling on Myrtle street. Mr. Hargrave is an experienced furnace builder, although quite a young man. He

has built 24 furnaces in the last twelve years and before two years more, will have made the total of 26; the last two under contract for the Carnegie Iron Company.

As soon as the furnace gets well under way, employment will be given to about 500 men and the two furnaces when completed will employ about one thousand men, mostly skilled laborers. In the mean time, the five hundred men required to build it must be fed, clothed and housed and all the money they earn must be spent in Johnson City and according to the estimate of Mr. Hargrave the combined earnings of the men will be in the neighborhood of \$20,000 a month. The furnaces are to be models in their way and will be modern in construction and make pig iron by the Bessemer process.

To give you an idea of what a 125-ton furnace is, it is only necessary to state that these will require four blowing engines with 42 inch steam cylinders and 84 inch blowing cylinders with 60 inch stroke. 32 boilers 54 inches in diameter and 34 feet long. Stack [stock?] houses 500 x 80 feet and cast houses 175 x 60. The highest smoke stack will be 160 feet and 8 feet in diameter. The furnaces are being built on the King farm recently purchased by the Carnegie Land Company, and will occupy twenty acres.

This is only the beginning of what is to be done and Johnson City is riding on the tidal wave to prosperity. With such men as Gen. Wilder to direct them is no danger of not reaching the goal in safety.”

20 February 1890. Johnson City Comet.

“Furnace Blasts. Last Monday dirt was broken on the first furnace to be built by the Carnegie Iron Company, and as soon as it gets well under way the second will be started. Mr. Harry Hargreaves, of Birmingham, is the contractor and is rustling things with that energy peculiar to himself. He has done the excavating for foundations about done. Eight side tracks are to be put down around the furnaces and a large force is now at work grading for them. Rock are now being placed for the foundation and masons are at work. The force is being increased as fast as circumstances will permit. Mr. Hargreaves says he will have the iron work ready in 60 days and by that time he expects to have 500 men at work. He has contracted for the erection of 68 houses for his men, who will camp in tents until they are built. Both furnaces will have a capacity of 125 tons of pig iron daily and that means a tonnage to a rail road company of 1,600 tons per day for 365 days in the year.”

20 February 1890. Johnson City Comet.

“Second Avenue leading to the furnace site was lined with vehicles all day, Monday, and contractor Hargraves was kept busy answering questions. It was a great sight for our people to see a furnace start.”

27 February 1890. Johnson City Comet.

“To Work on the Furnace. A number of Italians with all their worldly goods, arrived on No. 4, Sunday and were put to work Monday on the furnace. Others are expected daily, and in a few weeks a stranger coming to Johnson City will think he was in Italy.”

20 March 1890. Johnson City Comet.

“To Build a New Line. Col. T.E. Matson received a telegram yesterday from Gen. A. Pardee, of Philadelphia, directing him to make a survey at once from a point on the East Tennessee and Western North Carolina Railroad, near Wheelers, through Whiteside Gap, to the E.T.V. & G., and Three C’s junction. It is the intention of the Narrow Gauge people to build a line to the junction of the two roads mentioned, in order to save a great deal of expense in transferring and shifting cars, but principally to get a track through their ground on which they intend to erect two furnaces in the near future. Gen. Pardee arranged with Col. Matson while here the first of the month to make the survey and submit estimates of cost, &c. This line can be made cheaply and will be built at once.”

24 April 1890. Johnson City Comet.

“Started with a Rush. A list of Factories, Hotels, Business and Dwelling Houses Completed and Under Construction Since January 1st, 1890. – Nearly a Million dollars Already Spent in Building. – Carnegie Iron Co.’s Furnace. \$200,000...”

1 May 1890. Johnson City Comet.

“Several car loads of fire brick have arrived for the furnace.”

22 May 1890. Johnson City Comet.

“Iron for the Furnace. H.W. Hargreaves telegraphed from Pittsburg, Saturday to Gen. Wilder that a car of iron work for the furnace had been shipped and others would follow this week. When the iron work is commenced the furnace will begin to loom up over the surrounding hills. The stone work is about completed and the brick work has been started. Bad weather and waiting for material has delayed the work considerably but it will be pushed rapidly from now until it is completed.”

5 June 1890. Johnson City Comet.

“Iron for the Furnace. Five cars of iron were received yesterday for the Carnegie Iron Company’s furnace. Several were unloaded the first of the week and Superintendent Hargreaves says he will start the iron work Monday. Mr. Hargreaves is a hustler and is pushing the work rapidly. While other towns have been staking off foundations for a furnace, he has one half built.”

3 July 1890. Johnson City Comet.

“The Furnace. Superintendent Hargreaves is making a big show on the furnace now. The underground flues are finished, and the engine house is being rushed up. Rock masons are finishing the foundation for the boiler house, and four large boilers have arrived, and the other eight are made, ready to ship. They are 56 inches in diameter and 30 feet long, with double flues. Withroe & Darley, of Pittsburg, have the contract for the iron work and have a large portion of it now on the ground, and Mr. Hargreaves is expecting iron workers to arrive every day to begin the work. The first furnace in East Tennessee on the line of the East Tennessee, Virginia and Georgia road will be in operation before Christmas, 1890.”

28 August 1890. Johnson City Comet.

“The iron work at the furnace is well under way. One of the stoves is over fifty feet high, and the others are started. The columns for the furnace are in position, and the stack is being put up rapidly. Superintendent Hargreaves is a hustler and lets no ‘grass grow under his feet.’ By Christmas the furnace will be in blast.”

27 November 1890. Johnson City Comet.

“The Furnace. Superintendent H.W. Hargreaves is moving things about the furnace with his characteristic energy. The iron work is rapidly going up and some of the machinery is arriving. The large pumps are being placed and the lining of the furnace, stones and stack are about completed. The elevator is finished for carrying coke and ore up to the top of the furnace. It is believed that in 60 days, if the weather is favorable, the furnace will be ready for blast.”

8 January 1891. Johnson City Comet.

“The East Tennessee, Virginia and Georgia Railroad Company is preparing to build a sidetrack to the Carnegie Iron Company’s furnaces from a point on their line east of the furnace property. It will form a ‘Y’ with the Three C’s.”

22 January 1891. Johnson City Comet.

“Eyes of the World are Turned Toward Johnson City and Iron Masters are Anxiously Watching Developments that will Prove to be the Future Steel Producing Centre of the World. Ky. Trades Journal.) The result of three important enterprises that will show the value of Southern coal and iron ores will be made known early in the present year.

The first of these is the coke plant at Big Stone Gap...

The second of these tests is to be the iron plant at Johnson City, Tenn., and will be the first Southern furnace to produce Bessemer pig in any quantity. The ore used will be from the famous Cranberry mines in North Carolina, one of the largest deposits of non-phosphorus bearing, or Bessemer ore, in the country, and the coke from Big Stone Gap. The coke and the ore are separated only one hundred miles on an air line, giving advantages unequaled in any other part of the country. Tests made in the original Cranberry charcoal furnace for thirty days, using this ore and Pocahontas coke, resulted in the production of a grade of Bessemer pig-iron that sold at Johnson City for \$22, at a time when iron in the Birmingham district was selling at \$12 per ton, and was sold in Pittsburg at from \$2 to \$4 per ton above the price obtained for Bessemer pig made from Lake Superior ores.

The third test will be the basic steel plant being built at Middlesborough Ky...”

30 April 1891. The Iron Age.

“James P. Witherow, engineer and contractor, of Pittsburg... has the contract for the blast furnace plant of the Carnegie Iron Company at Johnson City, Tenn. It consists of one blast furnace 17 x 75 feet in size equipped with three Whitwell hot-blast stoves 18 x 60 feet in size. Work on this plant will be completed about May 15 next, at which date it is expected to go in blast...”³²⁹

4 June 1891. Johnson City Comet.

“The Carnegie Iron Company have a furnace nearly ready to go into blast...”

J. T. WILDER, Pres't. H. W. HARGREAVES, Sup't. J. W. CURE, Sec'y and Treas.

CARNEGIE IRON COMPANY,

JOHNSON CITY, TENN.,

MANUFACTURERS OF

Strictly Bessemer and High Grade Foundry Iron.

Will be ready to go in blast at an early date.

Figure 84. Johnson City Comet, 24 September 1891.

15 October 1891. Johnson City Comet.

“The Eastern Gem. Brief Outline of One of the Future Great Cities. – Johnson City, Where the Hammer and Saw are Making Music – Destined to be a Great Manufacturing City – The Thrift of the Inhabitants, Etc. – Special Correspondence Chattanooga Times... the Carnegie Company have built, and have almost ready for occupancy, one of the largest and best equipped furnace plants in the country. Its capacity will be 125 tons of iron per day. The casting house is a solidly built iron structure 150 feet in length, by ninety feet in width. The stack house is 102 feet by 250 feet. This furnace is being built under the immediate supervision of Mr. Hargeave, is his twenty-fifth furnace, and ‘the best I have ever built.’ An interesting feature of this building is that, its frame work is all cut from the trees which had to be cleared from the streets and roadways from the new town of Carnegie...”

21 January 1892. Johnson City Comet.

“Men are employed on the Carnegie iron furnace, getting the engines and boilers ready for operation.”

4 February 1892. Johnson City Comet.

“\$1,288,051. – These are the Startling figures will Tell of the Growth and Activity of the Plucky, Magnetic City – In 1891 – Complete List of Improvements during the Year... More than a million and a quarter of dollars have been expended in plants, business blocks, school houses, residences, street improvements, water works, railroads, etc... Carnegie Iron Furnace [\$]180,000”

11 February 1892. Johnson City Comet.

“The Electric Railway Co. will in all probability also extend the car line from Hotel Carnegie to the furnace.”

22 September 1892. Johnson City Comet.

“Johnson City would be in it, if only the Three C’s road and the Furnace would start up.”

3 November 1892. Johnson City Comet.

“The three new side tracks which are being put in by the East Tennessee road at this place from the main line to the Carnegie furnace, are nearing completion.”

3 November 1892. Johnson City Comet.

“The three new side tracks which are being put in by the East Tennessee road at this place from the main line to the Carnegie furnace, are nearing completion.”

1 December 1892. Johnson City Comet.

“When will the furnace go into blast, and when will work commence on the Three C’s? are questions asked every day.”

3 June 1893. American Economist.

“Carnegie Iron Company and Carnegie Land Company of Johnson City, Tenn. Own completed blast furnace at Johnson City. Assignment made June 3, 1893.”³³⁰

1893. “Carnegie (Cranberry) Furnace”, by John R. Waite.

“The uncertain economic times of the 1890s were not kind to Wilder’s financial fortunes. Several sections of the 3-Cs railroad were in operation and hopes were high that the entire line would soon be completed when the Financial Panic of 1893 brought construction to a halt. The failure of Baring Brothers, the British financiers, brought on the demise of the 3-Cs. Baring Brothers lost millions, Wilder lost \$760,000 and Johnson City was left with \$70,000 in unpaid bonds.

Construction of the furnace also stopped when Wilder’s Carnegie Furnace Company went bankrupt...”³³¹

8 June 1893. Johnson City Comet.

“Two Big Assignments. – The Carnegie Land and Carnegie Iron Company – Make a Temporary Assignment on Account of the Inability to Collect Debts or Float Bonds. – ‘The Carnegie Land Company and the Carnegie Iron Company have made an assignment!’

That announcement created more excitement in Johnson City yesterday than anything that has happened for years... Gen. Wilder, the president and resident manager of the companies, has worked hard and faithfully for the up-building of this city and section, and has done all in his power to avoid making the assignment, but lack of financial support from interested parties has rendered it absolutely necessary to protect the property and the creditors...

The following statement was telegraphed to several papers by The Comet Sunday:

‘The Carnegie Land Company and the Carnegie Iron Company, on account of the financial situation and inability to make collections, and lack of assistance from non-resident stockholders, have been forced into a temporary assignment to protect the property and their creditors. The financial pressure has been so great that the companies could not be sold or the amount due them could not be collected.

Gen. Wilder who owns one-third of the stock of both companies, has stood manfully by them and backed them up to the extent of \$55,000, while all other stockholders, holding two-thirds of the stock, have only furnished about \$18,000. Arrangements had been made to sell the bonds on the furnace in New York, but the recent stringency of the money market was so great that the party could not spare the means to take up the bonds at present. They, however, assure the companies that they will take the bonds later and pay off all the indebtedness.

The property consists of a large, new blast furnace, hotel, lands and town lots, and secured notes of \$600,000, while the total indebtedness does not exceed \$125,000, much of which is not due. There is not of this amount more than \$25,000 due to people in the South. The rest is owed to parties North and East.

The delay in completing the Three C’s railroad was the prime cause of all the trouble, and no doubt the whole thing will be straightened out and everybody paid within this year. J.W. Cure has been made assignee, and is thoroughly familiar with all the affairs of the companies...”³³²

15 February 1894. Johnson City Comet.

“About the Three C’s. – Gen. Wilder Talks Regarding the Road and the Carnegie Furnace. Gen. J.T. Wilder spent Friday in Bristol... The matter of putting the Carnegie iron furnace in blast is being considered and Gen. Wilder thinks the work may be accomplished during the coming spring. It will require but little more money to complete the furnace. It is nearly as large as the Bristol furnace, and Gen. Wilder is of the opinion that its capacity is as great, owing to the differences in engines, boilers, etc.

Gen. Wilder visited the Bristol furnace yesterday. He pronounces it a first class furnace. – Courier.”

15 March 1894. Johnson City Comet.

“We don’t know whether it is with a view of getting things in order to begin running the Carnegie furnace at some near day or not, but we are told that a number of hands commenced work on it Monday to get it in shape for some purpose. It has stood there idle long enough to get in a moss-covered state, and it may be that these hands were hired to pull the weeds out of the moss, so as to give it full swing in its growth.”

24 May 1894. Johnson City Comet.

“Will it Start. – A Movement on Foot to Blow in the Carnegie Furnace. There is a strong probability of the Carnegie furnace going into blast in a short time.

The Philadelphia company who have a claim against it in Federal court, have sent an agent down here to look over the grounds, and if his report is at all favorable, and it can hardly be otherwise, we look for operations to begin at once.

If the result of the agent’s investigation is satisfactory the Philadelphia company will allow the court to permit the original company here to issue receiver’s certificates to complete the furnace and put it into blast.

This is the matter which has been hanging for so long, but the claimants have decided at last that there is a better chance to get their claim out of a furnace in blast than one idle.

A responsible company has been found which will sign a contract to take the entire output of the furnace. We may confidently hope to see work begin in a few weeks, and new life infused into our city.”

20 December 1894. Johnson City Comet.

“Carnegie Iron Furnace. – Its starting will mean much for Johnson City. John W. Cure, secretary of the Carnegie Iron Furnace Company, was in Knoxville yesterday on his return from Louisville, where he had been in the interest of the furnace. He said that the company anticipated no difficulty in placing the receiver’s certificates which Judge Key ordered issued a few weeks ago. He says the \$40,000 thus authorized will be ample to pay off the indebtedness, complete the furnace and put it in blast. The company intends to begin work in January and have the furnace in operation in sixty or ninety days. – Knoxville Tribune.”

20 June 1895. Johnson City Comet.

“Furnaces to Start – Plants at Embreville and Carnegie Soon to go into Blast... A correspondent of the Baltimore Manufacturers’ Record, writing from Bristol, says... The Carnegie Furnace Company’s plant at Johnson City is also expected to start up at an early date, and preparations are now in progress. This plant, like the Embreville furnace, is good for 150 to 200 tons. It is expected that both furnaces will be supplied with coke from the Big Stone Gap plants that are certain to come...”

18 September 1895. Johnson City Comet.

“The indications now are that the Carnegie Iron Co.’s plant in this city will be sold under decree of the United States court in a short time. The receiver’s certificates formerly issues by order of the court, Judge Key president, have been recalled and all creditors were notified to file their claims by the first Monday in this month. If this course had been pursued long ago, the furnace would now, in all probability, be in operation.”

17 October 1895. The Iron Age. Also in Johnson City Comet, 10 October 1895.

“Notice of Sale of Carnegie Iron Company’s Property. In obedience to a decree of sale made and entered in the United States Circuit Court at Knoxville, Tennessee, in the cause of Jas. P. Witherow Co. et als. vs. Carnegie Iron Co. et als., and A.R. Wing, Assignee, &c., vs. Carnegie Iron Co. et als., No. 961, on the 5th day of October, 1895, I, as special commissioner, will on Friday, the 22d day of November, 1895, at 12 o’clock noon, upon the premises at or near Johnson City, Washington County, Tennessee, sell at public outcry to the highest and best bidder and in bar of the equity of redemption, the real estate and furnace of the Carnegie iron Co. lying in Johnson City, Washington County, Tennessee, in the Carnegie Land and Improvement Co.’s, alias the Carnegie Land Co.’s addition to Johnson City, Tennessee, said Companies being the same, and being described by meets and bounds as follows:

1st Tract. – Beginning on the North side of the East Tennessee, Virginia and Georgia Railroad, at the point where the line of right of way of said railroad intersects the East side of Iron street as shown by the map of the Carnegie Land and Improvement Co.’s addition to Johnson City, Tennessee, said line of right of way being parallel with and 30 feet from the center of said railroad

track, thence with the East side of Iron street North 49 degrees 55 minutes, West 366 feet to the point where it intersects the line of right of way of the Charleston, Cincinnati and Chicago Railroad and 20 feet from the center of said railroad track, thence Northeast with said line of right of way and following its courses 1325 feet to the point where it intersects the Albert King property line, thence with said property line North 33 East, 817 feet to a set stone, thence North 25 degrees 55 minutes East, 363 feet to a stone, thence South 40 degrees 13 minutes East, 235 feet with said King property line to the point where it intersects with said line of right of way of the East Tennessee, Virginia and Georgia Railroad, thence with said line of right of way South 33 degrees 11 minutes West, 1418 feet to the beginning, containing 18 and 28-100 acres.

2d Tract. – Beginning at the point where the East side of Iron street intersects the line of right of way of the Charleston, Cincinnati and Chicago Railroad on its West side, said line of right of way being parallel with and 80 feet from the centre of said railroad track, thence with the East side of Iron street North 49 degrees 55 minutes West, 340 feet to the South side of the alley between First Avenue and Second Avenue, thence with the South side of said alley North 40 degrees 5 minutes East, 300 feet to a stake, thence North 49 degrees 55 minutes West, 160 feet to a stake on the South side of Second Avenue; thence with the South side of Second Avenue, North 40 degrees 5 minutes East, 310 feet to the point where it intersects with said line of right of way of the Charleston, Cincinnati and Chicago Railroad, thence with the line of right of way and following its curves 81 feet to the beginning, containing 2 and 64-100 acres ‘including the iron furnace and all improvements and appurtenances erected and being upon said property.’

By said decree the minimum or upset price upon said property is fixed at sixty thousand (60,000) dollars and no lower bid than said minimum or upset price will be received. Said property will be sold on the following terms: One third of the purchase price is required to be paid in cash and for the remainder of the purchase money notes and security will be required from the purchaser in equal installments in nine and eighteen months, bearing interest from the date of sale, which security may be either personal security or deposit of collateral satisfactory to the undersigned, and liens will also be retained in the faces of said notes as further security.

I, as such special commissioner, will at same time and place and under same decree and on same terms sell separately all personal property of the Carnegie Iron Co. lying on or near to said real property and consisting of wood, brick, etc. H.H. Taylor, Special Commissioner.”³³³

21 November 1895. Johnson City Comet.

“Sale of Furnace. The advertised and long-talked of sale of the Carnegie furnace takes place to-morrow. Let it come. The sooner the better. Will Hunter will buy it, and when he does there will be no more loungers around town. They will all find ready employment at living wages. Meanwhile bring on the sale.”

28 November 1895. Johnson City Comet.

“Furnace Sold. Bought by New York Capitalists at the Upset Price. Pursuant to a decree of the federal court at its last term at Knoxville in the case of J.P. Witherow & Co. et al vs. the Carnegie Iron Co., the plant of the latter at this place, comprising several acres of land and an almost completed 100-ton iron furnace and appurtenances, was sold by Special Commissioner H.H. Taylor last Friday, the 22d inst. Wilberforce Sully, a son of Alfred Sully, of New York, was the only bidder, and, having bid the upset price, \$60,000, the plant was ‘knocked off’ to him. He complied with the terms of sale, which required \$20,000 to be paid in cash at the time of the sale.

Just what Mr. Sully means to do with the plant, now that he is the absolute owner, is an open question.

The senior Sully has been a large stockholder in the company since its organization and, for some reason, has not cared to complete the furnace and put it in blast. It is to be hoped, however, that he will do so now, and there are indications that he will. We may confidently expect some development after the sale of the Carnegie Land Company's property next Monday."

28 November 1895. The Iron Age.

"The Carnegie blast furnace, built at Johnson city, Tenn., five years ago, was sold at receivers' sale recently for \$20,000. Wilberforce Sully, son of Alfred Sully, one of the leading stockholders, was the only bidder."³³⁴

6 January 1896. Johnson City Comet.

"The Furnace. There have been rumors afloat for so long a time in regard to the Carnegie furnace going into blast that the subject is simply tiresome and has become nauseating – sickening in the extreme; but it is an assured fact that on the 30th of the following month smoke will be seen curling from its towering chimney and not less than two hundred hands put work. Wait and see."

23 January 1896. Johnson City Comet.

"Notice. I hereby give notice that I will receive bids until February 1, 1896, for painting the Carnegie furnace, one coat, 'Reeder's American Cold Japan' to be used.

For further information and particulars call on or address J.W. Cure, Jan. 22, 1896. Johnson City, Tenn."

"A product called Reeder's American Cold Japan is being quite generally handled by Hardware dealers as it is said to make an excellent coating for galvanized iron and one of the best roof protectors ever discovered. It is easily applied with a brush, like ordinary paint and on metal roofs, sidings, bridges, iron fences, etc., is recommended by those who have used it from ten to thirteen years in the highest terms..."³³⁵

Regrettably, the author has not found any description of the color or colors available with this product.

12 March 1896. Johnson City Comet.

"The contract for painting the furnace has been let to C.L. Taggart. This is the first work done by the new owners, but it is hoped that it will not be the last."

10 March 1898. Johnson City Comet.

"Carnegie Furnace. It has been noised around considerably lately that the Carnegie furnace was about to be leased to northern capitalists. How much truth there is in this report we are unable to tell, but this we do know, that it is a pity to allow such valuable property to go to ruin."

14 April 1898. Johnson City Comet.

“\$25.00 Reward. Anyone giving sufficient information to lead to the arrest and conviction of parties cutting wire fence northeast of Carnegie furnace will receive a reward of \$25.00. Frame King.”

May 7, 1898: One hundred and twenty-five years ago today, The Knoxville Sentinel reported that “General John T. Wilder states that a sale of the Carnegie blast furnace – at Carnegie, two miles north of Johnson City, has been consummated with Geo. Abington Parker – of England. The price of the sale is not given. It is understood that the furnace may be removed to Cranberry.”

“The furnace was built in 1891 at a cost of nearly \$200,000 and is of 150 tons capacity. It was never put in blast and has been taken care of all of these years. It is one of the best constructed furnaces in the south.”³³⁶

12 May 1898. Johnson City Comet.

“The Furnace Sold to English Capitalists – Will be Completed and Put in Blast at Once – Will Make Bessemer Pig. The old saw, ‘It never rains but it pours,’ is being verified in Johnson City now in an industrial sense.

Just as numerous new woodworking industries are getting under way comes the pleasant intelligence that the Carnegie Iron Company has sold the furnace to an English syndicate, the same that owns the Embreville furnace and mines. Mr. George Parker, represented the purchasers and closed the deal with Mr. Wilberforce Sully in New York last week. Mr. Guy R. Johnson, manager of the Embreville property, and Gen. J.T. Wilder, for Mr. Sully, closed the transaction locally a few days ago, too late for publication in The Comet last week

It is understood that the furnace will be completed at once and go into blast at the earliest possible moment – making Bessemer pig iron from the famous Cranberry magnetic ore, the only steelmaking ore in the South.

The fact that the furnace is now to be completed and in blast, will be good news to Comet readers and all interested in Johnson City, financially or otherwise.”

26 May 1898. Johnson City Comet.

“The Furnace. There is perhaps no longer any doubt that our furnace will be put in blast here. The purchasers made some requests of the board of Mayor and Aldermen and the Board of Trade and were so well pleased with the response, that they have about decided to leave the furnace in Johnson City. Mr. Guy R. Johnson, resident manager, has cabled Mr. [George Abington] Parker the result of his overtures and only awaits his reply to commence work on the plant. There are a few more details to arrange and then a force will be put to work completing the furnace. This will require several months time and the expenditure of a great deal of money. In the meantime rock quarries will be opened and large quantities of rock, Cranberry ore and coke dumped upon the ground to be ready as soon as the furnace is completed to put it in blast.

There is every indication now that a small army of mechanics will be at work around the furnace in 15 days building dwellings for workman, the commissary, offices, etc. It will be Johnson City’s army of invasion and no one will oppose the landing.”

9 June 1898. Johnson City Comet.

“Hon. A.A. Taylor says smoke will be curling out of the furnace stack in less than two weeks. We hope this prediction is based upon facts.”

July 22, 1898: One hundred and twenty-five years ago today, The Chattanooga Daily Times reported news with a dateline from Johnson City and a date of July 21. Readers learned that

“Arrangements are being made at Cranberry, N.C., for getting out a large quantity of iron ore to supply the Carnegie blast furnace at this place when it goes into operation. It is expected that one hundred and fifty tons will be shipped from there daily down the narrow gauge railroad.”

“All the cars of the Ohio River and Charleston road standing on the premises and in the sheds of the Carnegie furnace have been ordered removed in order that the grounds may be improved preparatory to starting the furnace.”

“As a consequence of the announcement that the furnace would go into blast, the demand for resident houses is more than can be supplied. A real estate agent said yesterday that not only cottages were wanted, but good substantial two-story houses were in demand. A part of this demand is due, however, to the fact that all the woodworking factories that have been idle for a year or two are now being operated at full capacity. Of these there are half a dozen, and their combined pay roll amounts to a considerable item in the volume of trade.”³³⁷

1 September 1898. Johnson City Comet.

“The Furnace. There is every indication that work will begin in earnest upon the furnace in a short time. The owners have notified parties using the adjacent land to burn lime to vacate, and there is an air of activity about the region that is encouraging after all these months of quiet. It will require several weeks to get the furnace ready to go into blast and while that is going on a supply of ore, coke and limestone will be put on the ground.”

8 September 1898. Johnson City Comet.

“Shipments of Ore Commenced. The Cranberry Iron Company has commenced shipping ore to the Embreville furnace people. Several car loads will be shipped to Embreville to test in the furnace in that place before the furnace here is put in blast. Ore is being transferred here daily for shipment to Embreville. If the test is satisfactory, and there is no doubt that it will be, regular daily shipments will be made to the furnace at this place.”

22 September 1898. Johnson City Comet.

“Getting Ready. It is evident that the Embreville Furnace Company is getting ready to start the furnace at this place. While work has not actually begun to complete the structure, other deals have been consummated that tend to show how the wind blows. During the last week the Company bought the half interest of G.C. Harris in a number of houses known as ‘Red Row’ near the furnace. These houses were built several years ago by Harris & Hargreaves to be used by furnace employes and are suitable for no other purpose, on account of their location. The indications are that work will be commenced in earnest upon the plant in a few weeks.”

27 October 1898. Johnson City Comet.

“Getting Ready. The Embreeville Furnace people are rapidly getting ready to put the Johnson City furnace in blast. They have leased magnetic ore beds near Roan Mountain station and will build a narrow gauge road to the mines as soon as they begin to develop the property. They have also bought for immediate use 200,000 tons of ore from the Cranberry Coal and Iron

Co. and are shipping it to Embreeville daily. They will begin to ship to the Johnson City plant as soon as certain arrangements are completed.”

3 November 1898. Johnson City Comet.

“At Work on the Furnace. There is no longer any doubt about the Johnson City furnace going into blast, and that too, at an early date. J.W. Cure who has been in the pension office at Knoxville for some time, has accepted a position with the furnace company and entered upon the discharge of his duties Tuesday morning, Nov. 1st. This should be conclusive evidence to the most skeptical, but aside from this, Mr. Guy R. Johnson, resident manager of the London owners, has been to Knoxville within the last week to hurry the Southern Railway about building sidetracks to the furnace. A contract has been closed for 200,000 tons of Cranberry ore and it will be put in as soon as the siding is completed. The company has also leased magnetic ore beds near Roan Mountain station and will develop them at once, building a narrow gauge road to the mines. In the meantime, the work of completing the furnace and buildings has been started and a large force of hands are now at work at the furnace. The work in all departments will be hurried to completion as rapidly as consistent.

Mr. Guy R. Johnson has leased the double dwelling of Mr. C. Elmondorf on Second Avenue near New street, for a residence for some of the furnace people.”

17 November 1898. Johnson City Comet.

“Will build a Storehouse. The Embreeville Furnace company will begin at once the erection of a storehouse 30 x 60 feet near the furnace. The building will be brick veneered and will be completed by the middle of December. It will then be used as the company store under the management of R.L. Bruner...

Work at the Furnace. There is an atmosphere of activity about the furnace that is very invigorating to one who has not enjoyed such a breeze since the days to the ‘boom’ in the happy long ago. It will do you good to go to the furnace and see the 100 men hard at work in the different branches, but all doing necessary work to get the furnace ready to go into blast at the earliest possible moment. A large force of men are at work on the sidings to be used around the furnace.

All masons that can work to advantage are lining the stoves with firebricks; it requires more than 200,000 brick to do this work and it will take 60 days to perform the labor, owing to the limited room in which to work.

Other laborers are at work on a large reservoir for holding waste water and repumping therefrom for use around the furnace. Others are also at work cleaning up the premises and doing other necessary work.

Several tenant houses will be built at once and everything put in readiness to put the furnace in blast early in 1899.”

24 November 1898. Johnson City Comet.

“Around the Furnace. If there is one thing that gives The Comet more pleasure to announce, than another, it is the fact that the furnace is being completed as fast as unlimited capital can command labor, and will be operated permanently. This is a particularly sweet morsel to us, for we have talked furnace until we scarcely believed our own stories. We would like to have seen the plant completed and operated under General Wilder’s management, but it could not be thus, and we are glad to have it fall into the hands of the Embreeville Iron Company Limited, and be managed by their popular manager, Mr. Guy R. Johnson...”

Down at the plant, there is life! Machinists are cleaning up and oiling the ponderous engines and pumps, the hoisting machinery and all other machinery about the place is being put in good condition.

Masons are relining the stoves and furnace with fire-brick and tearing out the arches beneath the ground and putting them in with fire-brick.

The reservoir is about completed, and the big cistern near the pump house is finished. Water for the reservoir will be brought from Brush Creek in a 12 inch main. The pipe line will be laid from the creek where it is crossed by the O.R. & C. near the tank. The water will be allowed to settle in the reservoir and then be strained into the cistern, settle again and then be pumped, into the boilers.

The contract for the store building has been let to M.H. Campbell and is to be ready for occupancy in 30 days. It will be 30 x 70 feet and will be built on 2nd avenue near the office.

The foundation for an addition to the dwelling formerly occupied by H.W. Hargreave has been completed and the woodwork is under way.

The furnace property, 51 acres, will be fenced with barbed wire and a high, close board fence will be put around the furnace proper. The lines for this fence will be run and staked off.

The company has a force of laborers at work building sidings around the plant for convenience in handling material. One track is being graded back of the blast house and around the stock house. Two others will be put in back of the boiler house through the ore yard.

Engine No. 2 came in from Embreville Monday for use at this furnace. It is kept busy on the yard now, and is in charge of our old townsman, Joe D. Collette.

The Southern Railway has put in three long sidings to the furnace property intersecting the main line at the crossing just below the Carnegie yards. This road will also lay a third rail on the main line from a connection with the Narrow Gauge to the furnace for the purpose of handling the ore cars from the Narrow Gauge. This line to avoid tearing out the O.R. & C. crossing will pass to the right of Biddle & Ellsworth's mill and go through to Carnegie yards on the side-track. Work on the spur track has been temporarily suspended because the O.R. & C. objects to being crossed at the point desired.

The Embreville Company received a Narrow Gauge engine Monday for use on the line of road it is building from Roan Mountain to the mines they are developing. The engine was sent to Embreville to be overhauled in their shops.

Several convenient cottages will be erected at once on 2nd avenue between the office and the Hargreaves residence. These cottages will be occupied by the foremen of the different departments. The Elmendorf house near New street will be used as a boarding house for furnace hands and will be run by J.T. Ferguson, of Washington College."

1 December 1898. Johnson City Comet.

"Furnace Notes. – Cold weather may come and cold weather may go, but the work at the furnace goes on forever. There is no stopping any department now for night or Sunday. The Watauga Lighting and power co. wired the plant throughout this week and now numerous arc lights are turning night into day and a night force has been put to work. Sunday never comes to the day shift, every hour of night and day is being used to complete the plant and get it in blast at the earliest possible moment.

The out-door work, track-building, etc., under Civil Engineer Dyer, is also being pushed. The grading for the sidings is about completed and the ditch for the water main to the reservoir is under way.

A private telephone wire is being put in from the furnace here to the Embreville office.

The limestone rock used in fluxing will be brought from the quarries at Watauga. Lee Smith will superintend the work at the quarry.

A third rail will be laid on the Southern to Embreville to allow the Narrow Gauge ore cars to be taken direct to the furnace and avoid the expense of transfer here.

There is a strong probability that the Southern will double track the main line from Embreville Junction to the Carnegie furnace. The business created by the furnace here in operation will keep the main line so busy that regular trains will be delayed getting between the points in question.

To give you some idea of how much material is being put into this plant it is only necessary to state that Manager Johnson paid the Southern a freight bill Wednesday alone over \$1,580.00 and the shipment of ore has not commenced.”

8 December 1898. Johnson City Comet.

“Carnegie Furnace Notes. – The furnace at Johnson City will be operated by the Carnegie Furnace Company. The same people who compose the Embreville Iron Company, own the furnace, but it will be run as a separate institution and made to survive or perish upon its own merits and advantages.

The work around the plant is somewhat retarded by the cold weather, but continues in every department.

The race from Brush creek to the reservoir is nearly completed and the water can be turned into the reservoir in a few days. This reservoir is a large affair, and will hold several million gallons, and will supply all the water used at the furnace for all purposes.

A large gang of bridge carpenters are now building, or rather, rebuilding the two trestles into the stock house.

The addition to the dwelling to be used by Supt. Maury is well under way and the house will be ready for occupancy by the 1st of January. This house will be heated by steam from the furnace. The pipes are now being laid. The office and commissary will also be heated by steam from the same pipe line.

A large building has been completed near the stock house. It will be used as carpenter shop, machine shop and blacksmith shop. The engine to run the machinery has already been set and several new pieces of machinery have been placed in the machine shop.

The laboratory is being built near the engine house and is nearing completion.

The foundation for the commissary is being laid as rapidly as the weather will permit.

The sidings are being pushed. One track has been laid around the plant back of the cast house and will be used as a loading track. An embankment is being built on one side as high as a car door to facilitate the handling of stock. Two other sidings are being put in back of the boiler house.

Supt. Maury told The Comet they would probably have to bring the limestone rock necessary for fluxing, from Embreville, as none could be found nearer than met all requirements from a scientific standpoint.”

15 December 1898. Johnson City Comet.

“The Carnegie Furnace – Will Go into Blast Before 1898 is Numbered with the Past. Unless there is some unforeseen difficulty the Carnegie furnace will go into blast this month. The furnace and stoves have been relined and a fire was put in the furnace Tuesday to dry it out. The boilers have been filled and steamed and the big engines and pumps given a trial. Supt. Maury told The

Comet they were now ready to go into blast but for drying the new brick work. He expects to blow in either the latter part of next or early in the following week, certainly before January...

Several tons of steel rails were shipped to Roan Mountain this week for use on the railroad the Embreville Iron Company is building to the mines. Tracklaying will begin next week.

The Shipment of ore to the Carnegie Furnace has commenced. Several car loads are daily arriving over the Narrow Gauge from Cranberry and being transferred here and forwarded to the furnace. The transfer will be avoided as soon as the hitch in the third rail proceedings over the Southern is adjusted."

22 December 1898. Johnson City Comet.

"The Furnace Work. So far as the furnace proper is concerned it is ready to go in blast. All work was completed last week and a large force of hands were laid off. While the furnace is being dried out ore is being accumulated on the yard and it is expected that all will be ready to make iron on January 1st, 1899.

The limestone will be brought from Watauga on trial. R. Horneck will have charge of the quarry and it now arranging to begin shipment.

The work on the sidings and other outdoor work at the furnace is progressing rapidly. The fence around the property is being put up.

The commissary is nearing completion. The plasterers are at work lathing the building. It will be occupied about January 1st. The commissary at Roan Mountain is completed and is a splendid store building 40 x 60 feet, two stories high.

Mr. R.L. Bruner, who will have charge of the two stores will leave next Tuesday for the East to purchase the stock for both places.

The third rail on the main line of the Southern from a connection with the Narrow Gauge to the Carnegie furnace will be laid next week. The difficulty of crossing the O.R. & C. has been overcome in a mechanical way rather than by agreement. An agent of the Pennsylvania Steel Co. was here a few days ago and made a map of the crossing and the company has now about completed a new crossing that... admit the third rail and yet... transfer of cars from other roads at this crossing as at present. It is an intricate crossing as well as an expensive affair." Portions of this image are illegible.

23 December 1898. American Manufacturer and Iron World.

"Bessemer Ores of the Cranberry and Adjacent Districts. – An item published in this paper some weeks ago to the effect that a Southern iron company would soon engage in the production of a special low phosphorus iron, has created a good deal of interest among those engaged in this line of business. It is understood that the Embreville Iron Company people are behind the project, and that the furnace of the Carnegie Iron Company, at Johnson City, Tenn., will be used. This furnace was never put in blast, but it is said that this will now be done, and that it will be operated on low phosphorus iron, and that the maximum phosphorus is to be put at 0.03..."³³⁸

29 December 1898. Johnson City Comet.

"Furnace Notes. Tomorrow, will be 'Good Friday' for Johnson City. On this day the Carnegie Furnace Company will begin to fill the furnace preparatory to making the first 'run,' and it will probably be made Saturday or Sunday.

The third rail on the Southern is being distributed along the road from the city to the furnace and will be put down this week.

R. Horneck, of Big Stone Gap, was in the city yesterday. He has the contract for quarrying stone for the Carnegie Furnace. He is operating the quarry at Watauga and has 300 or 400 car loads ready for shipment. Will begin shipping regularly this week.”

5 January 1899. Johnson City Comet.

“Making Iron. – Carnegie Furnace Blown in Tuesday Evening with Imposing Ceremonies – Johnson City’s fiery furnace is now in operation. Last Tuesday evening at 6:20 o’clock, Major Huger lighted the match that kindled the flames to melt the iron in the furnace that Wilder built.

The ceremonies that put in operation the biggest plant in Johnson City’s history were witnessed by a large number of our citizens as well as the officers of the company. Among those invited were Col. W.P. Harris and Mrs. Harris; Geo. T. Wofford and Mrs. Wofford; B.J. Sitton, G.W. Sitton and The Comet. General Manager, Guy R. Johnson, Superintendent M.H. Maury, John W. Cure, and Dr. W.J. Matthews represented the company. Major Huger, of Knoxville, had been selected to light the fire, and after a few happy and appropriate remarks in his most graceful manner, did so in the most approved fashion. It was an impressive scene:

The Major stood within the merry band,
With a lighted fire brand in his hand.
When all was ready and the word was given.
He started the flames and smoke to’rd heaven.

The fires were lighted through the furnace breast which was then filled with fire-brick and closed so as to leave only a small outlet for the flow of iron. The party lingered around the big firepot until the blast was turned on and then, being satisfied that they had witnessed the turning point in Johnson City’s history, the beginning of a new era, as it were, a select number repaired to the cozy residence of Superintendent Maury and discussed at length a most delicious menu, a particularly pleasant close to an evening fraught with great possibilities for Johnson City’s future.

The machinery is working perfectly and regular casts are being made every six hours...

Watauga... R. Horneck has about 20 men employed in the stone quarry preparing stone for flux for the furnace and the blasting with dynamite makes considerable noise... The quarry at this place is expected to furnish six cars of stone per day for the Carnegie Furnace.”

12 January 1899. Johnson City Comet.

“E. Harnick is looking for new fields of limestone to conquer. The output from the Watauga quarry will not meet the demand at the Carnegie furnace. A quarry will probably be opened on the Narrow Gauge road.”

19 January 1899. Johnson City Comet.

“The Third Rail Down. The third rail has been laid on the main line of the Southern from a connection with the E.T. & W.N.C.R.R., to the Carnegie furnace and narrow gauge cars of ore are now being sent to the furnace direct to save the transfer in the yard up town.”

1899. Along the ET&WNC, Volume IV: Freight Cars Part B.

“The furnace complex was located along the tracks of the Southern Railway, some distance away from the ET&WNC. When the Carnegie Furnace was originally ‘blown in’ during 1899, the

Southern laid a third rail for direct narrow gauge deliveries. When the furnace shut down, this rail was removed...³³⁹

2 March 1899. Johnson City Comet.

“A Few Furnace Facts. C.P, Perin, of Middlesboro, Ky., has succeeded Guy R. Johnson as general manager of the Embreville Iron Company. He took charge the first of the month.

F.E. Thompson, superintendent of the Carnegie Furnace Company has tendered his resignation to take effect March 15. His successor has not been named. It is likely the office will be abolished and the work done by J.W. Cure in connection with his other duties.

The shipment of iron ore from Cranberry mines to this furnace has been suspended. No reason has been assigned. Col. C.H. Nimson, who is now in Philadelphia, wired the superintendent at Cranberry to stop shipping ore to Carnegie furnace from that date, February 28, There is enough ore on hand to keep the furnace in blast until about the 20th inst.”

9 March 1899. Johnson City Comet.

“The Furnace. There have been all sorts of wild rumors floating around about the Carnegie Furnace, all tending to create the impression that the furnace was about to close down. Nothing could be farther from the truth. On account of a slight misunderstanding last week between the furnace company and the Cranberry Iron and Coal company, the shipment of iron was stopped for two days, but the difficulties have been adjusted and the ore is being shipped regularly...

The changes in the management will make no difference in the operation of the furnace, unless for the better. The price of iron is continually advancing and they can sell all they manufacture at a nice profit.”

16 March 1899. Johnson City Comet.

“The Furnace. Last Friday it was found necessary to bank the Carnegie Furnace on account of the lack of ore. Orders were sent to the company’s mines at Roan Mountain and to the Cranberry company to double the daily shipment, and a sufficient stock of material has been accumulated to warrant the furnace being started again and the blast was turned on today. Since the furnace started it has only been making 45 to 50 tons of iron a day, but after Mr. Perin, the new manager took charge, it was only a few days until 110 tons of iron were made daily. This increased demand soon reduced the stock of ore on hand and a wait was made necessary.

The new management will make several changes, it being the intention of Mr. Perin to operate this furnace and the one at Embreville upon the most economical basis consistent with good business judgment. The gate watchmen have been relieved of duty and every useless man will be cut off and the salary paid him, put upon the credit side of the expense account.

It is possible that a crusher will be put in the stock house and all ore and limestone crushed before put into the furnace.

The report that the commissary would be abandoned is utterly unfounded.”

25 May 1899. Johnson City Comet.

“The Furnace Deal. The Johnson City and Embreville furnaces have been sold to the Carter Syndicate, but here will be no change in the management of either of the properties at present. Mr. Carter and several officers of the Company were in Johnson City Wednesday on a tour of

inspection and went out to Embreville in the afternoon. They are not yet ready to announce their plans for the future...

Mortgage for \$10,000,000 to Secure Improvement Bonds of New Syndicate. – Johnson City and Embreville Furnaces in Deal. The Virginia Iron, Coal and Coke company, of which George L. Carter, of Bristol, Tenn., is president, has filed for registration in the register's office of Johnson county a mortgage covering all its property in Southwest Virginia, East Tennessee and Kentucky. The Manhattan Trust company, of New York, is made trustee, the amount secured therein being the large sum of \$10,000,000...

It is learned here that the mortgages referred to will also be filed at once in all other counties in which any properties of the above two companies may be located, and that it is made to secure improvement bonds of the above companies. The consolidation of all the iron and steel industries in Eastern Kentucky, Southwest Virginia and East Tennessee under one management is practically assured by the acquisition by the above mentioned syndicate of the two fine furnaces at Embreville and Johnson City, the deal which is reported as having been closed in New York. As published a few days ago, the same syndicate has also recently secured control of the Watts Steel and Iron syndicate, limited, of Middleboro, Ky., which completes the consolidation upon which the president of the new company, George L. Carter, formerly of Pulaski, Va., has been working for the past twelve months. It is understood that the combined capitalization of the iron and coal company and of the Virginia and Southern railway, which is controlled by the same interest, is \$15,000,000.

The properties, beside the above which are now included in the consolidation, are, as stated some time ago, the Carter Coal and Iron company, of Pulaski, Va.; the Graham Iron Works, Graham, Va.; the Crozer Iron company, Roanoke, Va.; the Consolidated Coal and Iron company, Max Meadows, Va.; the Camden Iron Works, Salem, Va., the Home Iron company, Bristol, Tenn...³⁴⁰

6 July 1899. Johnson City Comet.

“Furnace Banked. The Carnegie furnace was banked last week in order to make some necessary repairs. It was thought that it would be in blast again in a week, but they have been delayed waiting for a new bell. As soon as it arrived it will be put in place and the furnace again blown in.”

20 July 1899. Johnson City Comet.

“The Carnegie Furnace was blown in last Saturday after an idleness of a few days during which a new bell was put in the cupalo.”

21 September 1899. Johnson City Comet.

“The street committee was authorized to enter into a contract with the furnace people for slag for the purpose of Macadamizing the streets and other purposes. The furnace people have agreed to give all slag needed for street purposes provided the city will macadamize Second avenue from the furnace to the city, and this proposition will be accepted.”

29 March 1900. Johnson City Comet.

“Killed at the Furnace. Matthew Hardin, about 23 years old, as estimable colored man and well liked by both black and white, met a most untimely death at the Carnegie furnace Monday

morning. When at a height of about 80 feet, and while in the act of pushing a loaded ‘buggy’ from the platform to the elevator, the rope which held the elevator broke, and the platform, with its burden of human life and ore, went crashing through space to the earth below. Death was instantaneous, and the unfortunate man’s body mashed almost to a pulp.”

7 June 1900. Johnson City Comet.

“Falling Prices. Bristol, Tenn., June 4. – C. Shields, vice-president of the Virginia Iron, Coal and Coke company, has informed the Times correspondent that the action of his company in shutting down a furnace and other enterprises is due to exorbitant freight rates and the decline in the price of iron... The furnaces here, at Johnson City and Embreeville will continue in blast. (The furnace at Johnson City was closed down two weeks ago to make some necessary repairs, but during the last week this repair work was stopped and all men laid off. Ed. Comet.)”

14 June 1900. Johnson City Comet.

“The ore mines at Cranberry, N.C., have not been shut down, as reported. On account of the closing of the furnace at Johnson City that was running on Cranberry ore, several hands have been thrown out of employment. Only enough ore is now being mined to supply the furnace at Cranberry.”

7.2. Cranberry Iron Furnace, Operated by the Cranberry Iron & Coal Company, 1901 (leased; purchased 1905) to at least 1929

Summer 1901. “Carnegie (Cranberry) Furnace”, by John R. Waite.

The “Virginia Iron, Coal & Coke Company went bankrupt during the summer of 1901. The receivers contacted Cranberry Iron & Coal officials with an offer to sell or lease the Carnegie Furnace in Johnson City. The CI&C Board of Directors considered the offer and negotiated a short-term option to lease the furnace for three years.

The lease called for the payment of a royalty of 15 cents per ton of iron the first year, 20 cents per ton for the second year, and 25 cents per ton the third year. VIC&C controlled the shipment of coke from the nearest coalfield and agreed to protect CI&C with a favorable freight rate on coke during the duration of the lease. CI&C had the option to purchase the furnace for \$70,000 at anytime during the lease.

Cranberry management estimated that the furnace could produce at least 30,000 tons of special low-phosphorus pig iron per year. With the cost of mining the ore at \$1.30 per ton and the cost of shipping the ore on the ET&WNC at \$.70 per ton, they estimated a profit of at least \$2 per ton of pig iron. The railroad would also realize a profit of 35 cents per ton of ore hauled. The directors also estimated that \$100,000 would be needed to repair the furnace and provide operating capital.

Unfortunately CI&C had no money and already owed nearly \$350,000 to individual stockholders. The Board of Directors proposed issuing \$500,000 in bonds backed by a mortgage on the mines and the railroad. The creditor stockholders would accept the bonds to liquidate their claims and would purchase an additional \$100,000 in bonds to fund the furnace operations.

When some of the creditor stockholders balked at the proposal and the plan fell apart, a syndicate of the substantial stockholders took matters into their own hands. The Cranberry Furnace Company was incorporated in New Jersey on September 6, 1901, with authorized capital of

\$100,000. The new company leased the Carnegie Furnace and began making repairs. The CI&C Board of Directors then leased all of the Cranberry properties including the mines, mine machinery, store, farm, and houses to the furnace company.

Under the arrangement between the two companies, all the furnace company's profits would go the iron company to be used to reimburse furnace company stockholders for their investment. Once they recouped their investment, Cranberry Furnace Company would become a subsidiary of CI&C, and additional profits would be used to pay off the iron company's creditor stockholders...

The Cranberry companies now had a stable market for iron ore that was, of course, shipped over the ET&WNC Railroad. Coke for the furnace was brought in over the Virginia & Southwestern Railroad to Elizabethton then shipped by way of the ET&WNC to the furnace in Johnson City. Limestone was brought from a new quarry located near Happy Valley, next to the ET&WNC main line. Shipments of these products were to be the economic backbone of the railroad for the next two decades..."³⁴¹

22 August 1901. Johnson City Comet.

"Furnace to Start! – Cranberry Coal and Iron Company will make Bessemer Pig Here. – Lease Contract Signed Tuesday and the Work of Repairing the Furnace to Begin at Once. – The Cranberry Coal and Iron Company will operate the Carnegie furnace.

This will be glad tidings to the people of Johnson City...

All this time the company had its eye on the main chance, so to speak. They found an opportunity to get possession of the modern furnace at satisfactory figures and they have leased the plant of the Virginia Coal and Iron Company in this city for three years with the privilege of purchasing. The lease was signed in Johnson City Tuesday by the directors of the two companies...

The work of repairing the furnace will begin within two weeks and will be pushed with vigor. The mines at Cranberry will be started at once and they will begin handling ore, limestone and coke by the first of October, and by the middle of that month it is hoped that the furnace will go into blast..."

21 November 1901. Johnson City Comet.

"F. Firmstone was in the city Wednesday and in company with the Superintendent was driven out to the Carnegie furnace. The indications are that it will not take long before the work of repairing the plant will be commenced. Several car loads of fire brick have already arrived, but pending a settlement of some legal difficulties in connection with the lease were unloaded up-town."

28 November 1901. Johnson City Comet.

"Work at the Furnace. The lease for the Carnegie furnace to the Cranberry Coal and Iron Co. has been finally closed and the work of repairing is now going ahead. It is hoped to have the furnace ready to go into blast by the first of the year. Superintendent Wilkins is now here and will push the work rapidly."

1901. Directory of Iron and Steel Works of the United States.

"Virginia Iron, Coal, and Coke Company; general offices, Bristol, Tennessee. Officers at Bristol: Archer A. Phlegar, Receiver; G.B. Schley, President; James L. Brass, General Manager;

W.R. Wills, Secretary and Treasurer; D. Davies, Auditor; F.M. Eaton, Sales Agent; and John Warwick, Purchasing Agent. Officers at New York: Henry K. McHarg, Receiver and Vice-President, 40 Wall street, and C.L.E. De Gaugue, Assistant Secretary, 80 Broadway. Selling Agents: For coke pig iron – Crocker Brothers, New York; N.S. Bartlett & Co., Boston, Mass.; the Domhoff and Joyce Company, Cincinnati, Ohio; and Hickman, Williams & Co., Chicago, Ill., and Louisville, Kentucky. For charcoal pig iron – R.C. Hoffman & Co., Baltimore, Md. Board of Directors: George L. Carter, H.K. McHarg, George A. Crocker, Charles B. Squire, Grant B. Schley, E.J. Berwind, and W.B. Dickerman. Executive Committee: Grant B. Schley, E.J. Berwind, and Charles B. Squire. Capital stock authorized, \$10,000,000; outstanding, \$8,425,000. Bonded debt outstanding, \$8,425,000 of first mortgage 5 per cent. fifty year gold bonds. The company operates the following works:

Blast Furnaces – 1 Charcoal and 12 Coke Stacks.

Bristol Furnace, Bristol, Tenn...

Buena Vista Furnace, Buena Vista, Rockbridge county, Virginia...

Carnegie Furnace, Johnson City, Washington county, Tennessee. One stack, 74 x 17, partly erected by the Carnegie Iron Company; work suspended in 1892; stack completed in 1898 by the Carnegie Furnace Company and blown in January 2, 1899; three Whitwell stoves, each 65 x 18; fuel, Looney Creek or Stonega coke; ore, Cranberry; product, low-phosphorus and Bessemer pig iron; annual capacity, 43,000 gross tons. Brand, 'Carnegie.' J.W. Cure, Superintendent. (Formerly operated by the Carnegie Furnace Company.) – Active in 1900.

Crozer Furnaces, Roanoke, Roanoke county, Virginia...

Dora Furnace, Pulaski City, Pulaski county, Virginia...

Embreville Furnace, Embreville, Washington county, Tennessee...

Graham Furnace, Graham, Tazewell county, Virginia...

Max Meadows Furnace, Max Meadows, Wythe county, Virginia...

Radford – Crane Furnace, Radford, Montgomery county, Virginia...

Reed Island Furnace, Reed Island, Pulaski county, Virginia...

Watts Furnaces, Middleborough, Bell county, Kentucky...

Total annual capacity of the 13 stacks: 624,000 gross tons of coke pig iron and 2,250 tons of charcoal pig iron...

[A slightly different version in the same document:] Johnson City Furnace, The Cranberry Furnace Company, lessee, Johnson City, Washington county. Philadelphia office, Drexel Building. One stack, 74¼ x 18, partly erected by the Carnegie Iron Company; work suspended in 1892; stack completed in 1898 by the Carnegie Furnace Company and blown in January 2, 1899; three Whitwell stoves, each 65 x 18; fuel, coke; ore, Cranberry; product, low-phosphorus pig iron; annual capacity, 30,000 gross tons. Brand, 'Cranberry.' Frank Firmstone, President, Johnson City; Calvin Pardee, Vice-President, and F.P. Howe, Secretary and Treasurer, 225 Drexel Building, Philadelphia. (Formerly called the Carnegie Furnace. Owned by the Virginia Iron, Coal, and Coke Company.) – Last active in 1900, but will be blown in in the spring of 1902."³⁴²

16 January 1902. Johnson City Comet.

“One hundred men are now at work repairing the Carnegie furnace, which is soon to be put in blast by the Cranberry people.”

10 February 1902. The Iron Trade Review.

“The Cranberry Furnace Co. has leased from the Virginia Iron, Coal & Coke Co. its blast furnace at Johnson City, Tenn., and expects to have the furnace in blast toward the end of February. Special Bessemer iron, not exceeding .025 in phosphorus, will be made. Ore from the Cranberry mines will be used. The name of the furnace has been changed from Carnegie to Johnson City. William Wilkins is manager.”³⁴³

13 February 1902. Johnson City Comet.

“Sam Simcox, master mechanic at the Carnegie furnace has gone to Michigan to select some additional engines needed at the furnace.”

27 February 1902. Johnson City Comet.

“Sam Simcox has returned from a trip to Michigan, where he went to look at some locomotives for the Cranberry Coal and Iron Co. The company will purchase another narrow gauge switch engine and a broad gauge switch engine to be used at the furnace. Supt. Simcox thinks it will be at least thirty days before the furnace can go into blast and may be longer. There was a great deal of repairing to do about the boilers and other machinery and this is slow work, especially so on account of the severe weather. The brick work has been completed.”

27 March 1902. The Iron Age.

“Walsh & Weidner, Chattanooga, Tenn., are now... overhauling... the Cranberry furnaces at Johnson City, Tenn...”³⁴⁴

27 March 1902. Johnson City Comet.

“The East Tennessee Brick Company is moving its machinery to a lot just below the furnace and will soon have the plant in operation at the new location.”

3 April 1902. Johnson City Comet.

“At the Furnace. Work at the furnace has been progressing very satisfactorily since good weather set in and the management confidently hope to have it in blast by the first of May. After an idleness of several years the scenes around the iron maker have greatly changed. Where bats flew only a few months ago hammers are now flying, and where mud-dobbers worked iron-puddlers are preparing to work, and the tall stack breaths forth a black business-like air, and all is life and hustle.

May it never cease to rumble,
Nor its fires cease to roar,
Until it has exhausted
All the Cranberry ore.”

1 May 1902. Johnson City Comet.

“In Blast. The Carnegie furnace is again in blast. The fires were started Wednesday morning. Supt. Simcox received instruction Tuesday afternoon to start the fires Wednesday and in

order to do so it was necessary to work all of Tuesday night getting the bell in the furnace in position. This bell is a new model and very different from the one that was formerly used. The furnace is leased by the Cranberry Furnace Co. and has undergone a thorough overhauling and it is now in much better condition than it has ever been. There is ample capital behind the enterprise and it is likely to run so long as it will pay to make pig iron. The furnace will run exclusively on Cranberry ore.”

1 May 1902. Johnson City Comet.

“Eugene Pitts is running the switch engine at the furnace. The Cranberry Furnace Company borrowed an engine from the Southern Railway until the engine they recently purchased can arrive.”

8 May 1902. Johnson City Comet.

“The East Tennessee Brick Co. has moved its two brick machines to the new yard near the furnace and will soon have the new plant going. Several thousand dollars’ worth of new machinery has been installed.”

15 May 1902. The Iron Trade Review.

“The Carnegie Furnace, recently leased by the Cranberry Iron & Coal Co., Johnson City, Tenn., has been blown in. Repairs have been made costing nearly \$100,000.”³⁴⁵

22 May 1902. Johnson City Comet.

“Henry Weaver, an employee of the Cranberry Furnace Co., fell about 20 feet from a car on the ore dump at the furnace Saturday and sustained serious injuries. Several bones were fractured and he was badly injured internally.”

7 August 1902. Johnson City Comet.

“The Furnace Banked. The furnace has been banked since last Sunday on account of failing to get coke. Mr. Sweetser has gone to West Virginia to see what can be done and it is hoped to have the furnace in blast again next week. At the time the furnace was banked it was making for iron than at any time in its history. A week previous it had caused considerable anxiety. The material ‘stuck’ and for days they could not get it down. A furnace man from Roanoke was sent for and he relieved the situation in a short time by raising the heat on the stoves and increasing the pressure with the blowing engines. After this it worked nicely and was making nearly a hundred tons of iron daily when the coke supply gave out.”

14 August 1902. Johnson City Comet.

“The furnace will go in blast next week. The coke supply is arriving.”



New Store - New Goods

Cranberry Furnace Co.

This store will be formally opened at the Furnace on

Wednesday, Aug. 20,

and you are invited to come and examine the best and cheapest stock of goods in the city. It is the intention of the management to sell goods here at prices that it believes can not be duplicated elsewhere. ^{Finest} ^{possible} advantage is taken of all the ^{opportunities}. Competent buyers are backed with cash, and you well know that cash counts. It gives US bargains and it will get YOU bargains. A visit to the store will convince you. The entire stock is new and has been selected with great care, and to give you an idea of what we can do we propose to sell, on our opening day, a \$4.00 table damask (and well worth every cent of it) for \$1.00 a yard. Come down and look over the stock. ❁ ❁ ❁ ❁

Vehicles will be run from town to the store on opening day at small cost.

Cranberry Furnace Co.

JOHN W. HUNTER,
Manager of Store.

Figure 85. Johnson City Comet, 14 August 1902.

25 September 1902. Johnson City Comet.

“The furnace is still shut down on account of the impossibility to get coke.”

23 October 1902. Johnson City Comet.

“The furnace has been banked again for want of coke.”

20 November 1902. Johnson City Comet.

“After a wait of several weeks the Cranberry Furnace Co. has been able to get a sufficient supply of coke to put the furnace in blast again.”

18 December 1902. Johnson City Comet.

“The E.T. & W.N.C. railroad company is receiving several car loads of heavy rail to replace the light rail now in use. The company has recently purchased some heavy rolling stock to be used in delivering ore to the Cranberry Furnace Co.’s plant in this city and the heavier rail is a necessity.”

26 February 1903. Johnson City Comet.

“Smallpox in the City. Smallpox in a mild form has been discovered in the ‘Red Row’ at the furnace in Carnegie. There are only four cases and all are in one house. Dr. J.H. Preas, the furnace physician, called in County Physician Stuart last Wednesday and the disease was pronounced smallpox... All furnace employees have been vaccinated and every precaution taken to prevent the disease spreading...”

30 April 1903. Johnson City Comet.

“Relining Furnace. Brickmasons from Youngstown, Ohio, have arrived to reline the Cranberry Furnace Co.’s furnace in this city, and the work is under way. There was a delay of several days waiting for the fire tiling and brick to arrive. In speaking of the wages of these 14 men are to receive a local paper says ‘they will all receive 55 cents an hour’ and ‘get their fare to and from the city thrown in the bargain.’ This would indeed be a bargain for the furnace people but the fact is that these masons are not the cheap skates this paper tries to make it appear they are. They will ‘each’ receive 55 cents an hour.”

4 June 1903. The Johnson City Comet.

“Industrial Notes. The Cranberry Furnace has resumed operation after blowing out in order to reline the furnace.”

11 June 1903. Manufacturers Record.

“Johnson City – Furnace. – Cranberry Furnace Co. has resumed operations at its furnace, which was blown out for repairs.”³⁴⁶

20 August 1903. The Johnson City Comet.

“Well-Digger Wanted. We want a well dug immediately at our limestone quarry near Milligan. Apply at our quarry, or at the furnace office. Cranberry Furnace Co.”

LABORERS WANTED

To work in the mines of the Cranberry Furnace Co., at Cranberry, N. C., and the Limestone Quarry of the Cranberry Furnace Co., at Milligan, Tenn. Good wages and cheap board at both places. Permanent positions to steady workers. Apply to Chris Wissler, Supt., Cranberry, N. C., for position in mines. M. A. Earp, Foreman, Milligan, Tenn., for position at Quarry.

Cranberry Furnace Company,
Johnson City, Tenn.

Figure 86. Johnson City Comet, 7 July 1904

9 March 1905. Johnson City Comet.

“Furnace Purchased. The Cranberry Coal and Iron Co. has purchased the Carnegie furnace and acreage from the V.I.C. & C. Co. The furnace has been operated under a lease for two years in the nature of an option and the company decided in January to take the property. The papers passed lasty week. The plant will be operated as heretofore and it is not impossible to add that an additional furnace may be erected by the company in the near future.”

“Virginia Iron Coal & Coke Company. (Report for Fiscal Year ending June 30, 1905.) President Henry K. McHarg says in substance... Sale of Property – Payment of Bonds. – During the receivership a lease and option to purchase was given to the Cranberry Furnace Co. of the Carnegie furnace, near Johnson City, Tenn., for \$70,000, which option the leases availed themselves of...”³⁴⁷

4 May 1905. Johnson City Comet.

“Connection Established. – The Third Rail on the E.T. & W.N.C. Laid to Elizabethton. – The work of laying the third rail on E.T. & W.N.C. Railroad from Johnson City to Elizabethton has been completed and connection made at Elizabethton with the V.&S.W., and over that road with the outside world. The first standard gauge car was taken to Elizabethton Monday of this week, a car of coffee from New York. The completion of this line makes Johnson City a competitive point, in railroad parlance, and will be a great relief to merchants and shippers in the matter of freight rates...”

May 1905. “Carnegie (Cranberry) Furnace”, by John R. Waite.

“Once the furnace was in full production, management began working to streamline the movement of raw materials. When the dual-gauge track was completed between Johnson City and Elizabethton in 1905, coke could be brought into Johnson City over the ET&WNC without having to be transferred back and forth between standard and narrow gauge cars. One [Ore] from the mines still had to be transferred to standard gauge hoppers and hauled by the Southern Railway (successor line to the ETV&G) to the furnace...”³⁴⁸

11 May 1905. Johnson City Comet.

“Furnace Banked Tuesday. The Cranberry Furnace Company has shut down the furnace for the purpose of making some needed repairs. The furnace was blown out Tuesday. It will be idle several weeks and about 100 men will be thrown out of employment.”

Summer 1905. Third Annual Report of the Harvard Mining Club.

“Summer of 1905... Cranberry Furnace Company at Johnson City. In the town stands a disused iron furnace, the oldest of the district, which was of interest on account of its brick construction and general crudity...”³⁴⁹

13 June 1907. Johnson City Comet.

“Furnace to Blow in. The Cranberry Furnace will go in blast again this week. It has been idle for about six weeks to be relined and otherwise improved.”

17 October 1907. Johnson City Comet.

“L.T. Duke, a contractor of Johnson City, was in Bristol Sunday en route for Richmond, where he has business. He has just completed a contract which he had in Johnson City, having graded for a new track from Johnson City proper to the Carnegie iron furnace, which is intended to accommodate both standard and narrow gauge trains. – Bristol Herald Courier.”

1906 – 1907. Along the ET&WNC, Volume IV: Freight Cars Part B.

“In 1901, when the Cranberry Furnace Company took over the idle facility, there was again talk regarding the necessity of transferring ore between the incompatible gauges. As the productivity of the furnace increased, something needed to be done.

In 1907, the railroad acquired all the of right of way for a side spur to the furnace. Southern railway contributed some land and allowed the crossing in exchange for access and half ownership of the spur. This track was dual gauge, but was rarely used by SR.

The ability to deliver ore directly at the furnace led to the building of drop bottom hoppers, finally ending the manual handling of ore. These cars were similar to the standard style of coal hoppers used by many of the mainline railroads. There is no way to know for sure, but it appears that the powers that be on the ET&WNC simply went out into the yard, looked at empty hoppers cars waiting to be delivered back to the V&SW railroad in Elizabethton, and copied the design for the narrow gauge. The hipper cars built by the ET&WNC were built just like the typical hopper in use on the standard gauge coal roads in the first decade of the Twentieth Century.” The E.T. & W.N.C. appears to have built its first fourteen hoppers around 1906 and 1907.³⁵⁰

February 1908. “Carnegie (Cranberry) Furnace”, by John R. Waite.

“Railroad President, Frank Howe, and general manager, George Hardin sought to eliminate this bottleneck [between the E.T.&W.N.C. mainline and the Cranberry Furnace] by building a spur from the narrow gauge directly to the furnace. Since the spur would have to cross the Southern tracks, they were forced to negotiate with the larger railroad.

After lengthy negotiations the two railroads finally reached an accord. The agreement provided for joint ownership of a dual-gauge spur from the ET&WNC mainline near Exum Furniture, crossing the Southern mainline at grade, to the furnace. As a result, the Southern had access to the Exum plant and the ET&WNC could haul Cranberry ore directly to the furnace in narrow gauge cars. The Carnegie Extension was completed on February 8, 1908.

The Cranberry ore could now be shipped directly to the furnace, but it still had to be unloaded from gondolas by hand once it got there. To solve this problem, the ET&WNC began constructing hopper cars. Soon there were forty-five hoppers on the railway roster.

The iron business provided the Cranberry companies solid profits until the end of World War I...³⁵¹

28 May 1908. Johnson City Comet.

“Slight Wreck on Southern. In the Carnegie yard this afternoon No. 312 was delayed 30 minutes by a slight mishap. The Cranberry Furnace Co.’s yard engine was backing some coal cars toward the main line of the Southern and got past the clearance post just as passenger train No. 312 was trying to go past. The result was the coke cars were struck with such force that they were wrecked and the furnace engine tender destroyed. The engine No. 312 was damaged several hundred dollars and killed, a local engine being supplied and the dead engine and train sent to Bristol after only a slight delay.”

26 November 1908. Johnson City Comet.

“Carnegie Furnace. The Carnegie Furnace has gone into blast, with one new stove and an old one enlarged ten feet, increasing the capacity of the furnace to 125 tons.”

16 October 1911. Industrial World.

“Rumored Steel Project. The Cranberry Furnace Company, operating a blast furnace at Johnson City, Tenn., has purchased a considerable area adjoining its blast furnace plant, and is said to have in contemplation the building of open hearth furnaces. F.P. Howe, Philadelphia, is president; manager, J.A. Koss, Johnson City, Tenn.”³⁵²

28 March 1912. Johnson City Comet.

“Furnace Blows Out to be Repaired. The Cranberry furnace began blowing out Wednesday and will be shut down about two months while it is being relined and undergoing other necessary repairs.”

30 May 1912. Johnson City Comet.

“Furnace Again in Blast. The Cranberry Furnace has resumed operation after an idleness of about sixty days for repairs.”

1920. Directory of Iron and Steel Works of the United States and Canada.

“Cranberry Furnace Co. (The), Johnson City, Tenn. Inc. Sept. 6, 1901 in New Jersey. Capital stock: com. \$100,000. F.P. Howe, P. & G.M.; Ario Pardee, V.P.; at Philadelphia. Hammond Prosser, Sc. & T.; S.F. Boren, Furnace S.; at Johnson City.

Offices: Newark, N.J., Prudential Bldg.; Philadelphia, 253 Drexel Bldg.; and Johnson City, Tenn.

Selling agent: Crocker Brothers, 21 East 40th St., New York. Brand: ‘Cranberry.’

Cranberry Furnace, Johnson City, Tenn.: 1 stack, 75’ 9” x 17’; f.o. Jan. 2, 1899; relined 1919; 4 Whitwell stoves, 69’ x 18’; 3 steam blowing eng. (21,000 cfm.); 12 horizontal flue boilers (850 H.P.). Wise Co. coke; Cranberry ore from company’s mines at Cranberry, N.C., annual cap., 70,000 tons. Owns limestone quarry at Milligan College, Tenn., annual cap., 40,000 tons.

Product: standard low-phosphorus pig iron; annual cap., 36,000 tons.”³⁵³

1920 – 1929. “Carnegie (Cranberry) Furnace”, by John R. Waite.

“Sometime after 1908 [reported in the Johnson City Comet of 26 November 1908, see Map 29A] a fourth stove was added and improvements were made to the hoist and charging mechanisms for feeding the furnace.

By the 1920s the iron market had changed and the furnace could not compete with more modern facilities. The furnace and mines were shut down periodically through the 1920s. Finally, in 1929, the furnace was permanently closed. Within a few years, most of the facility was torn down. Today [1998] only one of the original buildings remains on the site [the Carpenter & Machine Shop]...”³⁵⁴

May 1922. The Blast Furnace and Steel Plant.

“Freyn, Brassert & Company, Chicago, have been retained as consulting engineers by the Cranberry Furnace Company of Johnson City, Tenn.”³⁵⁵

July 1922. The Blast Furnace and Steel Plant.

“The Cranberry Furnace Company, Cranberry, N.C., with branch plant at Johnson City, Tenn., has preliminary plans under way for improvements in its blast furnace, to include relining and other work. Hammond Prosser is secretary.”³⁵⁶

1923. Iron Trade Review.

“Stacks Remodeled during 1923... Cranberry Furnace Co., Johnson City, Tenn...”³⁵⁷

1923. American Mining and Metallurgical Manual.

“Cranberry Furnace Co., Johnson City. Incorporated, New Jersey, 1902. Stock, \$100,000. Shares, \$100.00.

Frank P. Howe, Johnson City, Tenn., President and Manager.

Hammond Prosser, Johnson City, Tenn., Sec. and Purchasing Dept.

S.H. Odom, Cranberry, North Carolina, Mine Superintendent.

S.F. Boren, Johnson City, Tennessee, Furnace Superintendent.

Cranberry Magnetite Iron Ore Mine, Cranberry, North Carolina. Slope. Steam and Compressor. 200-ton Crusher. S. Locomotive.

1 Blast Furnace. (36,000 Tons) 4 Whitwell 4-Pass Stoves.

3 Steam Blowing Engines. 850 HP. Steam Plant: 12 Boilers.”³⁵⁸

12 November 1925. Iron Trade Review.

“After being idle two years, Cranberry Furnace Co., at Johnson City, Tenn., has blown in [during October] on low phosphorus iron.”³⁵⁹

28 March 1929. Watauga Democrat.

“Cranberry Furnace to Resume Operations – The Johnson City Chronicle learns that arrangements for starting operation of the Cranberry Furnace within the next few weeks are going forward though no official announcement as to date has been made. It is understood that at present the plant is awaiting receipt of a shipment of ore from abroad, required as a mixture with the Cranberry product. Indications are that the mines at Cranberry will be opened simultaneous with the opening of the furnace, to supply the high grade magnetic ore to the furnace.

Information is that a number of orders are on hand for pig iron, and the period of contemplated operation is indefinite. The plant will employ about 125 workers.”

30 May 1929. Watauga Democrat.

“Cranberry Smelter to Resume Operations – The Cranberry Furnace Company’s smelter at Johnson City which has been idle since September 1927, is to be ‘set off’ again this week, according to announcement made by General Manager Hammond Prosser.

The Cranberry mines in Avery county have been operating for some time and large quantities of the iron ore have already been shipped to the smelter, where the average daily production of pig iron is about 125 tons. Only a small amount of iron was stacked on the furnace property and this amount is sold and will be shipped within a few days. Officials pointed out that

a large number of orders were now on hand and that the plant would be operated so long as the demand continued.

A supply of Timezrit iron, mined at Timezrit, Africa, and shipped to Johnson City, via New York, has been received and it to be mixed with the Cranberry product when smelted.”

1929. “The [Cranberry] furnace closed in 1929.”³⁶⁰ “Within a few years most of it was torn down. Today [1998], only one building and the remnants of a foundation remain at the site.”³⁶¹ The “Cranberry Furnace was replaced with a new Southern Mica Factory in the 1930s. A chemical fertilizer plant [operated by the Johnson City Chemical Company] located adjacent to the mica factory in the 1950s.”³⁶²

1938. “Iron Ores and Iron Industries of the Tennessee Valley Region.”

“The last furnace to use southern magnetite – the Cranberry Furnace at Johnson City – finally ran chiefly on Algerian ore during its last campaign with a certain admixture of Cranberry concentrates. That furnace has now been scrapped [by 1938] and no magnetite is being used in the United States south of Cornwall, Pennsylvania.”³⁶³

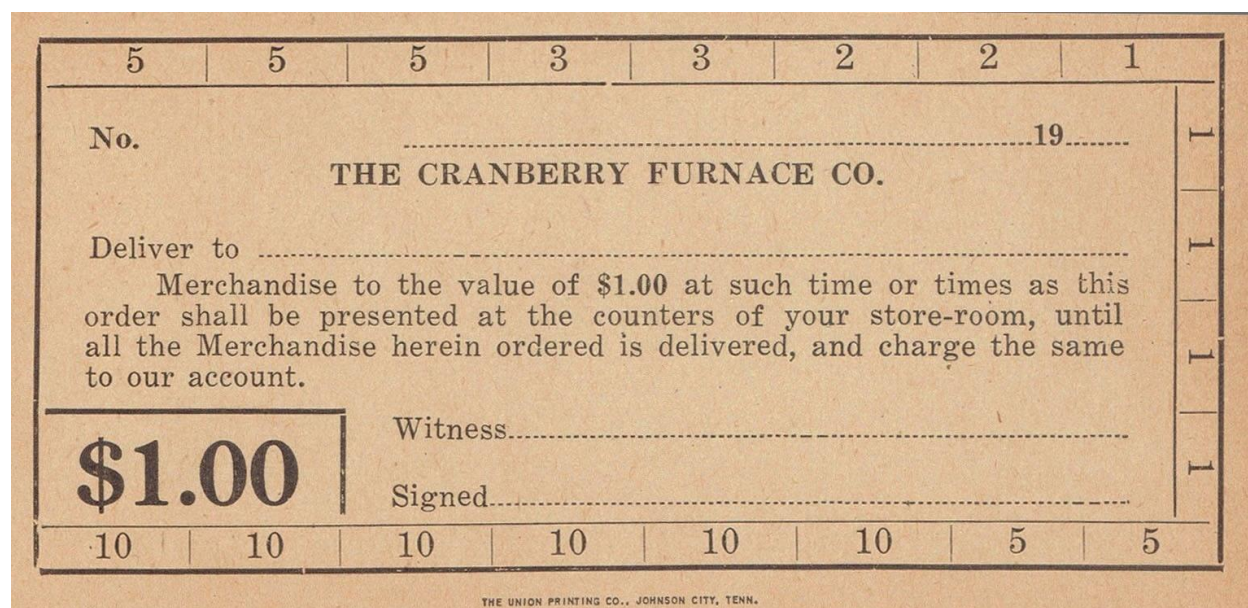


Figure 87. A Voucher from the Cranberry Furnace Company.³⁶⁴

Regrettably, the author could not locate the annual consumption and production rates for the Carnegie Furnace. However, historian Johnny Graybeal’s Along the ET&WNC, Volume IV: Freight Cars Part A, includes relevant general information, provided in the tables below. The furnace first went into blast in 1898, and was closed about 1929.

Table 68. “ET&WNC Freight Tonnage, by Category, Year Ending [30 June], in Tons...”³⁶⁵

	1898	1899	1900	1901	1902	1903	1904
Iron Ore	20	24,995	29,969		10,603	45,810	62,914
Pig Iron	405	147	1,214	1,908	238	20	
Coal	226	1,051	7,706	1,102	3,715	6,525	4,749
Limestone			3,304	(not listed)	(not listed)	(not listed)	(not listed)
Stone/Sand				1,622	4,891	17,694	24,876
Lumber	20,414	12,301	18,562	25,280	37,448	50,783	51,806
Total	24,908	43,232	68,164	38,782	64,224	128,567	152,372

Table 69. “ET&WNC Freight Tonnage, by Category, Year Ending [30 June], in Tons...”

	1905	1906	1907	1908	1909	1910	1911
Iron Ore	62,676	61,975	51,975	54,333	52,560	60,933	76,812
Coal	6,547	30,864	51,178	70,803	46,159	22,343	13,402
Coke			265			58	(not listed)
Stone/Sand	30,020	34,865	40,647	40,405	47,159	42,588	47,873
Lumber	51,806	62,703	82,708	89,473	82,009	81,850	31,250
Oth. Forest	(not listed)	(not listed)	(not listed)	(not listed)	(not listed)	(not listed)	33,852
Total	162,458	207,761	253,159	281,145	195,882	201,225	206,205

Note: “Standard gauge rail added [between Johnson City and Elizabethton in 1905] tends to skew data upward”.

Table 70. “ET&WNC Freight Tonnage, by Category, Year Ending [30 June], in Tons...”

	1912	1913	1914	1915
Iron Ore	77,710	84,440	75,584	70,658
Coal	21,119	25,238	17,204	18,862
Stone/Sand	58,150	70,921	50,142	50,559
Lumber	28,824	24,328	22,325	17,878
Oth. Forest	31,913	25,209	21,785	18,157
Total	213,231	224,334	189,479	173,075

Table 71. “ET&WNC Freight Tonnage, by Category, Year Ending [31 December], in Tons...”

	1916	1917	1918	1919	1920
Iron Ore	87,505	72,049	68,444	54,318	49,665
Coal	25,985	30,830	40,465	32,181	25,961
Stone/Sand	76,962	69,121	59,394	30,555	42,227
Lumber	31,108	33,996	14,670	78,260	22,908
Oth. Forest	46,669	50,993	22,010		1,374
Total	392,111	377,458	307,727	288,900	307,854

Table 72. “ET&WNC Freight Tonnage, by Category, Year Ending [31 December], in Tons...”

	1921	1922	1923	1924	1925	1926	1927
Iron Ore	57	4,840	51,267		18,168	11,612	32,175
Coal	10,600	15,192	25,149	20,981	26,070	51,624	58,813
Clay/Gravel	16,252	19,490	53,088	29,147	21,682	18,250	25,235
Lumber	11,616	19,095	23,529	19,039	41,819	29,828	16,886
Cord Wood		435	1,474	1,046	553	4,382	7,486
Pulp Wood	13,200	16,052	24,158	16,057	5,180	9,630	29,433
Total	120,856	171,892	325,627	185,579	204,453	259,710	[185,010]

Table 73. “ET&WNC Freight Tonnage, by Category, Year Ending [31 December], in Tons...”

	1928	1929	1930
Iron Ore		30,857	
Coal	79,809	135,697	143,736
Clay/Gravel	9,505	8,745	1,362
Lumber	16,839	1,495	3,001
Cord Wood		3,718	415
Pulp Wood	10,170	15,454	6,270
Total	[118,201]	104,016	[155,049]

Table 74. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year 1913.³⁶⁶

Firm Name	Nature of Business	No. Employees at Time of Inspection		Hours Labor Week		Gen'l. Cond.	No. Insp.
		Male	Female	Male	Female		
Cranberry Furnace Co.	Furnace	100		60 – 84	2 shifts	Fair	1

Table 75. "Manufacturing Industries and Firms Inspected and Reported... Johnson City, Washington County", for the Year Ending 1 December 1916.³⁶⁷

Firm Name	Character of Business	No. Employes at time of inspection				No. Hours Labor Males		No. Hours Female and Min. Under 16		General Condition
		Over 16		Under 16		Over 16		Min. Under 16		
		M.	F.	M.	F.	Da.	Wk.	Da.	Wk.	
Cranberry Furnace Co.	Blast Furnace	145				12	72			Good

Table 76. "Directory of Industries Inspected, with No. Employes, December 1, 1916, to December 31st, 1917... Washington County – Johnson City".³⁶⁸

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Cranberry Furnace Works	Blast Furnace	96			

Table 77. "Directory of Industries Inspected, with No. Employes... Washington Co. – Johnson City", for the Year Ending 31 December 1918.³⁶⁹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Cranberry Furnace Co.	Blast Furnace	90			

Table 78. “Directory of Industries Inspected, with No. Employes... Washington County – Johnson City”, for the Year Ending 31 December 1922.³⁷⁰

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Cranberry Furnace	Blast Furnace – Mfg. Pig Iron	71			

Table 79. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1925.³⁷¹

Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Cranberry Furnace Co.	Blast Furnace	82			

Table 80. “Directory of Industries Inspected with Number of Employes... Washington County – Johnson City”, for the Year Ending 31 December 1926.³⁷²

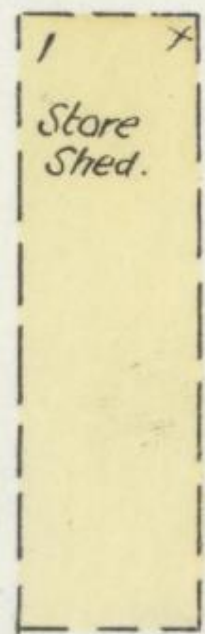
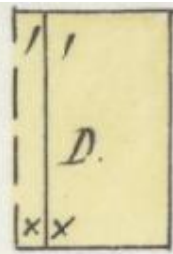
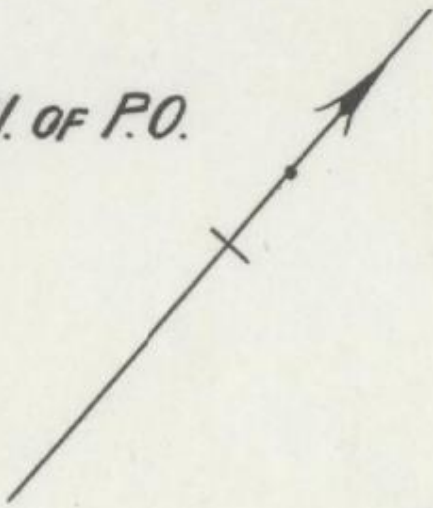
Style of Firm	Business	No. Employes			
		Over 16		Under 16	
		Male	Female	Male	Female
Cranberry Furnace Co.	Blast Furnace	75			

8. The Area Occupied by the Miller & Carmichael Handle Factory and Related Enterprises,
1892 – 1906

Miller & Carmichael Handle Factory, 1892 – 1896
Boring Brothers Plow Handle Factory, 1896 – 1899
J.D. Weaver & Sons, 1902 – 1903
Unaka Lumber Company, at this location 1903 – 1906
Guernsey/Gurney Manufacturing Company (Lessee), 1905 – 1906

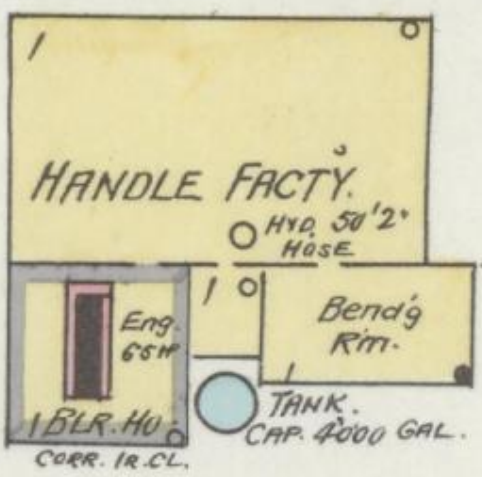
This facility was located about a mile north of the Carnegie Furnace complex, on the 3C's/O.R. & C./South & Western/Clinchfield furnace spur. The author did not find any information on the fate of this facility.

3 MILES N. OF P.O.

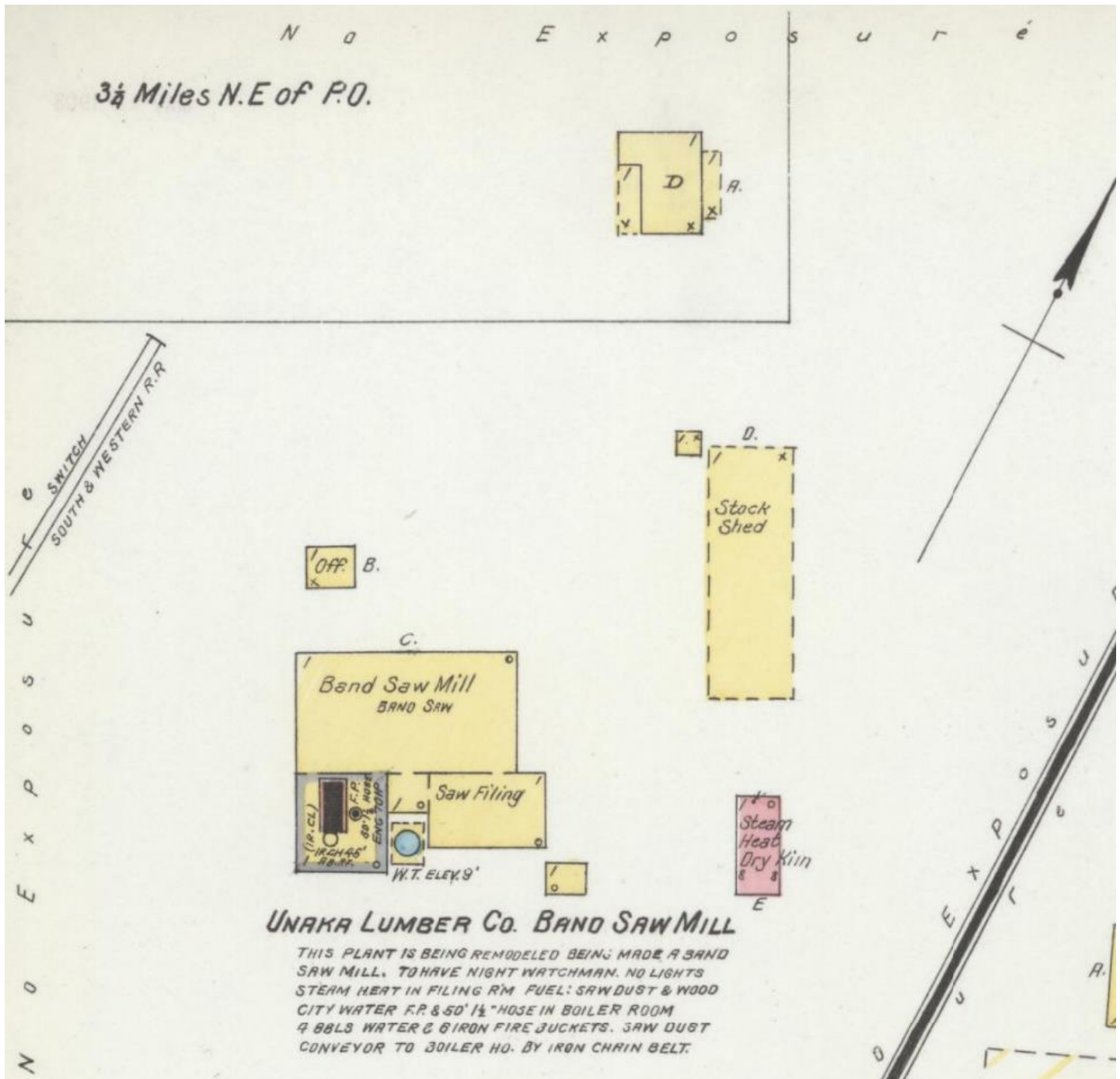


BORING BROS. PLOW HANDLE FAC.

WATCHMAN. NO CLOCK, POWER, STEAM.
FUEL: SHAVINGS & COAL, NO HEAT.
LIGHTS: CLOSED LANTERNS. 50' 2" HOSE.
CITY WATER. - NO EXPOSURE ANY SIDE.



Map 32. The "Boring Bros. Plow Handle Fac.", which was located north of the Carnegie Furnace, in May 1897.³⁷³



Map 33. The “Unaka Lumber Co. Band Saw Mill” “This plant is being remodeled being made a band saw mill...”, which was located north of the Carnegie Furnace, in April 1903.³⁷⁴ Regrettably, the author has not located any images of this facility.

Miller & Carmichael Handle Factory, 1892 – 1896
Boring Brothers Plow Handle Factory, 1896 – 1899
J.D. Weaver & Sons, 1902 – 1903
Unaka Lumber Company, at this location 1903 – 1906
Guernsey/Gurney Manufacturing Company (Lessee), 1905 – 1906

8.1. Miller & Carmichael Handle Factory, 1892 – 1896

August 1892. The Wood-Worker.

“Miller & Carmichael will erect a handle factory at Johnson City, Tenn.”³⁷⁵

6 October 1892. Johnson City Comet.

“Ready for Operation. – The Machinery at the New Handle Factory Given a Trial. – Yesterday the residents in Carnegie were startled by the sound of a new and strange whistle. It was at the handle factory of Miller & Carmichael near the furnace.

Everything was ready and the engine was steamed up to give the machinery a trial. Everything is the latest improved patterns and from the most reliable makers and worked like a charm.

As soon as material can be hauled to the grounds the factory will begin operation on a large scale.

A large number of operatives will be given employment at the factory and getting out timber, and the pay roll will make glad hearts of the laborers and merchants.

Thus, step by step, Johnson City continues to grow, thanks to enterprising strangers and residents, in spite of the Three C’s.”

15 December 1892. Johnson City Comet.

“Gone to Work. The handle factory commenced work yesterday. It has a capacity of from two to three thousand hammer handles daily, and a number of other handles can also be made. The factory will employ fifteen hands.”

15 December 1892. Johnson City Comet.

“Handle Factory. – The Machinery All Up and Work Will Commence in a Few Days. – The machinery for the new handle factory of Carmichael & Miller, in Carnegie, has all arrived, and been placed in position, and work will be commenced in a few days, probably the first of next week. They are receiving timber by the car load, and will turnout several hundred handles daily, when they commence.”

20 April 1893. Johnson City Comet.

“No Boom. – But it seems that Johnson City is Still Building Up. – One of the most substantial concerns in Johnson City, is the Handle Factory of Miller & Carmichael situated in the Carnegie Addition just outside of the present corporation on the 3C’s road. This factory has been in operation just long enough to protect its success. It has a capacity of two or three thousand

handles per day and it is encouraging to learn that orders sufficient are being placed to keep it running every day. It employs twenty-five or thirty hands, eighteen regularly inside the plant, the others in the woods. A saw-mill will be set up this week down in the Cash Hollow on the 3C's for getting pout material for handles. This new feature will facilitate business and give employment to several more hands. The reported found Mr. Carmichael in the store room packing handles to fill a big order for a New York firm and was assured by that clever gentleman that business was a success in every particular."

8.2. Boring Brothers Plow Handle Factory, 1896 – 1899

19 March 1896. Johnson City Comet.

"Another Factory. Our city will have another industry in a few days, or a revival of an old one at least. Receiver Broding has, by consent of all parties, sold the old axe handle factory, established three years ago by Miller & Carmichael, to the Boring Bros., and they will engage at once in the manufacture of plain handles. Originally the plant cost \$6000, but sold for \$1500. Deed was made yesterday, but all the property was not conveyed, as a portion of it did not suit the purchasers.

The purchasers are brothers of John W. Boring, who has been identified with the manufacturing interests of the city for some time, and are enterprising, public spirited men and will be a valuable acquisition to the city.

This is what Johnson City needs – more industries – and she will suffer, and that badly, if she doesn't get them."

16 April 1896. Johnson City Comet.

"The Boring Bros. began repairs on the old axe handle factory last Monday, and in the future will only manufacture plow handles. The machinery will be in place in a very short time, and the factory will be in operation within thirty days."

4 June 1896. Johnson City Comet.

"Boring Bros. axe handle factory was also started up last Monday and will give employment to about twenty operations. Let the good work go on."

10 December 1896. Johnson City Comet.

"The Factories are Running. Boring Bros. are running on full time making plow handles. They have been in operation every day for three months turning out about 200 handles a day. This is one of the most substantial industries in the city and employs about twenty men. The firm is gradually adding new machinery and has established a good trade.

17 February 1898. Johnson City Comet.

"Johnson City – The Future Manufacturing Center of East Tennessee... Plow handle factory, operated by Boring Bros., employing 25 hands..."

16 November 1899. Johnson City Comet.

“Handle Factory Sold. Boring Bros. have sold out, or rather agreed to stop making handles for a period of years for a handsome consideration. The factory has been closed. The purchasers are Atlanta capitalists.”

8.3. J.D. Weaver & Sons, 1902 – 1903

13 February 1902. Johnson City Comet.

“Will Start Planing Mills. J.D. Weaver & Sons have purchased the old handle factory near the furnace from Boring Bros. and will make it an up-to-date planing mill. For more than a year this firm has been doing a general contracting business, but were handicapped in getting material. To overcome this they have decided to put up a plant of their own and bought this old factory as a nucleus. The plant will be ready for operation in the early spring.”

8.4. Unaka Lumber Company, at this location 1903 – 1906

26 March 1903. Johnson City Comet.

“Modern Saw Mill. The Unaka Lumber Company recently purchased the old handle factory from J.D. Weaver and its putting in a modern lumber manufacturing plant. A large band-saw mill is now being placed, and other machinery will be added in order to furnish dimension stock of all kinds. A siding has been put in from the main line of the S. & W. road and the shipment of logs to the plant has been in progress for some time.”³⁷⁶

25 February 1904. Johnson City Comet.

“Lee Smith has his derrick up and is loading a large lot of fine logs for the Unaka Lumber Co.”

1 September 1904. Johnson City Comet.

“For Sale – The Unaka Lumber Company has five thousand cords of wood for sale, and also lumber as low as \$5.00 per thousand feet, at saw mill in Carnegie.”

4 October 1904. American Lumberman.

“From a Hardwood Center. Johnson City, Tenn., Oct. 4. – At 11 o’clock today fire caught in the lumber yard of the Unaka Lumber Company, at Carnegie, about a mile from this place, from a small dwelling house located in the upper end of the yard. At 4 o’clock about 3,500,000 feet of lumber with 350,000 feet of logs had gone up in smoke, with little hope of saving the mill. The drouth for the past six weeks has left everything as dry as tinder and but very little water remained with which to fight the fire. This stock of lumber belonged to the Galloway – Pease Company and was one of a high grade of quartered oak and was particularly well manufactured. Both the lumber and mill are understood to be fully covered by insurance. The loss will be about \$85,000.”³⁷⁷

6 October 1904. Johnson City Comet.

“Lumber Yard Burned. Last Tuesday Johnson City had the greatest fire in its history. The lumber yard of the Unaka Lumber Co. was completely destroyed and about four million feet of lumber went up in smoke, together with several dwellings that were occupied by the workmen. The fire originated in one of the dwellings, and on account of a strong wind spread rapidly to all parts of the yard. The yard is located in the edge of the city limits on the north, but out of reach of water and the fire department could be of no avail. The mill was not destroyed, as the wind was blowing north, and work was resumed Thursday morning. It is estimated that the loss will run up to about \$100,000, with \$85,000 insurance.”

29 October 1904. American Lumberman.

“Business Not Interrupted. As recounted in the Lumberman two weeks ago the Galloway – Pease Company, Johnson City, Tenn., lost its entire stock of hardwood lumber at its Unaka mill. The loss was \$95,000 and the company had an insurance of \$87,500 and a salvage that amounted to \$3,400. Outside of the loss of business entailed the financial damage to the concern was small. The saw mill was not burned and it suspended operations for only one day and is now sawing oak and poplar at the rate of about 35,000 feet a day. The company’s band mill No. 2 has been operating for about four months and has an accumulation of quartered oak and poplar. This stock, together with that at its mills at Emeryville Branch, Cranberry and Shell Creek, leaves the company fairly well equipped for present trade. The stock lost by the company through fire will embarrass it but little in filling orders, as it is rapidly accumulating a full line of stock.”³⁷⁸

19 November 1904. American Lumberman.

“The Unaka Lumber Company, of Sarnegie [Carnegie], Tenn., is recovering from the serious effects of the \$100,000 fire which destroyed the greater part of its yards a few weeks ago. It is filling orders promptly and has about finished the adjustment of the insurance, which was \$80,000.”³⁷⁹

8 December 1904. Johnson City Comet.

“Duplicating Plants. The Unaka Lumber company is duplicating its plant in this city. The large band mill that has been in operation on the land recently purchased by the company on Boone’s Creek, has been brought to this city and is being installed at the plant here. This company is doing an immense business and is constantly adding to its plant. The recent loss by fire has been adjusted, the company receiving \$78,000.”

16 March 1905. Johnson City Comet.

“For Sale. 1000 loads of wood at the Unaka Lumber Company’s plant near the furnace. Apply to F. Gillian.”

8.5. Guerney/Gurney Manufacturing Company (Lessee), 1905 – 1906

4 May 1905. Johnson City Comet.

“Will Make Bowls. The Unaka Lumber Co. has leased it plant north of the furnace to Guerney Bros. who will operate it in the manufacture of wooden bowls and other wood specialties.”

17 August 1905. Manufacturers Record.

“Manufacturing Wooden Bowls. Messrs. James W. and F.P. Gurney, who were president and secretary-treasurer, respectively, of the Gurney Manufacturing Co., late of Chattanooga, Tenn., which has discontinued business, are now engaged in the manufacture of bowls at Johnson City, Tenn., having taken over that feature of the Gurney Manufacturing Co. The Messrs. Gurney invented and built the machinery and developed the processes for manufacturing both pulleys and bowls, which machinery and methods are employed by the company with which they were lately connected.”

“Johnson City – Woodworking Factory. – Jas. W. Gurney and F.P. Gurney, formerly president and secretary-treasurer, respectively, of the Gurney Manufacturing Co. of Roan Mountain and later of Chattanooga, Tenn., and recently discontinued, are now actively engaged in the manufacture of wood bowls at Johnson City. The Messrs. Gurney state that they build and develop the machinery for the Gurney bowls, pulleys, etc.”³⁸⁰

23 August 1906. Manufacturers Record.

“Johnson City, Tenn. – Woodworking Plant, Guerney Manufacturing Co. has been incorporated with a capital stock of \$10,000 to manufacture bowls, pulleys, etc.; incorporators are J.W. Guerney, F.P. Guerney, T.J. May, J.W. Tipton and Lee F. Miller.”³⁸¹

23 August 1906. Johnson City Comet.

“Will Remove Plant. The Unaka Lumber Company has purchased a large boundary of timber near Columbia, S.C., and will remove its manufacturing plant from this city to Columbia. The transfer will be made about the middle of October, or as soon as the stock now on the yard can be worked up.” It appears that this article pertains to the machinery in the mill leased by Gurney/Guerney Manufacturing Company.”

20 September 1906. Manufacturers Record.

“Johnson City, Tenn. – Woodworking Plant. – Gurney Manufacturing Co. reported incorporated August 23 with \$10,000 capital stock, will manufacture wood bowls for bakers; use, with patent built-up rims; daily capacity 20 dozen. Buildings have not been decided on. James W. Gurney is president, and F.P. Gurney, vice-president, secretary and treasurer.”³⁸²

9. Unlocated Trackside Businesses in Carnegie

Oliver Cure's planing mill, 1887
Wood-Working Factory, 1890
J.M. Buck Lumber Company, by 1896 to at least 1905
A.M. Mettetal and Sons, at this location 1898 – 1901?
East Tennessee Brick Company, 1902
Wood – Galloway Company, 1905 – 1907
Southern Leather Company, 1907

9.1. Oliver Cure's planing mill, 1887

12 May 1887. Johnson City Comet.

“Land was bought Tuesday in Johnson City for Planing mills. Particulars next week.”

26 May 1887. Johnson City Comet.

“New Planing Mills. A few weeks ago we mentioned the fact that parties were here looking out a site for planing mills. Since then a site has been procured (having been donated by Messrs. Crandall and Harris) and lumber is now being prepared at Roan Mountain, Tenn., to be used in the building. Capt. W.J. Haney is proprietor of the works and has ample means to operate it on a large scale. He will build largely and will use the latest improved wood-working machinery. He will do a regular planing mill business and will carry about 100,000 feet of dressed lumber on hand at all times. This will be the largest planing mill in East Tennessee and was secured to Johnson City through the influence of Gen. J.T. Wilder.”

2 June 1887. Johnson City Comet.

“In our article last week about the new planing mill we said it would be built by Capt. W.J. Haney. While Capt. Haney has made arrangements to build and operate a planing mill and sash and door factory, and will begin building this summer, the gentlemen who will build the works referred to is Mr. Oliver Cure, a nephew of Gen. J.T. Wilder. The building will be commenced in a few weeks, just as soon as the lumber can be sawed.”

9.2. Wood-Working Factory, 1890

23 October 1890. Johnson City Comet.

“Located. – Johnson City Gets a Large Wood-Working Factory. – Work Will Begin on the Plant in a Few Days. – The Proctor Land Company has closed the contract with Cook & Co., of New Jersey, for the location of a large wood-working establishment in Johnson City. The plant will be located in the Proctor Land Company's Addition on the line of the Three C's road. By the terms of the contract, work is to commence on the plant within sixty days, by Mr. Cook says he will begin at once. The first building erected will be a large saw and planing mill with a capacity

of 30,000 feet of dressed lumber per day. This plant alone will cost \$100,000. The Company will also erect car works and a coffin factory. It is the intention of the company to put in a \$500,000 plant here and give employment to several hundred men. Johnson City is located in the geographical centre of the largest untouched forests of hard-wood timber in America and has natural advantages for establishments of this kind that are unexcelled.”

9.3. J.M. Buck Lumber Company, by 1896 to at least 1905

16 July 1896. Johnson City Comet.

“Fatal Accident. W.F. Brooks, a laborer in the J.M. Buck lumber yard, while transferring lumber in the Carnegie yard Monday afternoon slipped and fell, striking his head against a sill and crushing his skull, from the effects of which he died in a few minutes. Mr. Brooks was a poor but highly respected man, and leaves a wife and six children and a number of friends to mourn his loss.”

1 July 1897. Johnson City Comet.

“Killed by the Cars. Chas. Phillips, brakeman on the Narrow Gauge, was instantly killed Tuesday at 2 o’clock on J.M. Buck’s lumber yard below the foundry [should probably read “furnace”].

Phillips was switching when the accident occurred. He was on the end of a coal car and a pile of lumber struck him, knocking him off. He fell under the car and was dragged for thirty yards by the wheels and brake beam. His head and upper part of his body was badly crushed and mangled. Life was gone when he was reached.

Phillips was a young man only about 22 years old and was unmarried. He was the son of W.C. Phillips at Cranberry, and his mother and father both living. The sad intelligence was wired them at once and the remains of their beloved boy sent to them on the 3:45 train.

We understand that the Buck Lumber Co., is in no way responsible for the accident, as the lumber was being handled by an independent contractor, who had a contract at a certain price per thousand to take lumber from the car and pile it on their yard while it appears that for convenience the contractor placed a small pile of lumber near the track without the knowledge or direction of the Buck Lumber Co.”

19 August 1897. Johnson City Comet.

“Suit for Damages. As a consequence of the death of Chas. Phillips, the young man who was killed a few months ago while coupling cars in the Narrow Gauge yards in this city, suit for \$25,000 damages has been brought by Henry D’Armond, administrator, against the E.T. & W.N.C.R.R. Co., the J.M. Buck Lumber Co., and the Southern Railway Co. The Buck Lumber Co. filed a demurrer, which was sustained by the court and the case dismissed as to it.

It will be remembered that young Phillips, while holding to the ladder of a car on the Narrow Gauge road, was struck by a stack of lumber belonging to the Buck Co. and was instantly killed. The demurring company claimed exemption from liability because permission had been given by the railway company to stack lumber on its grounds, and the claim was held as good by the court.”

9 June 1904. Johnson City Comet.

“An amendment to the charter of the J.M. Buck Company, of Washington county, increasing the capital stock to \$60,000, was filed with the Secretary of State Saturday.”

17 November 1904. Johnson City Comet.

“Lumber Yard. The J.M. Buck Lumber Co. has purchased two acres of land for the Carnegie Development Co. below the S. & W. depot and will use it for a lumber yard for assorting purposes but will not build cottages upon it because there will not be room and it would render it impossible to get insurance on the lumber.”

1 December 1904. Johnson City Comet.

“Building Sidetrack. The J.M. Buck Lumber company began the construction of a sidetrack through its new lumber yard Monday morning. The yard is located on the south of the Southern railway and opposite the South and Western crossing. It will be reached by both roads. The ties are being delivered and track laying will begin at once.”

14 September 1905. The Johnson City Comet.

“Office Moved. The J.M. Buck Lumber Co. has moved its offices to room No. 103 in Hotel Carnegie, and Mr. Haskett Wood is in control, his father having bought the interest of Mr. J.M. Buck in the corporation. This is one of the largest lumber companies in this section, owning a large boundary of timber on the South & Western and operating a private railroad through its property.”

9.4. A.M. Mettetal and Sons, at this location 1898 – 1901?

4 August 1898. Johnson City Comet.

“A New Industry. Prof. A.M. Mettetal and sons have established a permanent agency for the sale of the leading brands of fertilizers at this city. They expect to keep on hand in their season the various brands of acid, potash and nitrogen preparations for the various crops, and assist farmers in making a proper selection by careful inspection of soil by actual test. Special fertilizers will be kept for wheat, oats, corn, tobacco, potatoes and garden vegetables.

This will obviate the necessity of going to Jonesboro and even Bristol for fertilizers, as many in this vicinity were compelled to do last spring.”

29 September 1898. Johnson City Comet.

“50 Tons of Fertilizer for Cash, Wheat or 12 months credit. Mettetal & Sons.”

Note: By June 1901, it appears that this enterprise had relocated to East Market Street and was no longer directly served by any of the railroads in Johnson City.³⁸³

Fertilizers and Chemicals.

A. M. METTETAL & SONS.

Office at Hilton's Store,
Market St.

Warehouse in Carnegie, near
Hotel, on Railroad.

Choice grades of Fertilizer in stock
and Chemicals for preparing special
Wheat Fertilizers always on hand.

Farmers can save one-half of Ferti-
lizer bill by preparing their own Ferti-
lizers. We give our customers com-
plete directions free for making High
Grade Goods.

Figure 88. Johnson City Comet, 20 July 1899.
This advertisement ran until at least 9 November 1899.

9.5. East Tennessee Brick Company, 1902

3 April 1902. Johnson City Comet.

“Brick Plants... The East Tennessee Brick Co. is moving its plant to a lot near the furnace and has it about ready to put in operation.

M.H. Campbell & Co. are going to make some hand-made brick on the old yard below Hotel Carnegie and are now getting ready to do so.”

9.6. Wood – Galloway Company, 1905 – 1907

19 October 1905. Johnson City Comet.

“New Wood Working Plant. Johnson City is to have a new wood working plant in the near future. T.J. Galloway and Haskett Wood have arranged to build and equip a planing mill upon their land near the S. & W. crossing in Carnegie. It is the intention of the promoters to cut non-shipping lumber into dimension stock of all kinds, particularly chair stock. The building will be equipped with the most modern machinery obtainable for that purpose. Work will be commenced upon the building at once.”

19 October 1905. Manufacturers Record.

“Planing-mill Machinery. – Wood – Galloway Company, Johnson City, Tenn., will be in the market for machinery and equipment for woodworking plant, also for planing mill.”³⁸⁴

1907. The Lumber Manufacturer and Dealer.

“C.J. Murrow has been selected as trustee of the Wood – Galloway Lumber Co., which went into voluntary liquidation at Johnson City some days ago. He represents William Whitmer’s Sons, of Philadelphia, one of the heaviest creditors. The assets of the concern will, it is believed, almost offset the liabilities, which will probably aggregate in the...”³⁸⁵

9.7. Southern Leather Company, 1907

2 May 1907. Johnson City Comet.

“Southern Leather Company. The Southern Leather Company has been incorporated with a capital stock of \$5,000 by John C. Laher, Thomas Cooper, A.G. Riddle, Chas. Snodgrass and T.A. Cox. The plant is located in Carnegie, opposite the South and Western depot and the company will soon be ready for business.”

Appendix A.
Map Legends


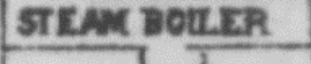
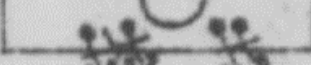
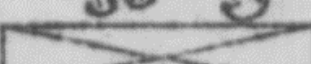
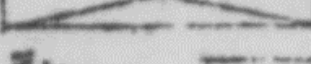

KEY									
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="padding: 2px;">NO. OF STORIES</td> <td style="text-align: center; padding: 2px;">3</td> </tr> <tr> <td style="padding: 2px;">SHINGLE ROOF</td> <td style="text-align: center; padding: 2px;">X</td> </tr> <tr> <td style="padding: 2px;">COMPOSITE ROOF</td> <td style="text-align: center; padding: 2px;">●</td> </tr> <tr> <td style="padding: 2px;">SLATE OR TILED ROOF</td> <td style="text-align: center; padding: 2px;">○</td> </tr> </table>	NO. OF STORIES	3	SHINGLE ROOF	X	COMPOSITE ROOF	●	SLATE OR TILED ROOF	○	I FIRE WALL 6 IN. AB. ROOF
NO. OF STORIES	3								
SHINGLE ROOF	X								
COMPOSITE ROOF	●								
SLATE OR TILED ROOF	○								
	H " " 12 " " "								
	III " " 18 " " "								
	FRAME PARTITION								
	OPENING & IRON DOOR								
	WINDOWS " " SHUTTERS								
COUNTING FROM LEFT TO RIGHT WHILE LOOKING TOWARDS BUILDING	WINDOW IN 1 ST STORY								
	" 1 ST & 3 RD "								
DOY REPRESENTS OPENING	" 2 ND & 4 TH "								
B'LD'GS COLORED YELLOW ARE FRAME	" " RED " BRICK								
" " " BLUE " STONE	" " GREY " IRON								
" " " BROWN " ADOBE	" " GREEN " SPECIALS								
" MARKED (Cl) ARE CLOTH LINED	(5) (19) INDICATE RELATIVE HEIGHTS								
ALTERNATE STREET NO'S ARE ACTUAL	CONSECUTIVE " " " ARBITRARY								
BROKEN LINE NEAR B'LD'G - WOOD CORNICE	SOLID " " " METAL "								

Figure 89. Map Legend for January 1891.³⁸⁶

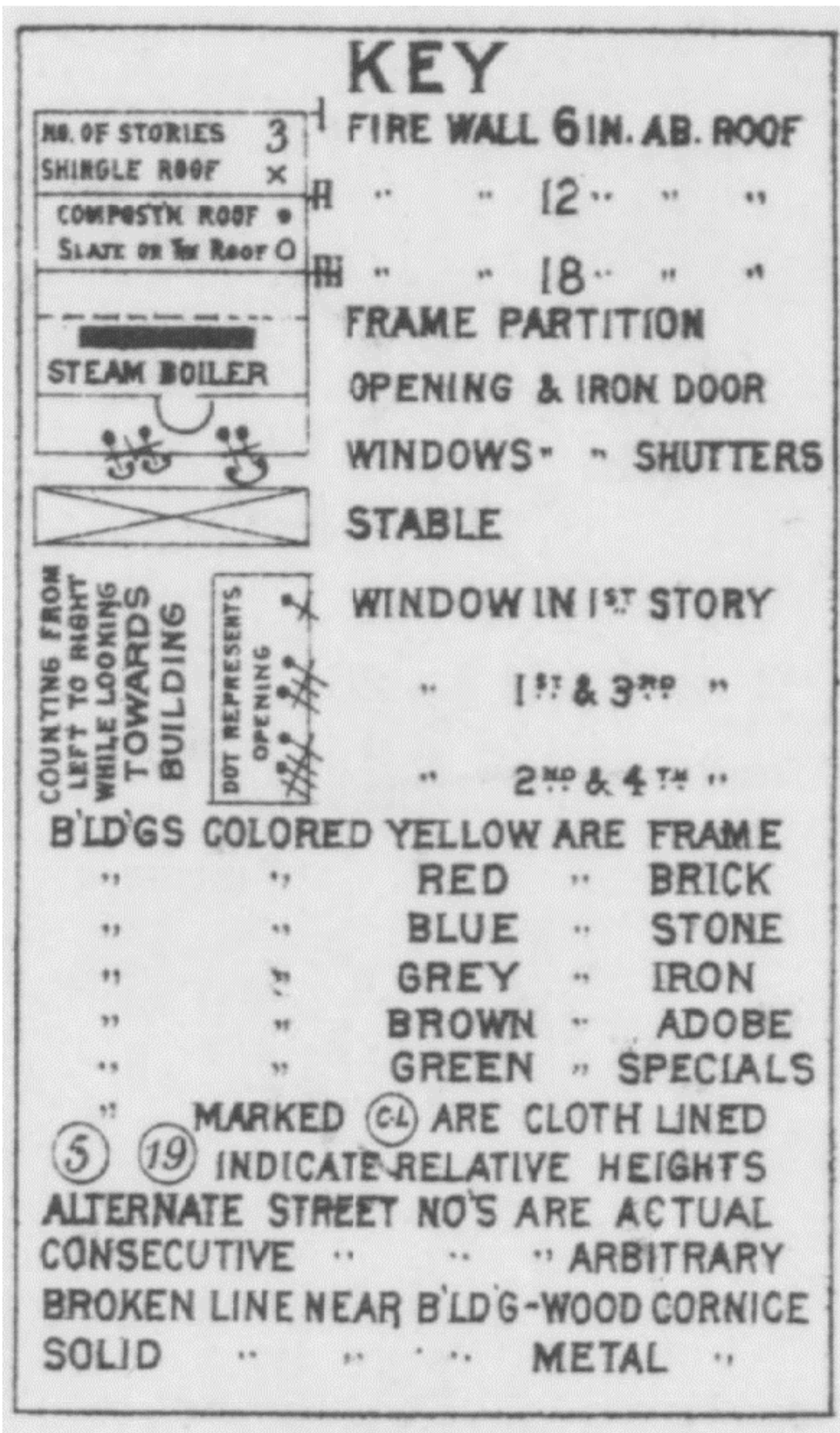


Figure 90. Map Legend for May 1897.³⁸⁷

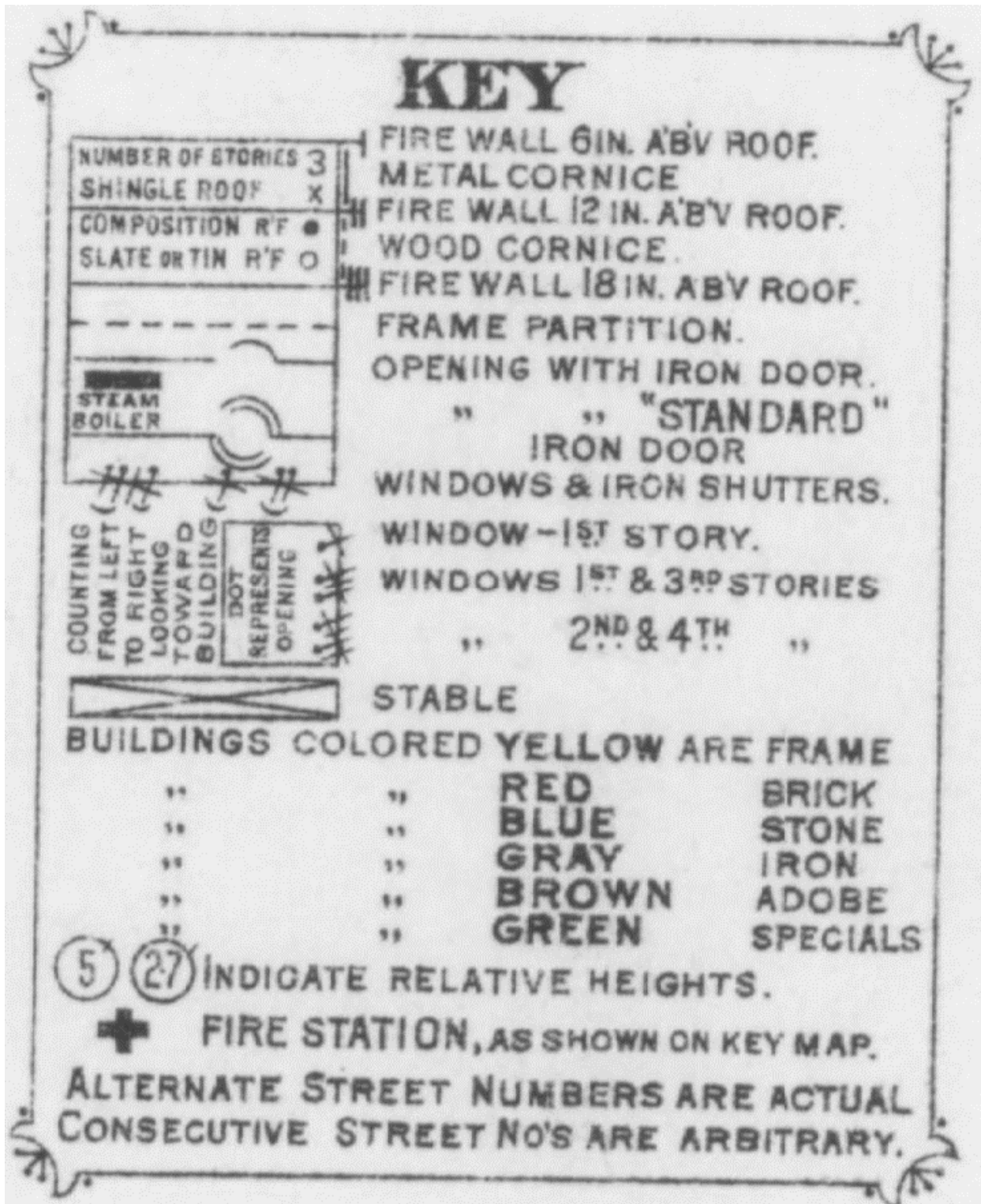


Figure 91. Map Legend for April 1903.³⁸⁸

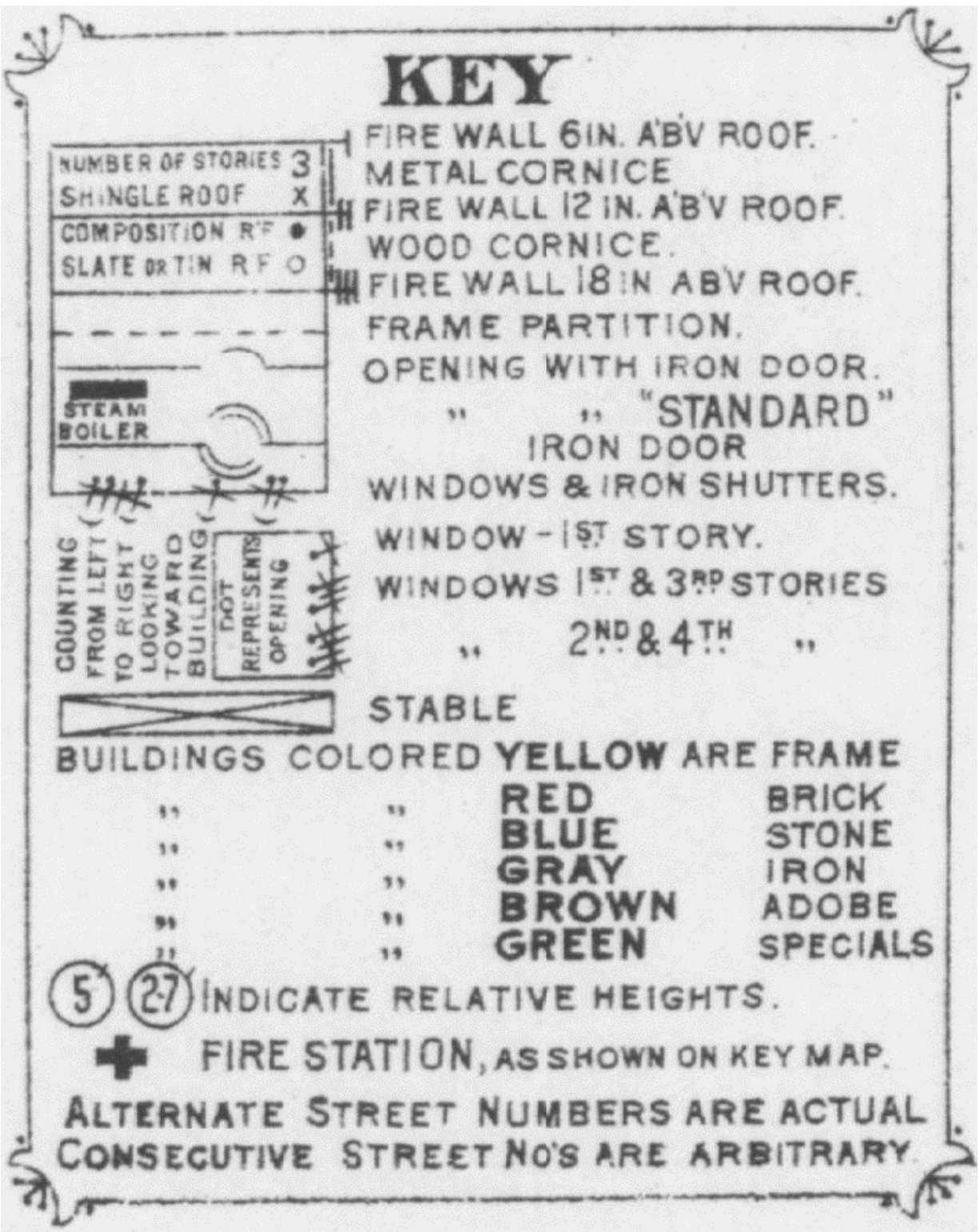


Figure 92. Map Legend for December 1908.³⁸⁹



Figure 93. Map Legend for July 1913.³⁹⁰

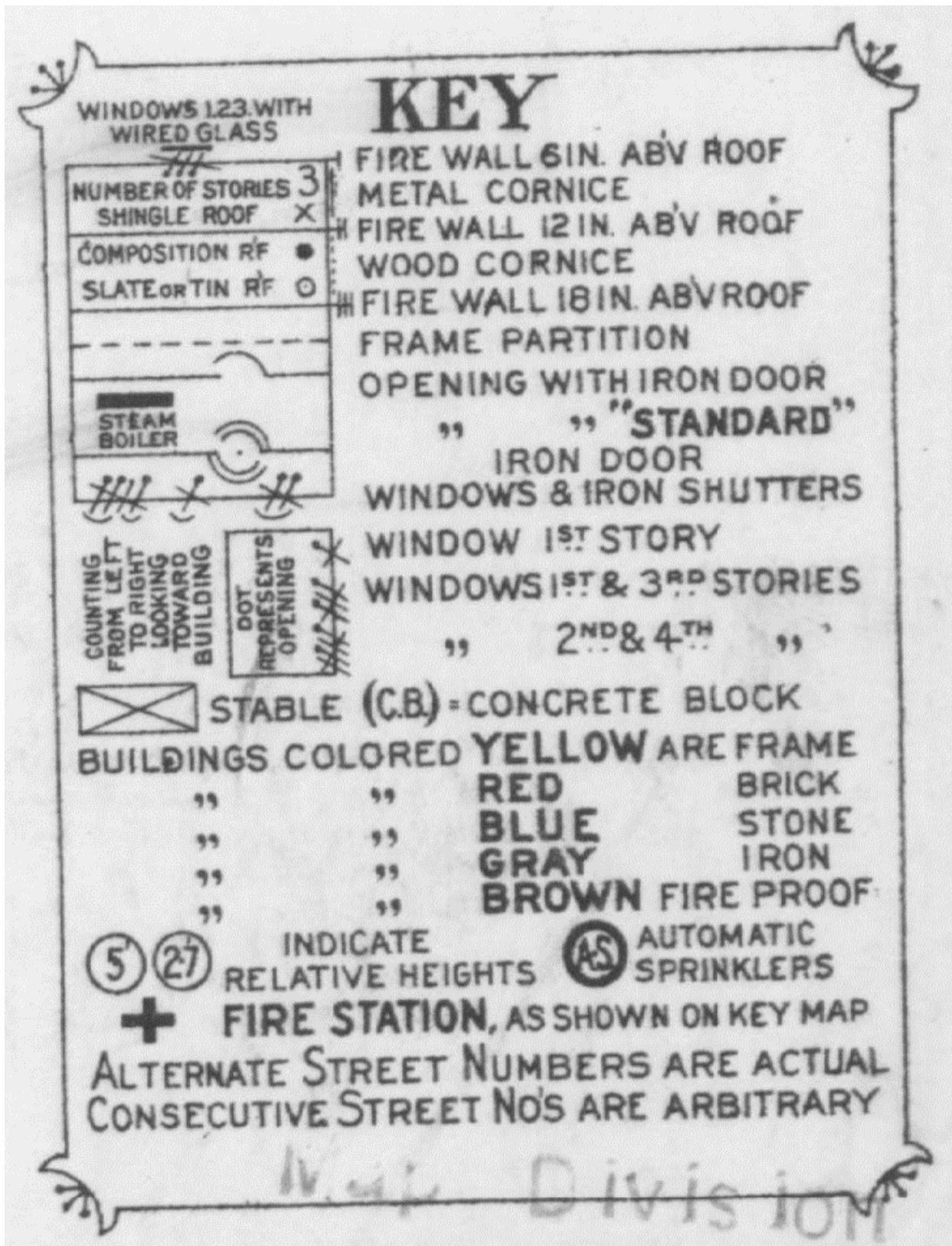


Figure 94. Map Legend for February 1920.³⁹¹

Appendix B.
 “Charleston, Cincinnati and Chicago Railroad Company”
 Extract from: Manual of the Railroads of the United States for 1889

Table 81. “Charleston, Cincinnati and Chicago Railroad Company

Lines of Road Projected:		
Charleston and Ohio River Division: Charleston, S.C., to Ashland, Ky.	620.0	
Augusta Division: Blacksburg, S.C., to Augusta, Ga.	140.0	
Charlotte Division: Lancaster, S.C., to Charlotte, N.C.	40.0	
Total length of projected lines		800.0 miles.
Completed December 31, 1888		
Camden, S.C., to Rutherfordton, N.C.	147.5	
Richardson, Ky., to White House, Ky.	8.5	
Total length of lines completed, December 31, 1888		156.0 miles.
Sidings, not reported. Gauge, 4 ft. 8½ in. Rail (steel), 60½ lbs.		

[Note that C.C. &C. had not completed any track in Tennessee at the time of this report.]

History. – This company was organized and chartered September 30, 1886, and succeeded to all the rights of the Charleston, Cincinnati and Chicago RR. Co. of South Carolina, the Rutherfordton Railway Construction Co. of North Carolina, and the Rutherford, Marion and Tennessee Ry. Co. of South Carolina. Work begun September 19, 1885. It is expected that the road will be completed to Marion, 318 miles from Charleston, by July next, thereby forming a connection with the Western North Carolina RR. and all points reached by it.

On the northern end of the line, fifty miles (between Ashland, Ky., and Richardson, Ky.) have been built under a local charter, and eight and one-half miles of additional road have been completed under another local charter to the canal coal. On February 13, 1888, the Legislature of Kentucky granted a charter to the Charleston, Cincinnati and Chicago RR. Co., which allows said road to consolidate with the above roads, or to build an independent line to the Ohio River, as they may deem best. On March 28, 1888, the Ohio, Kentucky and Virginia RR. Co. was consolidated with this company without change of name. During the present year the construction will be pushed southward through Kentucky and Virginia. Work will also be begun at Johnson City, and construction will be pushed northward toward the Ohio River, and southward toward Marion, and from Marion northward, in order that the whole line of road may be completed during the year 1890.

The counties, towns and townships through which the road runs have already voted aid to the amount of about \$1,500,000, payable in their 6 and 7 per cent. bonds in exchange for an equal amount of stock at par. These subscriptions are payable as the road is completed.

The line is being built under contract by the Massachusetts and Southern Construction Co., of Boston, Mass., A.B. Harris, 45 Broadway, New York, N.Y., President. In February, 1889, the Massachusetts and Southern Construction Co. contracted with McDonald, Shea & Co., Knoxville, Tenn., to construct the road from Marion to the town of Minneapolis, Va., its junction with the Clinch Valley Extension of the Norfolk and Western RR., and from that point to the coking coal, making a total distance from Charleston of 575 miles, which is expected to be completed during the year 1889, making a direct communication with the coal to the sea-coast. For further particulars see Manual for 1888. page 573.

Financial Statement. – Capital stock authorized, \$15,000,000; first mortgage bonds authorized, \$20,000,000, of which outstanding, January 1, 1889, \$4,612,500. These bonds are for the uniform rate of \$25,000 per mile of road, the mortgage covering franchises, sidings, stations, terminal facilities, branches, extensions and equipment, acquired and to be acquired. The bonds are dated August 2, 1887 – mature July 1, 1947, and interest at the rate of 5 per cent. is payable quarterly, January, April, July, and October. Trustee, Boston Safe Deposit and Trust Co., Boston, Mass., at whose office interest and principal are payable in gold.

Annual meeting, second Thursday in October, at Charleston, S. C.

Directors (elected October 11, 1888). – Frank Coxe, Wharton Barker, William F. Weld, Philadelphia, Pa.; Chester W. Chapin, P.P. Dickinson, New York, N.Y.; James A. Rumrill, Springfield, Mass.; William D. Bishop, Bridgeport, Conn.; Amos Barnes, James T. Furber, Boston, Mass.; John T. Wilder, Johnson City, Tenn.; W.L. Roddey, Rock Hill, S.C.; Peter J. Sinclair, Marion, N.C.

Frank Coxe, President – Philadelphia, Pa.

John T. Wilder, Vice-President – Johnson City, Tenn.

Sec. and Treas. – Henry K. Baker – Springfield, Mass.

General Manager – R.A. Johnson... New York, N.Y.

Principal Office and Address – 45 Broadway, New York, N.Y.

Corporate Office – Rock Hill, S.C.”³⁹²

Appendix C.

N.L.R.B. v. Empire Furniture Corporation
107 F. (2d) 92 (C.C.A. 6), November 8, 1939 – 10 N.L.R.B. 1026
On petition to enforce Board Order
Before Hicks, Simons, and Allen, Circuit Judges

(93) Simons, C.J.: The National Labor Relations Board seeks, by its petition, enforcement against the respondent of its order of January 7, 1939, directing the reinstatement of three of its employees and the payment of wages to a fourth who does not desire reinstatement, upon findings made by the Board that the respondent had been guilty of unfair labor practices including the discharge of employees for union activities. The cease and desist provisions of the order are the usual ones commanding the respondent to refrain from discouraging membership in labor organizations and coercing employees in the exercise of rights to self-organization, and also command it to bargain collectively with representatives of its employees.

The respondent challenges the validity of the order as in response to findings not based upon substantial evidence. The controversy appears here for the second time. The first order of the Board was challenged by a petition of the respondent to set it aside, filed on May 9, 1938. Subsequently, on the Board's petition, the order was remanded to it for further consideration in accord with familiar precedent (*Ford Motor Co. v. N.L.R.B.*, 305 U.S. 364, 59 S. Ct. 301, 83 L. Ed. 221); was thereafter vacated and supplemental proceedings led to the present decision. The Board petitions for enforcement of its latest order, there is no petition by the respondent to set it aside.

The respondent is engaged in the manufacture of furniture at Johnson City, Tennessee. It receives the major part of its raw materials from without the State through instrumentalities of interstate commerce, and ships much of its finished product into other States. While some question of jurisdiction was in the beginning suggested, attack upon its exercise is no longer pressed, and we conclude that the Board appropriately exercised jurisdiction.

In view of the issue and findings it seems important to consider the origin of the respondent's enterprise. It is successor to the Empire Chair Company which was liquidated in the bankruptcy court, and its plant surrendered to holders of defaulted bonds. For three years the plant was idle contributing to the distress of the unemployed in Johnson City. Through the activity of one Shumate, a retired banker and at present Secretary-Treasurer of the respondent, a new enterprise was started in the old plant through cooperation of private enterprise with community effort. The Johnson City Chamber of Commerce and the municipality gave substantial aid to it, in order to ease the local relief load; the Chamber of Commerce loaning the new corporation \$17,500 to help it meet conditions for the approval of a loan by the Reconstruction Finance Corporation of \$175,000, and the City aiding by remitting back taxes and lowering current assessments.

The respondent began operations in June of 1935. Up to the time the present labor controversy began it had made substantial progress, exhibiting its product at the markets in Chicago and in New York, but manufacturing principally only as orders were received. Its R. F. C. loan has been only slightly reduced. It employs a maximum of 350 men, and up to April, 1937, its relations with its employees appear to have been unattended with any difficulty or complaint on their part. Late in April one Barnett, not long in respondent's employ, communicated with the Textile Workers' Organizing Committee, an affiliate of the Committee for Industrial Organization, and requested that the employees of the plant be organized into a union. A

representative came to Johnson City and began the work of organization. It proceeded rapidly, – it is contended that approximately 300 men signed union cards.

It appears from the record that several of the respondent's foremen were hostile to unionization. While these foremen were entrusted with complete responsibility for hiring and discharging men, it by no means follows from this circumstance that an irrebuttable or even a reasonable inference arises that a lay-off or discharge was for union activity when the annual labor turn-over was 200 and there is cumulative evidence contra. We must search the record for something more substantial and definite to sustain the findings.

Our first question is whether the record substantially supports a finding by the Board that the respondent refused to agree to bargain collectively with the union if it were designated as bargaining agency of the employees in an election. The Board concluded that the respondent was full apprised from the beginning of the activity among its employees toward self-organization; that its foremen denounced the union; threatened its members with the loss of jobs; that its officer Shumate declared an intention to close the plant if the union succeeded in organizing; that the respondent declined to recognize the union as representative without an election and expressly refused to bargain with it if designated at an election.

We explore the record for support. Winebarger, a foreman, had expressed to an employee the view that unions had never done anything to help him; that strikes had resulted from the activities of organizers in which men had been killed, and that he had seen where the C.I.O. had caused much trouble; and Leonard, another foreman, had given it as his opinion that the C.I.O. were a 'a bunch of communists fit to stir up trouble'. There is no evidence that these observations reflected the view of respondent's management, that the foremen were expected to, intended, or did permit their personal views upon organization to influence them in the hiring or discharging of employees. We fail to find evidence of Shumate's declared intention to close the plant if the union succeeded in organizing. The testimony relied upon for this finding is that of the employee Morrison who was warned by Shumate that he must not solicit union members on company property or abuse those who failed to respond. He testified that Shumate said to him 'When it comes to where we can't do anything, we will lock the gate and leave out of here.' While this statement was vigorously and categorically denied, nevertheless, if it was made it fails to support the finding. When industry for whatever cause can no longer function, the locking of the gate is beyond the exercise of volition on the part of management.

The record supports the finding that the respondent declined to recognize the union as representative of its employees without an election. We do not understand this to be a condemned labor practice when the management desires reasonable proof of majority unionization and its selection of a bargaining agency. The executive officers requested proof that those who professed to represent the men had authority so to do. They offered to produce, though they did not produce, membership cards. This would have told the respondent nothing since it had no record of employee signatures with which to compare the cards. The respondent welcomed an election. Its repeated request for an election is not controverted. It wished to have it held forthwith but the union representatives urged upon it the necessity of time within which to perfect necessary machinery for holding it.

Finally the Board finds that the respondent refused to agree to bargain with the union even if at an election it were designated as the employee bargaining agency. This finding not only flies in the face of evidence produced by the respondent through witnesses whose credibility is not impeached, but is in direct conflict with the evidence of its own representative. Virgil C. Finch, field examiner for the Board, was asked, 'Q. Now, Mr. Finch, didn't Mr. Shumate and Mr. Gordon

both tell you that if the election was held and it showed that the union had a majority that they would be glad to sit down and discuss the matter around a table with a union representative? A. Probably they did, I think they did, yes.'

We understand the binding effect of the fact findings of the National Labor Relations Board, when such findings are supported by substantial evidence. We are (95) not bound by findings not so supported. *Washington, Virginia & Maryland Coach Company v. N.L.R.B.*, 301 U.S. 142, 57 S. Ct. 648, 81 L. Ed. 965; *Appalachian Elec. P. Co. v. N.L.R.B.*, 4 Cir., 93 F.2d 985. Substantial evidence is such relevant evidence as a reasonable mind may accept as adequate to support a conclusion. *Consolidated Edison Co. v. N.L.R.B.*, 2 Cir., 95 F.2d 390, affirmed, 305 U.S. 197, 59 S. Ct. 206, 83 L. Ed. 126. In a recent case involving the validity of an order of the Board, *N.L.R.B. v. Thompson Products*, 6 Cir., 97 F.2d 13, we said, 'The rule of substantial evidence is one of fundamental importance and is the dividing line between law and arbitrary power.' We adhere to that view and other Circuits give approval thereto. *Cupples Co. Manufacturers v. N.L.R.B.*, 8 Cir., 106 F.2d 100. There is no basis for the finding that the respondent would refuse to bargain with the representatives of a union certified to it as the bargaining agency of its employees as the result, of an election.

The men who were ordered to be reinstated were Buchanan, a hand sander, Barnett and Hicks, while the respondent was ordered to pay Morrison a minimum amount of wages. Buchanan was laid off; he was a member of the union and had solicited others to join. There was nothing unusual in this laying off of Buchanan. His work was that of a common laborer and when there was not sufficient work the number of sanders was from time to time reduced. The Board draws the inference first that Buchanan was not laid off but discharged, and second that his discharge was for union activity, from the fact that there was a practice at the plant to pay off discharged men immediately, but to pay off those who, of their own accord leave, at the next pay day. It ignores, however, the evidence that Buchanan demanded his pay at once and it was for that reason given to him. By building one inference upon another, and by the simple expedient of rejecting controverting evidence destructive of both as not entitled to credence even though unimpeached, the Board arrived at its finding. That Buchanan was discharged and for union activity is further sought to be supported by his hearsay statement that after his discharge two additional sanders were immediately employed. To recite this evidence is sufficiently to characterize it. Buchanan testified, 'the day after I was laid off they put two more men on'. 'Q. Are you sure of that? A. Lonnie Honeycutt said he would take the stand on it'. Honeycutt was not produced and there was no explanation of his absence. We understand fully that the Board is not bound strictly by technical rules of evidence. We do not understand that this is a caveat to arbitrarily substitute surmise, suspicion and guess for proof.

Barnett was engaged in assembling table bases by means of an instrument called a 'horse clamp'. The method proved unsatisfactory and was discarded. Other men were assigned to assembling by a new method, Barnett being meanwhile laid off. He concluded that he was discharged and the next day called for his tools and drew his pay. His conclusion that he was discharged is not otherwise supported and there is no evidence associating the discharge, if he was discharged, to union activities. Bernie Morrison was likewise laid off because there was no further work. There is credible evidence not impeached that Morrison's work had fallen off greatly in quality and it also appears that he secreted certain patterns. The only evidence that could possibly connect his discharge to union activity is solicitation of members during working hours, and abuse of fellow workers. If so connected his discharge was not in pursuance of an unfair labor practice. We have recently pointed out, *N.L.R.B. v. Sands Mfg. Co.*, 6 Cir., 96 F.2d 721, that the statute

does not interfere with the employer's right to discharge for violation of contract and that discharge is not an unfair labor practice merely because the discharged employee is a member of a union.

Hicks was discharged for violating a company rule against running to the time clock. A serious accident to an employee resulting from this practice and a demand by the company carrying group insurance that it cease, led to the adoption of the rule. While compliance may not always have been vigilantly enforced, the rule existed and there is cumulative evidence not only from his superiors but from his coworkers that Hicks violated the rule and so incurred the penalty of discharge threatened for its violation. There is no support for the finding of the Board that (96) the discharge of Hicks was due to union activities.

The petition of the board for enforcement of its order must be denied because its findings of unfair labor practices are unsupported by substantial evidence. Sensible of the great social purpose of the National Labor Relations Act, 29 U.S.C.A. § 151 et seq., courts have gone far to uphold rulings of the administrative agency charged with its enforcement, doubtless in the belief that over-zealousness must in time yield to expertness in weighing evidence and that time and responsibility must develop a judicial approach to disputed issues in a tribunal which, though administrative, exercises to such large extent the high judicial function. It may not be amiss-indeed, it may be in the highest public interest to observe that the beneficent purposes of the Act will not, be effectuated by decisions such as that presently reviewed.

Petition denied.³⁹³

Endnotes

¹ The Blue Ridge Stemwinder, by John R. Waite and Chris H. Ford, 2003, pages 76 – 77. An interesting overview of the history of the Carnegie Addition is available at: “Carnegie, Tennessee,” by Robbie D. Jones, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), pages 3 – 9.

² The Blue Ridge Stemwinder, by John R. Waite and Chris H. Ford, 2003, pages 76 – 77.

³ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” January 1891, image 5, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891, which included: “No watchman, two men on night duty. Blake F-Pump No. 8 supplied from creek & cistern. 500’ 2” hose. Open kerosene lights. Heat, steam. Fuel, spent tan”.

⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” May 1897, image 5, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897, which includes: “No watchman. 2 men on night duty. Heat: Steam. Fuel: spent tan & coal. Lights: elec. Blake F.P. No. 8 supplied from creek. 700’ 2” hose. 500’ 2½” hose. Few fire pails. City water.”

⁵ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 10, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903, which includes: “(Corrections made from outside of Bldg.) Night & Sunday watchman. Elec. Clock. 8 sta’s. Hourly rounds. Lights: elec. Heat: steam. Fuel: refuse & tan bark. City water. 2” & 2½” D. H’s. scattered around plant. 700’ 2” hose. 500’ 2½” hose. A few fire pails dist’d.”

⁶ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” December 1908, image 17, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908, which included: “Admittance Refused. Night and Sunday watchman. Electric clock. 8 stations. Hourly rounds. Fuel refuse and tan bark. Heat: steam. Lights: electric. City water. 2” and 2½” double hydrants scattered all around plant. 700’ 2½” hose. 500’ 2½” hose. A few fire pails distrib’d.”

⁷ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” July 1913, image 20, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913, which includes: “Night & Sunday watchman: Nanz clock 12 stations. Hourly rounds. Fuel: spent tan & refuse. Heat: steam. Lights: elec. 10 2½” & 10 2” hyds. In yard fed by 6” city main. 2 Blake pumps drawing from creek also supply fire lines. 1800’ 2½” hose attd. To hyds. & on carts. V.P. in finishing ho. with 50’ 2” hose attd. Ea. Fl. casks & pails distrib’s. thr’out permanent ladders to roofs.”

⁸ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920, which included: “2 night & Sunday watchmen, 1 for tannery & 1 for extract plant. Newman clock, 11 stations, hourly rounds. City water supply & private water system from Brush Creek by force pump cap’c’y. 500 Gals. Per Min. to 8,000 Gal. gravity tank on steel trestle 50’ high. Outside 2½” D. hyds. With 200’ 2½” hose as shown. 3 hand carts & 300’ 2½” hose to each & chemical fire ext’g’rs. Dist’d. Organized fire co. amongst employees (10 men) with periodical drills. Total length of 2½” hose about 1,250’ automatic sprinklers to Hair Ho. & Leach Ho. under roofs only. Heat: steam. Lights: elec. (I.E.P.) Power: steam & elec.”

⁹ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), page 6.

¹⁰ “Horton, Yokum, and Company Tannery (Watauga Tannery), Established November 1883 at Water and Division Streets,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/20667>. See also “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” January 1891, image 5, U.S. Library of Congress, available online at:

http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891. The plant shown on the 1891 map is more developed (has more buildings), than are shown in this photograph.

¹¹ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), page 4.

¹² “Old Tannery Bridge and Gildersleeves Home, Woman Posing With Bicycles,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11695>.

¹³ Image from: “View of a Section of Johnson City”, Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11699>. Caption from: “View of Johnson City, looking north from Tannery Knob (hill above current city hall),” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/193>.

¹⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” January 1891, image 5, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891

¹⁵ The Packages, August 1904 (Volume VII, No. 8), page 42, available online at: <https://books.google.com/books?id=7QNZAAAAYAAJ>.

¹⁶ “Lumber Yards,” Archives of Appalachia, Clifford A. Maxwell Photographs, 1940 – 1986, available online at: <https://archivesofappalachia.omeka.net/items/show/12088>.

¹⁷ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), pages 3 – 12.

¹⁸ A History of the New York Swamp, by Frank Wayland Norcross, 1901, page 78, available online at: <https://books.google.com/books?id=p6ELAQAIAAJ>.

¹⁹ Unless otherwise noted, all newspaper articles in this documentary history were accessed from “Chronicling America, Historic American Newspapers,” U.S. Library of Congress, available online at: <https://chroniclingamerica.loc.gov/>.

²⁰ A History of the City of Newark, New Jersey: Embracing Practically Two and a Half Centuries, Volume III, 1913, pages 206 – 207, available online at: <https://books.google.com/books?id=1IsGcdS77jIC>, included: “John H. Yocum, the well-known authority on the manufacture and tanning of leather goods, was born in 1870, at Ashland, Pennsylvania, the son of Seth H. and Lucinda (Horton) Yocum. The first ten years of his life was spent at Bellefonte, Pennsylvania, but in 1880 his family removed to Johnson City, Tennessee, and it was there that he received his early education and later began his business career under his father, who was a member of Horton, Yocum & Company. From the outset the scientific side of the business interested the young man, and after a few years spent in his father's firm, he decided to enter the Pennsylvania State College with a view to studying chemistry and chemical processes in relation to the preparation and tanning of leather goods. He was graduated in 1891, and immediately re-entered Horton, Yocum & Company as chemist. He remained with his father's firm until 1893, when a fine offer by the United Leather Company called him to New York as chief chemist for that concern. For six years he remained in this position to the entire satisfaction of his employers, but in the summer of 1899, having received the appointment of superintendent of the works of T.P. Howell & Company, tanners and manufacturers of patent leathers, of Newark, New Jersey, he decided to leave New York and accept that position. Being by this time master of both the commercial and scientific requirements of the tanning business, he determined to establish a plant of his own, and in 1900 the Yocum Manufacturing Company and laboratory were started. Becoming more and more interested in the scientific side of the leather business, Mr. Yocum finally decided to relinquish the active control of his company and sold out his holdings to J.H. Ladew & Company in 1904, remaining with them, however, in the capacity of general manager and chemist. In 1906 he was made vice-president and general manager of the Harvey Company, a position which he filled until January, 1912, when he retired permanently from both the Harvey and the Ladew firm to devote himself entirely to practice as a chemical engineer and laboratory expert. He is now at the head of a commercial tanning laboratory, one of the largest in the East. In spite of the many professional and business demands upon his time and interest, Mr. Yocum finds leisure to belong to several fraternal and social societies. He is a Knight Templar, and a noble of the

Ancient Arabic Order of the Nobles of the Mystic Shrine, and is also a member of the Essex County Country Club, the North End Club and the Union Chemist Club of New York City. In October, 1900, Mr. Yocum married Florence Knapp and they have one daughter, Margaret, who attends the public school of East Orange.” He died before 5 February 1921.

²¹ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), pages 4 – 6.

²² Shoe and Leather Reporter, 1 January 1891 (Volume LI, No. 1), page 56, available online at: <https://books.google.com/books?id=mE-AQAAMAAJ>.

²³ Shoe and Leather Reporter, 16 April 1891 (Volume 51), page 974, available online at: <https://books.google.com/books?id=mE-AQAAMAAJ>.

²⁴ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), page 4.

²⁵ The National Provisioner, 2 September 1905 (Volume XXXIII, No. 10), page 17, available online at: <https://books.google.com/books?id=Eh1LAQAAMAAJ>.

²⁶ Poor’s Manual of Industrials, 1913 (Fourth Annual Number), page 530, available online at: <https://books.google.com/books?id=9GY3AQAAMAAJ>.

²⁷ The National Provisioner, 23 April 1910 (Volume 42, No. 17), page 18, available online at: <https://books.google.com/books?id=G TmAAAAMAAJ>.

²⁸ Manufacturers Record, 13 September 1913 (Volume LXIV, No. 11), page 60, available online at: <https://books.google.com/books?id=rE72zhMxGPOC>.

²⁹ Hide and Leather, 26 June 1915 (Volume 49, No. 26), page 25, available online at: <https://books.google.com/books?id=GqE7AQAAMAAJ>.

³⁰ The Iron Age, 1 July 1915 (Volume 96, No. 1), page 64, available online at: <https://books.google.com/books?id=uAY-AQAAMAAJ>.

³¹ Hide and Leather, 4 September 1915 (Volume L, No. 10), page 40, available online at: <https://books.google.com/books?id=eKA7AQAAMAAJ>.

³² Hide and Leather, 13 November 1915 (Volume L, No. 20), page 23, available online at: <https://books.google.com/books?id=eKA7AQAAMAAJ>.

³³ Sixth Annual Report – Carolina, Clinchfield and Ohio Railway – For the Fiscal Year ended June 30, 1916, page 33, available online at: <https://books.google.com/books?id=vMk7AQAAMAAJ>.

³⁴ Shoe and Leather Facts, September 1919, page 47, available online at: <https://books.google.com/books?id=g9I-AQAAMAAJ>.

³⁵ Shoe and Leather Reporter, 25 May 1922 (Volume 146, No. 8), page 32, available online at: <https://books.google.com/books?id=J8ytD0Ti4dwC>.

³⁶ Moody’s Manual of Investments and Security Rating Service, 1926, page 1038, available online at: <https://books.google.com/books?id=wH-0g8SLXWEC>. See also Moody’s Manual of Investments and Security Rating Service, 1922, page 199, available online at: <https://books.google.com/books?id=MVJWAAAAYAAJ>.

³⁷ Railway and Industrial Compendium, Railway Number, 25 May 1928, page 142, available online at: https://books.google.com/books?id=LSmck_yzLEcC.

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- ³⁸ Industrial Employment Information Bulletin, July 1931 (Volume XI, No. 7), page 31, available online at: https://books.google.com/books?id=ljGDKbimh_IC.
- ³⁹ “The Tannery at Johnson City,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., January/February/March 1997 (Volume 9, Number 3), page 12.
- ⁴⁰ The Buccaneer, 1941, Archives of Appalachia, East Tennessee State University, page 69, available online at: <https://dc.etsu.edu/yearbooks/24>.
- ⁴¹ The Buccaneer, 1946, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/29>. Similar advertisements appeared in The Buccaneer from 1947 to 1951, available online at: <https://dc.etsu.edu/buccaneer/>.
- ⁴² The Buccaneer, 1964, Archives of Appalachia, East Tennessee State University, page 340, available online at: <https://dc.etsu.edu/yearbooks/47>.
- ⁴³ The Buccaneer, 1965, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/48>.
- ⁴⁴ The Buccaneer, 1969, Archives of Appalachia, East Tennessee State University, page 331, available online at: <https://dc.etsu.edu/yearbooks/50>.
- ⁴⁵ Fourth Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning December 1, 1915, and Ending December 1, 1916, State of Tennessee, page 45, available online at: <https://hdl.handle.net/2027/uc1.b3030564>.
- ⁴⁶ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.
- ⁴⁷ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.
- ⁴⁸ Bureau of Workshop and Factory Inspection, Seventh Annual Report, January 1st, 1919 to December 31st, 1919, State of Tennessee, pages 27 and 30, available online at: <https://hdl.handle.net/2027/uc1.b3030567>.
- ⁴⁹ Bureau of Workshop and Factory Inspection, Eighth Annual Report, January 1st, 1920 to December 31st, 1920, State of Tennessee, pages 38 – 39, available online at: <https://hdl.handle.net/2027/uc1.b3030568>.
- ⁵⁰ Bureau of Workshop and Factory Inspection, Ninth Annual Report, January 1st, 1921 to December 31st, 1921, State of Tennessee, page 47, available online at: <https://hdl.handle.net/2027/uc1.b3030570>.
- ⁵¹ Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.
- ⁵² Third Annual Report, Department of Labor, State of Tennessee, 25 February 1926, page 68, available online at: <https://hdl.handle.net/2027/umn.31951d03021441k>. The 1923 edition of this report is available online, but only contains a summary of the data at the county level. The author did not find a copy of the 1924 edition of this report online.
- ⁵³ Fourth Annual Report, Department of Labor, State of Tennessee, 25 February 1927, page 73, available online at: <https://hdl.handle.net/2027/uc1.b2991541>.
- ⁵⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 11, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903, which included: “Biddle & Ellsworth Flour Mill. No watchman. Lights: closed lant’s. Heat: steam. Fuel: coal. City water. Water bbl’s

& buckets on ea. Fl. Machinery. Bas't: 2 cleaning mach's. 1 scourer, 1 Separator, 1 Bran duster. 1st: 6. D'b'le sets rollers. 1 Run stone, 1 Flour packer. 2d. 1 [Run stone], 1 Rolling Mach., 1 Purifier. 3d. 1 Scalping Mach., 13 reels." "L. McCollum M'F'G. Co. to be Sproles M'F'G Co. planing Mill. Night & Saturday watchman. Clock, stations. Hourly rounds. No lights. Heat: steam in Off. only. Fuel: coal & shavings. City water. 2" hyd & 100' 2" hose in eng. Rm. 5 bbls & 10 buckets distributed."

⁵⁵ "Workmen in Johnson City, Tenn., c. 1920," James Agee Film Project Photographs, 1875 – 1956, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/768>.

⁵⁶ Johnson City, by L. Thomas Roberts, 2018, page 68.

⁵⁷ Manufacturers Record, 24 September 1903 (Volume XLIV, No. 16), page 190, available online at: <https://books.google.com/books?id=nnI9AQAAAJ>.

⁵⁸ Packages, February 1907 (Volume X, No. 2), page 62, available online at: https://books.google.com/books?id=N_9YAAAAAJ.

⁵⁹ American Lumberman, 23 February 1907 (Whole Number 1657), page 77, available online at: <https://books.google.com/books?id=XbGGQ38WXIQC>.

⁶⁰ The Insurance Field, 27 August 1908 (Volume XVIII, No. 9), page 13, available online at: <https://books.google.com/books?id=1Ec1AQAAAJ>.

⁶¹ The Lumber Trade Journal, 1 September 1908 (Volume 54, No. 5), page 24, available online at: <https://books.google.com/books?id=oOoxAQAAAJ>.

⁶² The Lumber Trade Journal, 1 September 1908 (Volume 54, No. 5), page 24, available online at: <https://books.google.com/books?id=oOoxAQAAAJ>.

⁶³ "Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee," January 1891, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891. Note, image 1 indicates that this building was on the southeast corner of 1st Avenue and New Street, in the Carnegie Addition.

⁶⁴ "Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee," May 1897, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897, which included: "No watchman. Heat & Power: steam. Fuel: coal. Lights: closed lanterns. Fire pails & bbls on each fl. Hyd. 50' 1" hose in eng. Rm. City water. Bas. 2 cleaners. 1 branduster. 1 scourer. 1 separator. 1st. a run stone. 6 rolls. 1 packer. 2d. 1 run stone. 1 roller. 1 purifier. 3d. 15 reels. 1 scalper."

⁶⁵ "Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee," February 1920, image 36, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920.

⁶⁶ Biographical and Portrait Cyclopeda of Blair County, Pennsylvania, edited by Samuel T. Wiley, Winfield Scott Garner, 1892, pages 484 – 485, available online at: <https://books.google.com/books?id=RtEwAQAAAJ>.

⁶⁷ The Weekly Northwestern Miller, 17 December 1897 (Volume 44, Number 25), page 986, available online at: <https://books.google.com/books?id=MmtZAAAAAJ>.

⁶⁸ The Modern Miller, 19 March 1898 (Volume 23, Number 41), page 14, available online at: <https://books.google.com/books?id=OX4AAAAAJ>.

⁶⁹ The Weekly Northwestern Miller, 25 November 1898 (Volume XLVI), page 913, available online at: <https://books.google.com/books?id=LHsxAQAAAJ>.

⁷⁰ The Southern Architect, December 1898 (Volume X, No. 2), page 616, available online at: <https://books.google.com/books?id=UKv-GBYUYD8C>.

⁷¹ The Weekly Northwestern Miller, 12 December 1898 (Volume XLVI), page 1046, available online at: <https://books.google.com/books?id=LHsxAQAAMAAJ>.

⁷² The Weekly Northwestern Miller, 8 October 1897 (Volume 44, Number 15), page 552, available online at: <https://books.google.com/books?id=MmtZAAAAYAAJ>.

⁷³ The Northwestern Miller, 8 February 1899 (Volume 47, Number 6), page 231, available online at: <https://books.google.com/books?id=pnBDAQAAMAAJ>, which included: “Johnson City, Tenn., Dec. 31, 1898. The Prinz & Rau Mfg. Co., Milwaukee, Wis. Gentlemen: Answering your favor of the 28th, we are pleased to say that the Iron Prince Scourer that we put in last summer meets every expectation, so far as cleaning the wheat goes. In fact, it surpasses what we really expected, especially in cleaning smutty wheat, and is the only machine that we have ever seen that really does clean this class of wheat so that it is in grinding condition. We have used nearly every cleaning machine on the market, and are free to say that it is as far ahead of any of them as they are ahead of the old style smutters in use thirty years ago. There is no other machine in the same class and there is no machine that we have ever used in a mill that we can recommend as cheerfully as we do this one. The machine is well made, and, from appearances, should be a durable one. It is a very light running machine, considering the quality of work done; in fact, we think it is lighter running than the average close scourer. It does not break wheat, and it is only in this respect that we have been disappointed, and, of course, agreeably so. We had been told many times that it broke so much wheat that we had serious doubts of it in this respect. We don’t know how we could do any miller any greater favor than to recommend him put in an Iron Prince Scourer. With best wishes and the compliments of the season, we remain, yours truly, (signed) Biddle & Ellsworth, Ltd.”

⁷⁴ American Miller, 1 December 1904 (Volume XXXII, No. 12), page 1017, available online at: <https://books.google.com/books?id=tzpWv7Qd1TwC>.

⁷⁵ Manufacturers Record, 28 September 1905 (Volume XLVIII, No. 12), page 274, available online at: <https://books.google.com/books?id=lWE9AQAAMAAJ>.

⁷⁶ Manufacturers Record, 5 October 1905 (Volume XLVIII, No. 13), page 299, available online at: <https://books.google.com/books?id=lWE9AQAAMAAJ>.

⁷⁷ Flour & Feed, October 1905 (Volume 6, No. 5), page 50, available online at: <https://books.google.com/books?id=MxEkISJBZ6kC>.

⁷⁸ Engineering World, October 1905 (Volume II, No. 1), page 191, available online at: <https://books.google.com/books?id=lTnjBIIdswY8C>.

⁷⁹ Industrial Employment Survey Bulletin, January 1922 (Volume II, No. 1), page 19, available online at: <https://books.google.com/books?id=RQwAAAAYAAJ>.

⁸⁰ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 36, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920.

⁸¹ This map included: “Night & Sunday watchman. Eco. Watchman’s clock, 7 stats. Hourly rounds. Power & Lights: electric. Heat: steam. Fuel: coal. Grinnell Auto. Spr’kl’rs, thro’out, wet system. Heads spaced 8’ x 10’ square. Total No. of heads 250. Supplied by city 6” water main, with outside steamer conn. Also chem fire ext’g’rs & 1 outside d.h. with 100’ 2½” hose att’d.”

⁸² “Hosiery Mills,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11672>. Also at “New Street plant, Johnson City Mills,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/199>.

⁸³ “New Street plant, Johnson City Mills,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/95>.

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- ⁸⁴ American Wool and Cotton Reporter, 16 February 1922 (Volume XXXVI, No. 7), page 1156, available online at: https://books.google.com/books?id=D_ZYAAAAYAAJ. The incorporation of the Johnson City Mills was registered in Tennessee on 14 October 1916, see Public Acts of the State of Tennessee Passed by the Sixtieth General Assembly, 1917, page 615, available online at: <https://books.google.com/books?id=ibo4AAAAIAAJ>.
- ⁸⁵ The Buccaneer, 1942, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/25>. A similar advertisement appeared in the 1943 edition of The Buccaneer, available online at: <https://dc.etsu.edu/buccaneer/index.2.html>.
- ⁸⁶ The Buccaneer, 1948, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/31>.
- ⁸⁷ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.
- ⁸⁸ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.
- ⁸⁹ Bureau of Workshop and Factory Inspection, Seventh Annual Report, January 1st, 1919 to December 31st, 1919, State of Tennessee, pages 27 and 30, available online at: <https://hdl.handle.net/2027/uc1.b3030567>.
- ⁹⁰ Bureau of Workshop and Factory Inspection, Ninth Annual Report, January 1st, 1921 to December 31st, 1921, State of Tennessee, page 47, available online at: <https://hdl.handle.net/2027/uc1.b3030570>.
- ⁹¹ Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.
- ⁹² Third Annual Report, Department of Labor, State of Tennessee, 25 February 1926, page 68, available online at: <https://hdl.handle.net/2027/umn.31951d03021441k>. The 1923 edition of this report is available online, but only contains a summary of the data at the county level. The author did not find a copy of the 1924 edition of this report online.
- ⁹³ Sixth Annual Report, Department of Labor, State of Tennessee, 1 April 1929, pages 123 – 124, available online at: <https://hdl.handle.net/2027/uc1.b2991542>. The author did not find a copy of the 1927 edition of this report online.
- ⁹⁴ Seventh Annual Report, Department of Labor, State of Tennessee, 1929, pages 223 – 224, available online at: <https://hdl.handle.net/2027/uc1.b2991543>.
- ⁹⁵ Eighth Annual Report, Department of Labor, State of Tennessee, 1930, pages 194 – 195, available online at: <https://hdl.handle.net/2027/uc1.b2991544>.
- ⁹⁶ Annual Report of the Department of Labor for the Year 1933, State of Tennessee, page 128, available online at: <https://hdl.handle.net/2027/uc1.b2991545>. The author did not find copies of the 1931 and 1932 editions of this report online.
- ⁹⁷ Annual Report of the Department of Labor for the Year 1935, State of Tennessee, pages 130 – 131, available online at: <https://hdl.handle.net/2027/mdp.35112104245289>. The author did not find a copy of the 1934 edition of this report online, nor of any subsequent reports..
- ⁹⁸ This map included: “Night watchman, no clock. Power & Heat: steam. Lights: Electric. Fuel: waste material. City water supply & 2 3-way hyds. With 100’ 2½” hose att’d. t pea. Bbls of water & buckets also chem fire ext’g’rs. dist’s. Shavings conveyed to furn. & shavings vault by blower system.”
- ⁹⁹ “Johnson City, Tenn., industry, c. 1920,” James Agee Film Project Photographs, 1875 – 1956, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/785>.

¹⁰⁰ “Millers Lumber Company, New Street and Southern Railway,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/114>. The has not found any support for the identification of this enterprise as the “Millers Lumber Company”, unless this photograph was taken after the Tennessee Box Company left this facility, which appears to have been after 1950. However, note the very early model automobile in this photograph, which seems to indicate that this photograph was taken in the 1920s.

¹⁰¹ “ET&WNC Interchange,” East Tennessee and Western North Carolina Railroad Historical Society Collection, 1883 – 2005, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/16602>.

¹⁰² Hardwood Record, 25 September (Volume XL, No. 11), page 36, available online at: <https://books.google.com/books?id=VwU3AQAAAJ>.

¹⁰³ Barrel and Box, September 1915 (Volume XX, No. 7), page 33, available online at: <https://books.google.com/books?id=FSBaAAAAAJ>.

¹⁰⁴ Barrel and Box, March 1916 (Volume XXI, No. 1), page 134, available online at: <https://books.google.com/books?id=nhtaAAAAAJ>.

¹⁰⁵ Carolina, Clinchfield and Ohio Railway, Sixth Annual Report for the Fiscal Year Ended June 30, 1916, page 31, available online at: <https://books.google.com/books?id=vMk7AQAAAJ>.

¹⁰⁶ Sixth Annual Report – Carolina, Clinchfield and Ohio Railway – For the Fiscal Year ended June 30, 1916, page 33, available online at: <https://books.google.com/books?id=vMk7AQAAAJ>.

¹⁰⁷ Hardwood Record, 25 July 1918 (Volume XLV, No. 7), page 43, available online at: <https://books.google.com/books?id=Dtc6AQAAAJ>.

¹⁰⁸ American Lumberman, 31 January 1920 (Whole Number 2333), page 104, available online at: <https://books.google.com/books?id=cAFQAAAAAJ>.

¹⁰⁹ Southern Lumberman, 22 April 1922 (Volume 103, No. 1385), page 77, available online at: <https://books.google.com/books?id=NBFQAAAAAJ>.

¹¹⁰ Southern Lumberman’s Directory of American Lumber Consuming Factories, 1922, page 833, available online at: <https://books.google.com/books?id=f6MeAAAAAJ>.

¹¹¹ Packages, June 1926 (Volume XXIX, No. 6), pages 13, 14, available online at: <https://books.google.com/books?id=iv4cAQAAAJ>.

¹¹² Packages, July 1926 (Volume XXIX, No. 7), pages 18, available online at: <https://books.google.com/books?id=iv4cAQAAAJ>.

¹¹³ Packages, August 1926 (Volume XXIX, No. 8), page 13, available online at: <https://books.google.com/books?id=iv4cAQAAAJ>.

¹¹⁴ Packages, December 1926 (Volume XXIX, No. 12), page 20, available online at: <https://books.google.com/books?id=iv4cAQAAAJ>.

¹¹⁵ The Packages, February 1928 (Volume XXXI, No. 2), page 24, available online at: <https://books.google.com/books?id=VAAdAQAAAJ>. See also: American Lumberman, 18 February 1928 (Whole Number 2753), page 98, available online at: <https://books.google.com/books?id=wbwcAQAAAJ>, which included: “Tennessee, Johnson City – Tennessee Box Co. installing additional machinery in plant of the Von Cannon Lumber Co., recently purchased, and will double its capacity; will manufacture boxes.” The Packages, March 1928 (Volume 31), page 18, which included: “The Tennessee Box Co. has started the installation of box machinery in the plant of

the Von Cannon Lumber Co., Johnson City, Tenn., which it recently purchased. The concern will manufacture only boxes in the plant acquired.” Southern Lumberman, 15 July 1931 (Volume 144, No. 1807), page 34, available online at: <https://hdl.handle.net/2027/mdp.39015082994420>, which included: “Planing Mill Fire. Johnson City, Tenn., July 3. – The Von Cannon Lumber Company suffered the loss of its planing mill and several hundred thousand feet of lumber by fire Thursday. It is thought that the fire originated from lightning. The damage is estimated at \$50,000, partly covered by insurance.” Perhaps this report applies to a different plant or the Tennessee Box Company had leased this property.

¹¹⁶ Manufacturers Record, 1932 (Volume 69), page 7.

¹¹⁷ Fourth Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning December 1, 1915, and Ending December 1, 1916, State of Tennessee, page 45, available online at: <https://hdl.handle.net/2027/uc1.b3030564>.

¹¹⁸ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.

¹¹⁹ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.

¹²⁰ Bureau of Workshop and Factory Inspection, Seventh Annual Report, January 1st, 1919 to December 31st, 1919, State of Tennessee, pages 27 and 30, available online at: <https://hdl.handle.net/2027/uc1.b3030567>.

¹²¹ Bureau of Workshop and Factory Inspection, Ninth Annual Report, January 1st, 1921 to December 31st, 1921, State of Tennessee, page 47, available online at: <https://hdl.handle.net/2027/uc1.b3030570>.

¹²² Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.

¹²³ Sixth Annual Report, Department of Labor, State of Tennessee, 1 April 1929, pages 123 – 124, available online at: <https://hdl.handle.net/2027/uc1.b2991542>. The author did not find a copy of the 1927 edition of this report online. For additional information on the Von Cannon plant, see “A Draft Documentary History of Industries along the Railroads through Johnson City, Part 1...” by William C. Schneck, Jr., available online at: <http://www.etwnccrrhs.org/histories.html>.

¹²⁴ Seventh Annual Report, Department of Labor, State of Tennessee, 1929, pages 223 – 224, available online at: <https://hdl.handle.net/2027/uc1.b2991543>.

¹²⁵ Eighth Annual Report, Department of Labor, State of Tennessee, 1930, pages 194 – 195, available online at: <https://hdl.handle.net/2027/uc1.b2991544>.

¹²⁶ Annual Report of the Department of Labor for the Year 1935, State of Tennessee, pages 130 – 131, available online at: <https://hdl.handle.net/2027/mdp.35112104245289>. The author did not find a copy of the 1934 edition of this report online, nor of any subsequent reports..

¹²⁷ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” December 1908, image 19, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908, which included: “Night & Sunday watchman. Clock. Stations. Hourly rounds. Heat: steam in Off. Fuel: coal & shavings. No lights. City water. 2” hyd & 100’ hose in eng. Rm. 5 bbls & 10 buckets distrib’d. Not in operation at time of survey.”

¹²⁸ American Lumberman, 3 November 1900 (Whole Number 1328), page 26, available online at: <https://books.google.com/books?id=Iuc3LPJTXpIC>. The Johnson City Comet, on 13 September 1900, included: “New Citizen. Johnson City is constantly gaining new citizens that any town would be pleased to have. The latest arrival is Dr. L. McCollum and his estimable family from Tiffin, Ohio. They are occupying the Thomas residence on Boon street and Watauga avenue. The doctor has purchased a large tract of timber through Dr. W.J. Sproles, and has

moved here to look after it. He has brought with him some blooded horses and will be a prominent figure at the Fair this fall.”

¹²⁹ The Tradesman, 11 April 1912 (Volume LXVII, No. 15), page 58, available online at: <https://books.google.com/books?id=u8RGLlgDolQC>.

¹³⁰ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” July 1913, image 22, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913, which included: “Night watchman. Newman Clock. 9 stations. Hourly rounds. Lights: Elec. I.E.P. No heat. Fuel: Coal & waste. 3 D.H’d in yard & 200’ 2½” hose. Blowers to machines. Steam jet in shavings vault. Casks & pails thro’out mill. Boiler rm is all brick & iron.”

¹³¹ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 36, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920, which included: “Night & Sunday watchman. – Newman’s clock. – 9 stations, hourly rounds. – Heat: Steam – Power: Steam & Electric. – Lights: Electric, I.E.P. – Fuel: Coal & waste. – International Auto. Spr’kl’r. System. – (Dry) thro’out, (except office & Bl. Sm.). Heads spaced 8’ x 10’ square. – Total number of heads, about 630. – Supplied by 40,000 gal. gravity tk. elev’d 60’ abv. Sprinkler heads also city conn. To 3 D.H.’s & 2 T.H.’s in yard with 100’ 2½” hose at each. Bbls. of water & fire pails dist’d. thro’out. Bldgs.. – Steam jet in shavings vault. – Shavings conveyed to furnace & shavings vault by blower system. – Boiler Rm. is brick & iron constr’n.”

¹³² “Southern Bending Company,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11675>, annotated “Southern Bending Company located in the Carnegie Section.” Also at “Southern Bending Company (located in the Carnegie district), c. 1920,” James Agee Film Project Photographs, 1875 – 1956, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/632> and “Southern Bending Company; Southern and Carolina, Clinchfield and Ohio Railways; opposite Main Street, Carnegie,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, accessed August 27, 2023, <https://archivesofappalachia.omeka.net/items/show/210>.

¹³³ “Southern Bending Company; Southern and Carolina, Clinchfield and Ohio Railways,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/115>.

¹³⁴ “Lumber yard,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/250>.

¹³⁵ “Johnson City Industrial View,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11770>.

¹³⁶ Manufacturers Record, 9 May 1912 (Volume LXI, No. 18), page 75, available online at: <https://books.google.com/books?id=NHM9AQAAMAAJ>.

¹³⁷ Fibre and Fabric, 6 July 1912 (Volume 56, No. 1426), page 13, available online at: <https://books.google.com/books?id=8Tg8AQAAMAAJ>.

¹³⁸ American Machinist, 30 May 1912 (Volume 36, No. 22), page 78, available online at: <https://books.google.com/books?id=mLBLAQAAIAAJ>.

¹³⁹ Hardwood Record, 10 January 1915 (Volume XXXIX, No. 6), page 38, available online at: <https://books.google.com/books?id=VwU3AQAAMAAJ>.

¹⁴⁰ Wooden and Willow-ware Trade Review, 15 January 1915 (Volume XLVI, No. 9), page 72, available online at: <https://books.google.com/books?id=yShRAAAAYAAJ>, a portion of the image available online was illegible.

¹⁴¹ American Lumberman, 23 January 1915 (Whole Number 2071), page 76, available online at: <https://books.google.com/books?id=eAwkgqonTkQC>.

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- ¹⁴² Mill Supplies, January 1915 (Volume V, No. 1), page 67, available online at:
<https://books.google.com/books?id=3wcgAQAAMAAJ>.
- ¹⁴³ The Hub, January 1915 (Volume LVI, No. 10), page 36, available online at:
<https://books.google.com/books?id=6nzmAAAAMAAJ>.
- ¹⁴⁴ American Lumberman, 20 February 1915 (Whole Number 2075), page 71, available online at:
<https://books.google.com/books?id=eAwkgqonTkQC>.
- ¹⁴⁵ The Mississippi Valley Lumberman, 5 March 1915 (Volume 46, No. 10), page 37, available online at:
<https://books.google.com/books?id=neMwAQAAMAAJ>.
- ¹⁴⁶ Hardwood Record, 10 July 1915 (Volume XL, No. 6), page 35, available online at:
<https://books.google.com/books?id=VwU3AQAAMAAJ>.
- ¹⁴⁷ Hardwood Record, 25 September (Volume XL, No. 11), page 36, available online at:
<https://books.google.com/books?id=VwU3AQAAMAAJ>.
- ¹⁴⁸ Mill Supplies, October 1915 (Volume V, No. 10), page 59, available online at:
<https://books.google.com/books?id=3wcgAQAAMAAJ>.
- ¹⁴⁹ The Spokesman, October 1915 (Volume XXXI, No. 10), page 468, available online at:
<https://books.google.com/books?id=kFo-AQAAMAAJ>.
- ¹⁵⁰ Lumber World Review, 10 May 1916 (Volume XXX, No. 9), page 48, available online at:
https://books.google.com/books?id=OEE_AQAAMAAJ.
- ¹⁵¹ The St. Louis Lumberman, 1 June 1916 (Volume LVII, No. 11), page 78, available online at:
<https://books.google.com/books?id=zPwxAQAAMAAJ>.
- ¹⁵² Automobile Topics, 17 August 1918 (Volume LI, No. 2), page 168, available online at:
<https://books.google.com/books?id=6B5aAAAAAYAAJ>.
- ¹⁵³ American Machinist, 24 October 1918 (Volume 49, No. 17), page 96c, available online at:
<https://books.google.com/books?id=5I1MAAAAYAAJ>.
- ¹⁵⁴ Packages, November 1918 (Volume XXI, No. 11), page 33, available online at:
<https://books.google.com/books?id=OglZAAAAAYAAJ>.
- ¹⁵⁵ Textile World Journal, 4 October 1919 (Volume LVI, No. 14), page 137, available online at:
<https://books.google.com/books?id=Rn85AQAAMAAJ>.
- ¹⁵⁶ Textile World Journal, 15 – 22 November 1919 (Volume LVI, No. 18), page 142, available online at:
<https://books.google.com/books?id=Rn85AQAAMAAJ>.
- ¹⁵⁷ Textile World Journal, 3 July 1920 (Volume LVIII, No. 1), page 138, available online at:
<https://books.google.com/books?id=tRLT1nbIfacC>.
- ¹⁵⁸ American Lumberman, 10 May 1924 (Whole Number 2556), page 72, available online at:
<https://books.google.com/books?id=IkAcAQAAMAAJ>.
- ¹⁵⁹ Lumber, 25 July (Volume LXXIII, No. 1049), page 45, available online at:
<https://books.google.com/books?id=u1HIAAAAMAAJ>.
- ¹⁶⁰ Manufacturers Record, 27 August 1925 (Volume LXXXVIII, No. 9), pages 91, 96, available online at:

<https://books.google.com/books?id=yUnBUJbvSY4C>.

¹⁶¹ “Southern Wood-Using Industries and List of Dimension Stock Producers, Supplement to Southern Lumberman 1929 Christmas Issue,” 15 December 1929, page 210-I, available online at: <https://hdl.handle.net/2027/mdp.39015082994461>.

¹⁶² The First Annual Report of the Department of Workshop and Inspection, For the Nine Months Ending the Fiscal Year January 1st 1914. State of Tennessee, pages 70 – 71, available online at: <https://hdl.handle.net/2027/mdp.39015069384603>. The author did not find any predecessors to this report online.

¹⁶³ Second Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning January 1, 1914, and Ending December 1, 1914, State of Tennessee, page 94, available online at: <https://hdl.handle.net/2027/uc1.b3030562>.

¹⁶⁴ Third Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning December 1, 1914, and Ending December 1, 1915, State of Tennessee, pages 37 – 38, available online at: <https://hdl.handle.net/2027/uc1.b3030563>.

¹⁶⁵ Fourth Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning December 1, 1915, and Ending December 1, 1916, State of Tennessee, page 45, available online at: <https://hdl.handle.net/2027/uc1.b3030564>.

¹⁶⁶ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.

¹⁶⁷ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.

¹⁶⁸ Bureau of Workshop and Factory Inspection, Seventh Annual Report, January 1st, 1919 to December 31st, 1919, State of Tennessee, pages 27 and 30, available online at: <https://hdl.handle.net/2027/uc1.b3030567>.

¹⁶⁹ Bureau of Workshop and Factory Inspection, Ninth Annual Report, January 1st, 1921 to December 31st, 1921, State of Tennessee, page 47, available online at: <https://hdl.handle.net/2027/uc1.b3030570>.

¹⁷⁰ Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.

¹⁷¹ Sixth Annual Report, Department of Labor, State of Tennessee, 1 April 1929, pages 123 – 124, available online at: <https://hdl.handle.net/2027/uc1.b2991542>. The author did not find a copy of the 1927 edition of this report online.

¹⁷² “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 36, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920.

¹⁷³ Quote from: “Carnegie, Tennessee,” by Robbie D. Jones, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), page 5. Image from: “Johnson City Shale Brick Company, Broadway and Southern Railway,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/81>.

¹⁷⁴ “General Shale Johnson City Plant, 1928”, Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/12385>. Also at “General Shale, Johnson City Plant, 1928”, Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/20651>.

¹⁷⁵ “Johnson City Brick & Tile Co. 2, 3’... Johnson City, Tenn. 8 – 38”, Harold K. Vollrath Collection.

¹⁷⁶ “0-4-0-6,” Archives of Appalachia, Delbert W. Loudermilk Railroad Collection, 1947-2001, available online at: <https://archivesofappalachia.omeka.net/items/show/4488>, “0-4-0-6” appears to be a mistaken reference to the wheel arrangement of this locomotive, which appears to be an 0-4-0. Notice the “Wilder Block” in the left background.

¹⁷⁷ The Clay – Worker, January 1920 (Volume 73, No. 1), page 67, available online at: <https://books.google.com/books?id=YB5bAAAAYAAJ>.

¹⁷⁸ Brick and Clay Record, 1920 (Volume 56, No. 1), page 74, available online at: https://books.google.com/books?id=IY4_AQAAMAAJ.

¹⁷⁹ Manufacturers Record, 1 January 1919 (Volume LXXVII, No. 1), pages 174, 176, available online at: <https://books.google.com/books?id=RJs-AQAAMAAJ>.

¹⁸⁰ “Report on the Natural Resources Found of that Portion of the Tennessee River Basin Traversed by and Immediately Tributary to the Carolina, Clinchfield & Ohio Railway,” by D.C. Boy, Appendix E, in Tennessee River and Tributaries, North Carolina, Tennessee, Alabama, and Kentucky, submitted by Lansing H. Beach, Chief of Engineers, Document No. 319, House of Representatives, 2d Session, 67th Congress, 19 May 1922, pages 145 – 146, available online at: <https://books.google.com/books?id=F680AQAAMAAJ>.

¹⁸¹ The South’s Development: A Glimpse of the Past, the Facts of the Present, A Forecast of the Future, 11 December 1924, page 46, available online at: <https://books.google.com/books?id=mLYgAQAAMAAJ>.

¹⁸² The Clay-Worker, June 1925 (Volume 83, No. 7), pages 621 – 622, available online at: <https://books.google.com/books?id=tqTmAAAAMAAJ>.

¹⁸³ Blue Book of Southern Progress, 1926, pages 174 – 176, available online at: <https://books.google.com/books?id=tFjNAAAAMAAJ>.

¹⁸⁴ The Ceramist, January 1926 (Volume 7, No. 4), page 247, available online at: https://books.google.com/books?id=wyRByVo_-YQC.

¹⁸⁵ Manufacturers Record, 11 February 1926 (Volume LXXXIX, No. 6), page 95, available online at: <https://books.google.com/books?id=kpQ2gKVLsnEC>.

¹⁸⁶ Manufacturers Record, 27 May 1926 (Volume LXXXIX, No. 21), page 104, available online at: <https://books.google.com/books?id=kpQ2gKVLsnEC>.

¹⁸⁷ Manufacturers Record, 3 June 1926 (Volume LXXXIX, No. 22), page 112, available online at: <https://books.google.com/books?id=kpQ2gKVLsnEC>.

¹⁸⁸ Manufacturers Record, 3 June 1926 (Volume LXXXIX, No. 24), page 92, available online at: <https://books.google.com/books?id=kpQ2gKVLsnEC>.

¹⁸⁹ The Clay-Worker, July 1926 (Volume 86, No. 1), page 54, available online at: <https://books.google.com/books?id=RZMw77yKQN4C>.

¹⁹⁰ Daily Bulletin of the Manufacturers Record, Volume 60, page 3.

¹⁹¹ Daily Bulletin of the Manufacturers Record, 12 September 1928 (Volume 60), page 12.

¹⁹² “General Shale Products Corporation,” Archives of Appalachia, available online at: https://archives.etsu.edu/agents/corporate_entities/135.

¹⁹³ Old Hickory, 1929, Archives of Appalachia, East Tennessee State University, page 108, available online at: <https://dc.etsu.edu/yearbooks/8>.

¹⁹⁴ The Buccaneer, 1942, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/25>. Similar advertisements appeared in The Buccaneer between 1948 and 1950, available online at: <https://dc.etsu.edu/buccaneer/> and <https://dc.etsu.edu/buccaneer/index.2.html>.

¹⁹⁵ The Buccaneer, 1951, Archives of Appalachia, East Tennessee State University, unpaginated, available online at: <https://dc.etsu.edu/yearbooks/34>. Similar advertisements appeared in The Buccaneer between 1952 and 1956, available online at: <https://dc.etsu.edu/buccaneer/>.

¹⁹⁶ “George C. Sells Papers, 1962 – 1991”, Archives of Appalachia, East Tennessee State University, available online at: <https://archivesofappalachia.omeka.net/collections/show/49>.

¹⁹⁷ Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.

¹⁹⁸ Third Annual Report, Department of Labor, State of Tennessee, 25 February 1926, page 68, available online at: <https://hdl.handle.net/2027/umn.31951d03021441k>. The 1923 edition of this report is available online, but only contains a summary of the data at the county level. The author did not find a copy of the 1924 edition of this report online.

¹⁹⁹ Sixth Annual Report, Department of Labor, State of Tennessee, 1 April 1929, pages 123 – 124, available online at: <https://hdl.handle.net/2027/uc1.b2991542>. The author did not find a copy of the 1927 edition of this report online.

²⁰⁰ Seventh Annual Report, Department of Labor, State of Tennessee, 1929, pages 223 – 224, available online at: <https://hdl.handle.net/2027/uc1.b2991543>.

²⁰¹ Building the Clinchfield, by James A. Goforth, 1989, page 14.

²⁰² Building the Clinchfield, by James A. Goforth, 1989, page 31.

²⁰³ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” December 1908, image 19, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908.

²⁰⁴ East Tennessee: Historical and Biographical, 1893, pages 448 – 449, available online at: <https://books.google.com/books?id=i-4xAQAAMAAJ>, which continued: “He also assisted in the organization of the Johnson City Real Estate Company, the first in the city, and at the present time is a director in the same. As an engineer, he laid off the first addition to Johnson City, and is at this writing a member of the Board of Trade. He is a man of orders, and is a Mason and a Knight of Pythias. He was elected mayor of Johnson City in March, 1892, on the Democratic ticket, as one of the popular and entirely essential men of this growing and prosperous town. He was married, in 1879, in New London, Connecticut, to Frances M. Tousley, daughter of Judge H.S. Tousley and Mrs. H. Tousley, of Noble county, Indiana, and is the father of three children. Johnson City has many thoroughly alive men, but none more so than Mr. Matson, to whom is due a fair share of the prosperity of the place

²⁰⁵ The Railroad Gazette, 5 October 1888, page 657, available online at: <https://books.google.com/books?id=OmM2AQAAMAAJ>.

²⁰⁶ The Railroad Gazette, 22 February 1889, page 126, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.

²⁰⁷ The Railroad Gazette, 8 March 1889, page 171, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.

²⁰⁸ The Railroad Gazette, 28 June 1889, pages 436, 437, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.

²⁰⁹ The Railroad Gazette, 16 August 1889, pages 546, 547, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.

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- ²¹⁰ The Railroad Gazette, 30 August 1889, page 577, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.
- ²¹¹ The Railroad Gazette, 4 October 1889, page 653, available online at: <https://books.google.com/books?id=668xAQAAMAAJ>.
- ²¹² Manual of the Railroads of the United States for 1889, by Henry V. Poor, Volume 22, pages 563 – 564, available online at: <https://books.google.com/books?id=PLwvAQAAMAAJ>.
- ²¹³ Engineering News and American Railway Journal, 1 March 1890 (Volume XXIII, No. 9), page 215, available online at: <https://books.google.com/books?id=qN42AQAAMAAJ>.
- ²¹⁴ Railroad Gazette, 11 July 1890, page 500, available online at: <https://books.google.com/books?id=TrAxAQAAMAAJ>.
- ²¹⁵ Engineering News and American Railway Journal, 13 September 1890 (Volume XXIV, No. 37), page 241, available online at: <https://books.google.com/books?id=uvVZAAAAYAAJ>.
- ²¹⁶ The Railway Review, 25 October 1890 (Volume XXX, No. 43), page 645, available online at: <https://books.google.com/books?id=7m9xkyGksGUC>.
- ²¹⁷ “Carnegie Realty Co. et al. v. Carolina, C. & O. Ry. Co. et al...” 18 November 1916, in The Southwestern Reporter, Volume 189 (December 6, 1916 – January 10, 1917), pages 371 – 372, available online at: <https://books.google.com/books?id=gXk7AAAIAAAJ>.
- ²¹⁸ Manual of the Railroads of the United States for 1891, by Henry V. Poor, pages 632 – 633, available online at: <https://hdl.handle.net/2027/uc1.b4647643>.
- ²¹⁹ See also Johnson City Comet, 1 January 1891, Chronicling America: Historic American Newspapers, Library of Congress, available online at: <https://chroniclingamerica.loc.gov/lccn/sn89058128/1891-01-01/ed-1/seq-2/>.
- ²²⁰ Manual of the Railroads of the United States for 1892, by Henry V. Poor, pages 111 – 112, available online at: <https://books.google.com/books?id=3ktJAAAAMAAJ>.
- ²²¹ The Railroad Gazette, 19 February 1892, page 147, available online at: <https://books.google.com/books?id=Z5wxAQAAMAAJ>.
- ²²² The Railroad Gazette, 20 May 1892, page 379, available online at: <https://books.google.com/books?id=Z5wxAQAAMAAJ>.
- ²²³ The Railroad Gazette, 20 January 1893 (Volume 25), page 54, available online at: <https://books.google.com/books?id=cbC4yB8tXkcC>.
- ²²⁴ Johnson City Comet, 8 June 1893, available online at: <https://chroniclingamerica.loc.gov/lccn/sn89058128/1893-06-08/ed-1/seq-3/>.
- ²²⁵ Poor’s Manual of the Railroads of the United States, 1897 (Thirtieth Annual Number), pages 166 – 167, available online at: <https://hdl.handle.net/2027/mdp.39015039352151>.
- ²²⁶ Travelers’ Official Railway Guide for the United States, Canada and Mexico..., November 1895 (28th year, No. 6), page 765, available online at: <https://books.google.com/books?id=rMUsAQAAMAAJ>.
- ²²⁷ The Official Railway Equipment Guide, June 1895 (Volume XI, No. 1), page 157, available online at: <https://books.google.com/books?id=X-04AQAAMAAJ>.

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- ²²⁸ Poor's Manual of the Railroads of the United States, 1900 (Thirty-third Annual Number), page 1451, available online at: <https://books.google.com/books?id=MPocAAAIAAJ>. The Official Railway Equipment Register, April 1901 (Volume XVI, No. 11), page 218, available online at: <https://books.google.com/books?id=yA0qAQAAIAAJ>, which that five years later, the "South Carolina & Georgia Extension Railroad Co.," the former "Carolinas Division" of the Ohio River & Charleston Railway, had 9 locomotives, 135 40,000 pound boxcars, 44 60,000 pound boxcars, 50 40,000 pound flatcars, 20 60,000 pound flatcars, and 9 60,000 pound gondolas; as well as 10 passenger cars.
- ²²⁹ Poor's Manual of the Railroads of the United States, 1898 (Thirty-first Annual Number), pages 171, 1390, available online at: <https://hdl.handle.net/2027/mdp.39015039352169>.
- ²³⁰ Poor's Manual of the Railroads of the United States, 1899 (Thirty-second Annual Number), page 478, available online at: <https://hdl.handle.net/2027/mdp.39015039352177>.
- ²³¹ The Railway Age, 6 October 1899, page 743, available online at: <https://books.google.com/books?id=SM5LAAAAYAAJ>.
- ²³² The Railway Age, 29 December 1899, page 977, available online at: <https://books.google.com/books?id=SM5LAAAAYAAJ>.
- ²³³ Poor's Manual of the Railroads of the United States, 1900 (Thirty-third Annual Number), page 1451, available online at: <https://books.google.com/books?id=MPocAAAIAAJ>.
- ²³⁴ The Official Guide of the Railways and Steam Navigation Lines of the United States, Porto Rico, Canada, Mexico and Cuba, July 1901 (34th Year, No. 2), page 839, available online at: <https://hdl.handle.net/2027/chi.096493209>.
- ²³⁵ Poor's Manual of the Railroads of the United States, 1903 (Thirty-sixth Annual Number), page 323, available online at: <https://hdl.handle.net/2027/mdp.39015039352219>.
- ²³⁶ When Steam Ran the Clinchfield, by James A. Goforth, 1991, page 8.
- ²³⁷ Image from: "Carolina, Clinchfield and Ohio Railway Locomotive," Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/12232>. Quotes from: The Railroads of Johnson City, by Johnny Graybeal, 2007, page 135; and Greasy Cove in Unicoi County: Authentic Folklore, by Pat Alderman, 1975, page 15.
- ²³⁸ Adapted from The Official Railway Equipment Register, July 1905, page 262, available online at: <https://books.google.com/books?id=YhUIAAAAMAAJ>. The April 1905 edition, page 262, available online at: <https://books.google.com/books?id=WCIqAQAAIAAJ>, showed no change.
- ²³⁹ Poor's Manual of the Railroads of the United States, 1907 (Fortieth Annual Number), page 495, available online at: <https://books.google.com/books?id=8dAhAQAAAMAAJ>.
- ²⁴⁰ "South and Western Offices Will Come to Johnson City – Hotel Carnegie and Other buildings Have Been Leased and the Transfer Will be Made Within Next Sixty Days – Means Much to Our City. – The Bristol Herald of the 5th instant says: 'It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter have been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted an otherwise repaired and will afford ample room for the use which will be made of them.'"
- ²⁴¹ The Coal Trade Journal, 10 October 1906, page 743, available online at: <https://books.google.com/books?id=WXBQAAMAAJ>.
- ²⁴² The Official Railway Equipment Register, July 1907 (Volume XXIII, No. 2), page 434, available online at: <https://books.google.com/books?id=JB4qAQAAIAAJ>. The May 1907 edition, page 434, available online at:

<https://books.google.com/books?id=JB4qAQAAIAAJ>, had the same data on rolling stock, but with slight changes to the location of the personnel.

²⁴³ The Official Railway Equipment Register, August 1907 (Volume XXIII, No. 3), page 434, available online at: <https://books.google.com/books?id=5R0qAQAAIAAJ>. There appears to have been few changes to this information in the October and November editions of 1907, page 434, available online at: <https://books.google.com/books?id=sx4qAQAAIAAJ>.

²⁴⁴ The Official Railway Equipment Register, February 1908 (Volume XXIII, No. 9), page 454, available online at: <https://books.google.com/books?id=MyEqAQAAIAAJ>. Similar information in the March 1908 edition, page 454, available online at: <https://books.google.com/books?id=MyEqAQAAIAAJ>.

²⁴⁵ Industrial World, 9 March 1908 (42d Year, No. 10), page 297, available online at: <https://books.google.com/books?id=wqDAQAAMAAJ>.

²⁴⁶ The Railway and Engineering Review, 25 April 1908 (Volume XLVIII, No. 17), page 344, available online at: https://books.google.com/books?id=4DDfqJmv_AgC.

²⁴⁷ The Clinchfield Railroad. The Story of a Trade Route Across the Blue Ridge Mountains, by William Way, Jr., 1931, pages 172 – 187 (the quoted portion is on pages 185 – 186), available online at: <https://hdl.handle.net/2027/mdp.39015006368792>, which notes: “Colonel Fred A. Olds, of the North Carolina Historical Commission, was probably the first person not connected with the Clinchfield to travel over its line across the Blue Ridge mountains. Colonel Olds wrote a fascinating account of his trip for the Charlotte Daily Observer. Permission has been granted to use this story, and it appears below as published in the Charlotte Daily Observer of August 30 and September 2, 1908...” The Clinchfield Railroad. The Story of a Trade Route Across the Blue Ridge Mountains, by William Way, Jr., 1931, page 154, available online at: <https://hdl.handle.net/2027/mdp.39015006368792>, this section reads in its entirety: “After crossing the North and South Forks of the Holston River, and passing through Kingsport, the line of the [Clinchfield] railroad breaks across the various ridges and valleys which slope in a northeasterly direction to the Watauga River, and finally reaches Johnson City. Just north of Johnson City, the road passes through its longest cut, called Soldiers’ Home cut. It is 3,000 feet long, and has a maximum depth of 85 feet. In its excavation, there was involved the removal of about 500,000 cubic yards of earth. The material was hauled several miles to Knob Creek fill, 2,700 feet long. At Erwin, sixteen miles south of Johnson City, are located the operating headquarters of the company. Extensive terminal facilities, shops, and a modern building for the general offices of the company have been erected. The large railway yard stretches out for some distance beyond the ends of the town along a flat valley almost completely surrounded by high mountains.”

²⁴⁸ This article took up three pages of the Johnson City Comet, the complete text is available online at: <https://chroniclingamerica.loc.gov/lccn/sn89058128/1909-04-15/ed-1/seq-1/>, <https://chroniclingamerica.loc.gov/lccn/sn89058128/1909-04-15/ed-1/seq-2/>, and <https://chroniclingamerica.loc.gov/lccn/sn89058128/1909-04-15/ed-1/seq-3/>.

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- ²⁴⁹ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” January 1891, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891.
- ²⁵⁰ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” May 1897, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897.
- ²⁵¹ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 11, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903.
- ²⁵² “Carnegie Hotel,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6265>. Quoted text is from *Johnson City*, by L. Thomas Roberts, 2018, page 83. *The Railroads of Johnson City*, by Johnny Graybeal, 2007, page 174, which included as a caption: “Switcher No. 1647 was a 1906 Pittsburgh 0-6-0, builder’s number 37676, and was used in the Carnegie yard area...” Note, Southern engine No. 1647 appears to have been a Class A-7 locomotive. See also “Southern Steam Switchers,” available online at: http://southern.railfan.net/images/archive/southern/steam/switchers/steam_switchers.html.
- ²⁵³ “Old Industrial Buildings in Carnegie,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at <https://archivesofappalachia.omeka.net/items/show/11689>. Also available at: “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/images/photos/photos1/carnhotel1.jpg>.
- ²⁵⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” December 1908, image 19, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908. The “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 11, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903, identifies the C.C.&O. General Office as “The Wilder Block.” An image very similar to that shown of the CC&O Railroad General Offices in 1908 is available at “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” July 1913, image 23, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913. By 1920, this building was occupied by the Empire Chair Company and the Hotel Carnegie was gone, see “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 33, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920.
- ²⁵⁵ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” July 1913, image 23, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913.
- ²⁵⁶ “Millers Lumber Company, New Street and Southern Railway,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/114>.
- ²⁵⁷ “McCorkle Lumber Company, Empire Furniture Company, Broadway Street and Southern Railway, Carolina, Clinchfield & Ohio, Carnegie,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/106>.
- ²⁵⁸ “McCorkle Lumber Company, Empire Furniture Company, Broadway Street and Southern Railway, Carolina, Clinchfield & Ohio, Carnegie,” Burr Harrison Photographs, circa 1881 – 1958, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/106>.
- ²⁵⁹ “Building,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/12068>.
- ²⁶⁰ “Johnson’s Depot,” available online at: http://www.stateoffranklin.net/johnsons/faq/empire_1917.jpg, which included: “Ten years ago the Empire Chair Co., employed two men and two boys. Today they employ 180 people. Their factory has 150,000 feet of floor space. Material is handled by us, from the stump to the finished article. We have our own sawmill, of 15,000 feet daily capacity. Here we make our own Quartered Oak; also much other material.

All our Quartered Oak is White Oak, carefully selected, which means a beautiful, distinct flake or grain. All the lumber we use is air dried before going to the Dry Kilns – the only way to properly prepare furniture lumber.”

²⁶¹ “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/faq/empirefum1930.jpg>.

²⁶² “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/faq/empire1950.jpg>.

²⁶³ “Johnson’s Depot,” available online at:

<http://www.cfordart.com/photoalbum/photopages/fullsizepages/1999junefullsize/23fullsize.html>, which included: “One last look at the outside of the old CC&O office building. My photos from 1997 show the original entrance to this building, the north side, which is on the right-hand side of the structure. The part of this building that all matches is the east side of the original building. This side would have faced the CC&O furnace spur and the ET&WNC furnace spur and the Southern line going north. The side opposite this side is VERY plain and looks nothing like this side. Kind of strange. The small part on the right that doesn’t match is the covered connector that covers the original front of the structure and covers old Centre Street and extends the building over to the manufacturing part of the business, which is Empire/Vaughn Manufacturing. I would have liked to have seen this before all the windows were bricked up. Looks like some very nice arched openings there. If this makes any sense at all to anyone, I’ll be amazed. Anyway, the brickwork is very nice, and this building has a lot of history in it. I arrived as everyone was going to lunch. At the rate they were leaving, I don’t think many of them cared a whole lot about the history bit.”

²⁶⁴ “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/images/photos/photos3/ruins1.jpg>. A similar photograph is available at: “Carnegie, Tennessee,” by Robbie D. Jones, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), page 3.

²⁶⁵ “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/images/photos/photos3/ruins3.jpg>.

²⁶⁶ “Johnson’s Depot,” available online at:

<http://www.stateoffranklin.net/johnsons/images/photos/photos3/backhotel.jpg>. Additional “recent” photographs of the interior are available online at:

<http://www.stateoffranklin.net/johnsons/images/photos/photos3/ruinsinside1.jpg>,

<http://www.stateoffranklin.net/johnsons/images/photos/photos3/arches.jpg>,

<http://www.stateoffranklin.net/johnsons/images/photos/photos3/stairs.jpg>, and

<http://www.stateoffranklin.net/johnsons/images/photos/photos3/floor3.jpg>. Additional “recent” photographs are also

available at: <http://www.cfordart.com/photoalbum/photopages/fullsizepages/1997junefullsize/1fullsize.html>,

<http://www.cfordart.com/photoalbum/photopages/fullsizepages/1997junefullsize/2fullsize.html>,

<http://www.cfordart.com/photoalbum/photopages/fullsizepages/1997junefullsize/3fullsize.html>,

<http://www.cfordart.com/photoalbum/photopages/fullsizepages/1997junefullsize/4fullsize.html>, and

<http://www.cfordart.com/photoalbum/photopages/fullsizepages/1997junefullsize/5fullsize.html>.

²⁶⁷ “Empire Furniture, Men Working,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/14543>.

²⁶⁸ “Empire Furniture, Man Working,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/14541>.

²⁶⁹ “Empire Furniture, Men Working,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/14540>.

²⁷⁰ “South and Western Offices Will Come to Johnson City – Hotel Carnegie and Other buildings Have Been Leased and the Transfer Will be Made Within Next Sixty Days – Means Much to Our City. – The Bristol Herald of the 5th instant says: ‘It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter have been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted an otherwise repaired and will afford ample room for the use which will be made of them.’”

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- ²⁷¹ The Coal Trade Journal, 10 October 1906, page 743, available online at: <https://books.google.com/books?id=WXBJAQAAMAAJ>.
- ²⁷² Carolina, Clinchfield and Ohio Railway, Sixth Annual Report for the Fiscal Year Ended June 30, 1916, page 31, available online at: <https://books.google.com/books?id=vMk7AQAAMAAJ>.
- ²⁷³ The Furniture Worker, December 1917 (Volume 34, No. 12), page 45, available online at: <https://books.google.com/books?id=9WgoAAAAYAAJ>.
- ²⁷⁴ The Furniture Worker, December 1917 (Volume 34, No. 12), page 526, available online at: <https://books.google.com/books?id=9WgoAAAAYAAJ>.
- ²⁷⁵ The St. Louis Lumberman, 15 December 1917 (Volume LX, No. 12), page 56, available online at: <https://books.google.com/books?id=wdUwAQAAMAAJ>.
- ²⁷⁶ The Furniture Journal, July 1922 (Volume LII, Number 1), page, available online at: <https://books.google.com/books?id=VVkXKPJjt5MC>.
- ²⁷⁷ Southern Lumberman's Directory of American Lumber Consuming Factories, 1922, page 833, available online at: <https://books.google.com/books?id=f6MeAAAIAAJ>.
- ²⁷⁸ American Lumberman, 6 March 1926 (Whole Number 2651), page 76, available online at: <https://books.google.com/books?id=8MMcAQAAMAAJ>.
- ²⁷⁹ Manufacturers Record, 30 September 1926 (Volume XC, no. 13?), page 106, available online at: <https://books.google.com/books?id=qBCAW2IKmWwC>.
- ²⁸⁰ Manufacturers Record, 11 November 1926 (Volume XC, No. 19), page 111, available online at: <https://books.google.com/books?id=qBCAW2IKmWwC>.
- ²⁸¹ Wood Working Industries, December 1926 (Volume II, No. 12), page 27, available online at: <https://books.google.com/books?id=F7YmAQAIAAJ>.
- ²⁸² Industrial Woodworking: A Magazine for Woodworking Production Executives, Volume 32, 1931, page 62.
- ²⁸³ Tennessee: A History, 1673 – 1932, Volume 4, by Philip May Hamer, 1933, page 618.
- ²⁸⁴ Labor Relations Reference Manual, 1940 (Volume 5), page 788.
- ²⁸⁵ “N.L.R.B. v. Empire Furniture Corporation,” 8 November 1939, “On petition to enforce Board Order,” in Court Decisions Relating to the National Labor Relations Act, Volume 1, Cases Decided Before December 31, 1939, March 1944, pages 1381 – 1386, available online at: <https://books.google.com/books?id=Siq5AAAAIAAJ>. Additional information is available in: “In the Matter of Empire Furniture Corporation and Textile Workers Organizing Committee,” “Case Nos. C-305 and R-386. – Decided January 7, 1939,” in Decisions and Orders of the National Labor Relations Board, Volume 10 (December 1, 1938 – January 31, 1939), pages 1026 – 1042, available online at: <https://books.google.com/books?id=m0fC39uThEC>.
- ²⁸⁶ “North American Van Lines, Inc., Extension – Overall, No. MC-107012 (Sub. No. 61),” Interstate Commerce Commission Reports: Motor Carrier Cases, Volume 107, page 758, available online at: <https://books.google.com/books?id=6divBEIz2Y0C>.
- ²⁸⁷ The Buccaneer, 1941, Archives of Appalachia, East Tennessee State University, page 77, available online at: <https://dc.etsu.edu/yearbooks/24>. Similar advertisements appeared in the 1949 and 1950 editions of The Buccaneer, available online at: <https://dc.etsu.edu/buccaneer/>.

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- ²⁸⁸ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.
- ²⁸⁹ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.
- ²⁹⁰ Bureau of Workshop and Factory Inspection, Seventh Annual Report, January 1st, 1919 to December 31st, 1919, State of Tennessee, pages 27 and 30, available online at: <https://hdl.handle.net/2027/uc1.b3030567>.
- ²⁹¹ Bureau of Workshop and Factory Inspection, Eighth Annual Report, January 1st, 1920 to December 31st, 1920, State of Tennessee, pages 38 – 39, available online at: <https://hdl.handle.net/2027/uc1.b3030568>.
- ²⁹² Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.
- ²⁹³ Third Annual Report, Department of Labor, State of Tennessee, 25 February 1926, page 68, available online at: <https://hdl.handle.net/2027/umn.31951d03021441k>. The 1923 edition of this report is available online, but only contains a summary of the data at the county level. The author did not find a copy of the 1924 edition of this report online.
- ²⁹⁴ Fourth Annual Report, Department of Labor, State of Tennessee, 25 February 1927, page 73, available online at: <https://hdl.handle.net/2027/uc1.b2991541>.
- ²⁹⁵ Seventh Annual Report, Department of Labor, State of Tennessee, 1929, pages 223 – 224, available online at: <https://hdl.handle.net/2027/uc1.b2991543>.
- ²⁹⁶ Annual Report of the Department of Labor for the Year 1935, State of Tennessee, pages 130 – 131, available online at: <https://hdl.handle.net/2027/mdp.35112104245289>. The author did not find a copy of the 1934 edition of this report online, nor of any subsequent reports.
- ²⁹⁷ “Carnegie Hotel,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6267>. Also available at: “Hotel Carnegie,” Ridley Wills East Tennessee Postcard Collection, 1906 – 1996 and undated, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/2264>, postmarked 11 August 1906; “Carnegie Hotel,” Clifford A. Maxwell Photographs, 1940 – 1986, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/11998>; “Carnegie Hotel,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6266>; and “State of Franklin,” “Johnson’s Depot,” available online at: <http://www.stateoffranklin.net/johnsons/images/postcards/post1/carnghotel.jpg>. See also <http://www.stateoffranklin.net/johnsons/links/carnegiehotel.pdf>.
- ²⁹⁸ “Hotel Carnegie,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6264>.
- ²⁹⁹ “Hotel Carnegie: Front View,” Ridley Wills East Tennessee Postcard Collection, 1906 – 1996 and undated, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6262>. See also “Hotel Carnegie (Front View),” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/2235>, which was postmarked 17 December 1908. This photograph, or a very similar one, appeared in the Johnson City Comet on 15 April 1909.
- ³⁰⁰ “Partial View of Office and Lobby, Hotel Carnegie,” Ridley Wills East Tennessee Postcard Collection, 1906 – 1996 and undated, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/2237>.
- ³⁰¹ “Partial View of Main Dining Room: Hotel Carnegie,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6263>.

³⁰² “Hotel Carnegie – The Finest Hotel in East Tennessee Completed... Johnson City Comet – July 8, 1891,” at Johnson’s Depot, available online at: <http://www.stateoffranklin.net/johnsons/links/carnegiehotel.pdf>.

³⁰³ Johnson City Comet, 7 April 1910, *Chronicling America: Historic American Newspapers*, Library of Congress, available online at: <https://chroniclingamerica.loc.gov/lccn/sn89058128/1910-04-07/ed-1/seq-4/>.

³⁰⁴ “South and Western Offices Will Come to Johnson City – Hotel Carnegie and Other buildings Have Been Leased and the Transfer Will be Made Within Next Sixty Days – Means Much to Our City. – The Bristol Herald of the 5th instant says: ‘It is stated upon authority that the offices of the South and Western Railway will be moved from Bristol to Johnson City within sixty days. The reason for this change is lack of room, the offices at present being scattered in various portions of this city. At Johnson City the South and Western will occupy the Carnegie Hotel and the Wilder Block, the latter have been originally designed for railroad offices. These buildings will be put in shape at once for occupancy. They will be repainted an otherwise repaired and will afford ample room for the use which will be made of them.’”

³⁰⁵ The Coal Trade Journal, 10 October 1906, page 743, available online at: <https://books.google.com/books?id=WXBQAAMA AJ>.

³⁰⁶ Packages, December 1924 (Volume XXVII, No. 12), page 40, available online at: <https://books.google.com/books?id=CxkdAQAAMA AJ>. Manufacturers Record, 4 January 1917 (Volume LXXI, No. 1), page 75, available online at: <https://books.google.com/books?id=d3U9AQAAMA AJ>, included: “Va., Nora. – McCorkle Lumber Co., capital \$50,000, incptd.; MC. McCorkle, Prest.; M.R. McCorkle, Secy-Treas.; both of Big Stone Gap, Va.”

³⁰⁷ Manufacturers Record, December 1924 (Volume LXXXVI, No. 23), page 110, available online at: <https://books.google.com/books?id=5q0mHaLc6dAC>. History of Virginia, Volume VI, Virginia Biography, 1924, page 289, available online at: <https://books.google.com/books?id=GikSAAAAYAAJ>, included: “Claiborne Ross McCorkle, commonwealth attorney of Wise County, was born at Willowton, Mercer County, West Virginia, on December 26, 1882, and is the son of Mastin Clay McCorkle, who now resides at Nora, Dickenson County, Virginia. The father was born on September 3, 1853, near Alderson, West Virginia, and was there reared and educated. When yet a young man he removed to Mercer County, West Virginia, and became connected with the timber and lumber manufacturing business, and has ever since followed that occupation. His business has grown extremely in extent and his products find a market over a very wide range of country. He is accordingly well-to-do, with large property holdings and a high reputation as an industrialist and citizen. He is president of the McCorkle Lumber Company, Inc. He served as deputy sheriff of Mercer County from 1894 to 1898, is a stanch republican, and an eminent member of the Disciples of Christ Church. He is also a member of the Masonic Fraternity, being a Knights Templar and a member of Kazim Temple, Ancient Arabic Order Nobles of the Mystic Shrine, of Roanoke. Some time after reaching manhood he married Miss Rhoda Jane Cannaday, who was born June 19, 1859, at Willowton, West Virginia, and was given a good education in girlhood. The family of children born to this marriage were as follows: Merton Raymond, who now lives at Johnson City, Tennessee, and is associated with his father in the Lumber business...”

³⁰⁸ The Lumber Manufacturer & Dealer, 12 December 1924 (Volume LXXIII, No. 40), page 65, available online at: <https://books.google.com/books?id=u1HIAAAAMA AJ>.

³⁰⁹ Lumber World Review, 10 December 1924 (Volume XLVII, Number 11), page 61, available online at: <https://books.google.com/books?id=Ht9eaAi7jTEC>.

³¹⁰ The New York Lumber Trade Journal, 15 October 1925 (Volume LXXIXC, Whole No. 944), page 46, available online at: <https://books.google.com/books?id=5khPAAAAYAAJ>.

³¹¹ The Lumber Manufacturer & Dealer, February 1928 (Volume LXXXI, No. 2), page 64, available online at: <https://books.google.com/books?id=9QJOAAAAYAAJ>. Manufacturers Record, 8 March 1928 (Volume XCIII, No. 10), page 83, available online at: <https://books.google.com/books?id=hAvlRG8odDsC>, included: “How a Lumberman Regards It. M.E. McCorkle, McCorkle Lumber Corporation, Johnson City, Tenn. – We enclose check for \$10 for two years’ subscription to the *Manufacturers Record* to January, 1930, and, if living then and the *Record* is as good as it

is today, the subscription will be cheerfully renewed again. The writer used the Manufacturers Record yesterday in Sunday School, read several paragraphs from a number of articles in the editorial and advised everyone in the school that could take it to subscribe for it. I consider it among the best journals published today, and cannot understand why any subscriber cannot find time to read it.”

³¹² “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” January 1891, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891.

³¹³ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” May 1897, image 6, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897.

³¹⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 11, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903, which included: “Constant watchman. No clock. Lights: gas. No heat. Fuel: coal & coke. 2 F.P.’s capacity 1 million gal’s ea. per 24 hours. Supply hyd’s as shown. 300’ 2’ hose & 200’ 2½” hose on premises.”

³¹⁵ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” December 1908, image 19, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908, which included: “Constant Watchman. No clock. Fuel: coal & coke. No heat. Lights: elec. Two F.P.’s capacity one million gals each per 24 hrs. Supply hyd’s as shown. 300’ 2” hose & 200’ 2½” hose on premises.”

³¹⁶ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” July 1913, image 23, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913, which included: “Constant watchman. – No clock. – No heat. – Lights: Elec. – 2 p[umps, cap’y 1 million gal. per 24 hrs. each. – Supply from reservoir. – 6” main from city water works. – 10 H. & 6 – 2” hyds. In yard. – 200’ 2” hose. 100’ 2½” hose. – Fuel: coal & coke.”

³¹⁷ “Carnegie (Cranberry) Furnace,” by Robbie D. Jones and John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), page 14.

³¹⁸ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” February 1920, image 35, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920, which included: “Men on duty day & night. No clock, no heat. Light: elec. 2 pumps cap’y 1-million gals, per 24 hours each. Supply from reservoir. 6” main from city water works. One D.H. & 6 – 2” hyds. In yard. 200’ 2” hose. 100’ 2½” hose. Fuel: coal & coke. 60 gal. chem. Eng. & chem. Fire extgrs in store building.”

³¹⁹ “Carnegie Furnace,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6583>.

³²⁰ “Postcard: Cranberry Furnace By Night, Johnson City, TN. Color Photo,” East Tennessee and Western North Carolina Railroad Historical Society Collection, 1883 – 2005, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/16603>. Also at “Cranberry Furnace By Night,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6584>, postmarked 19 May 1925. A black and white version is available at “Postcard: Cranberry Furnace By Night, Johnson City, TN. Color Photo,” East Tennessee and Western North Carolina Railroad Historical Society Collection, 1883 – 2005, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/16604>, note, the title of this file includes “Color Photo”, which is plainly incorrect.

³²¹ “Carnegie Furnace, Cranberry Iron Company (Johnson City, Tenn.),” 1925?, Hagley ID: 1986268_0081, Series II, Albert T. Keller photographs, Box 19, Folder 139, American Iron and Steel Institute photographs and audiovisual materials (Accession 1986.268), Audiovisual Collections and Digital Initiatives Department, Hagley Museum and Library, Wilmington, DE 19807, available online at: https://digital.hagley.org/1986268_0081, which noted: “View of Carnegie Furnace, built in 1890 and operated by the Cranberry Iron Company until 1929. This photograph is undated, but was probably taken at some point in the 1920s.”

³²² Image from “Cranberry Furnace,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/6586>, captioned: “7130 – The Furnace of the Cranberry Furnace Co. Johnson City, Tenn. Souvenir Post Card Co. N.Y.” Text quoted from Johnson City, by L. Thomas Roberts, 2018, page 67.

³²³ The Magnetic Ores of East Tennessee and Western North Carolina, by W.S. Bayley, Bulletin 29, Division of Geology, Department of Education, State of Tennessee, 1 April 1923, Page 103 (Plate XIV), available online at: <https://books.google.com/books?id=73HI7R110boC>, pages 11 and 12 included: “The field work on which this report is based was done during parts of the summers of 1919, 1920, and 1921... In 1920 the openings in the Cranberry belt of deposits extending westward from Cranberry into Carter County were again visited, about two weeks being spent in the study of critical areas... In 1921... A number of the most important deposits were again examined and the Cranberry mine was sampled for an experimental study of susceptibility of its ores to magnetic separation methods.”

³²⁴ Chris Ford noted in an email to the author dated 22 September 2024: “The little wood ‘engine house’ outside the boiler house was for what I believe to be a [stationary] steam operated ‘engine’, most likely a water pump for the boilers. I don’t think it was large enough for a RR steam engine, unless they had some very small Porter type engine, but I never heard of anything like that at the furnace. I think it was an expendable structure that could have been gone in a few years... it definitely wasn’t permanent looking.” For more information on the use of switch engines at the Carnegie Furnace, see the Johnsn City Comet on 24 November 1898 and 27 February 1902, in this section. Regrettably, the author has not located any additional information on these engines. Nevertheless, the arrangement of the track, engine house and shed are unusual if they existed only for the operation of a stationary steam engine.

³²⁵ “Carnegie (Cranberry) Furnace,” by Robbie D. Jones and John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), page 11.

³²⁶ Quote from: “Carnegie (Cranberry) Furnace, by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., Fall/Winter 2003 (Volume 14, Number 2), page 4. Image from: “The Cranberry Furnace,” by 1909. Ridley Wills East Tennessee Postcard Collection, 1906 – 1996 and undated, Archives of Appalachia, available online at: <https://archivesofappalachia.omeka.net/items/show/2220>, postmarked 1 August 1910. Also at “Cranberry Furnace,” Mildred S. Kozsuch Papers, 1867 – 2007, Archives of Appalachia, accessed August 25, 2023, <https://archivesofappalachia.omeka.net/items/show/6585>, and Johnson City: The Way We Were, by J.O. Lewis, 1909 (reprinted 1989), page 32. Regrettably, the Burr Harrison version of this photograph does not appear to have been included among those photographs that the Archives of Appalachia made available online.

³²⁷ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at: <http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”. See also “Carnegie (Cranberry) Furnace,” by Robbie D. Jones and John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), pages 10 – 14.

³²⁸ American Machinist, 26 February 1887 (Volume 10, No. 9), page 9, available online at: <https://books.google.com/books?id=Fy8tPP3EHSsC>.

³²⁹ The Iron Age, 30 April 1891 (Volume XLVII, No. 18), page 835, available online at: <https://books.google.com/books?id=oOs-AQAAMAAJ>.

³³⁰ American Economist, 14 July 1893 (Volume XII, No. 2), page 24, available online at: <https://books.google.com/books?id=n3UpAAAAYAAJ>.

³³¹ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at: <http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”. For general information on the economic situation, see “Panic of 1893,” available online at:

https://en.wikipedia.org/wiki/Panic_of_1893.

³³² Johnson City Comet, 8 June 1893, available online at:
<https://chroniclingamerica.loc.gov/lccn/sn89058128/1893-06-08/ed-1/seq-3/>.

³³³ The Iron Age, 17 October 1895 (Volume LVI, No. 16), page 62, available online at:
<https://books.google.com/books?id=rj4WU8FlxfC>. Similar in Johnson City Comet, 10 October 1895.

³³⁴ The Iron Age, 28 November 1895 (Volume LVI), page 1106, available online at:
<https://books.google.com/books?id=rj4WU8FlxfC>.

³³⁵ Hardware, 25 October 1903 (XXVIII, No. 2), page 19, available online at:
<https://books.google.com/books?id=ppo7AQAAMAAJ>.

³³⁶ “Today in Johnson City History: May 7,” by Rebecca Henderson, available online at:
<https://www.johnsoncitypress.com/living/today-in-johnson-city-history-may-7>.

³³⁷ “Today in Johnson City History: July 22,” by Rebecca Henderson, available online at:
<https://www.johnsoncitypress.com/living/today-in-johnson-city-history-july-22>.

³³⁸ American Manufacturer and Iron World, 23 December 1898 (Volume LXIII, No. 26), page 907, available online at:
https://books.google.com/books?id=Wbg_AQAAMAAJ.

³³⁹ Along the ET&WNC, Volume V: Freight Cars Part B, by Johnny Graybeal, 2004, pages 33 and 36.

³⁴⁰ See also “Wilberforce Sully, Trustee; Wilberforce Sully in his own Right, Alfred B. Carhart, Mrs. Mary P. Myton, and The Travelers’ Insurance Company, Plaintiffs in Error vs. The American National Bank et al,” Supreme Court of the United States, October Term, 1899, No. 266, available online at:
<https://books.google.com/books?id=x1hBAAAAYAAJ>.

³⁴¹ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at:
<http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”.

³⁴² Directory of Iron and Steel Works of the United States, “Corrected to December 31, 1901 (15th Edition), pages 126, 127, and 244, available online at: <https://books.google.com/books?id=8O3mAAAAMAAJ>.

³⁴³ The Iron Trade Review, 10 February 1902 (Volume XXXV, Number 8), page 41, available online at:
<https://books.google.com/books?id=HelBAQAAMAAJ>.

³⁴⁴ The Iron Age, 27 March 1902 (Volume 69), page 30, available online at:
<https://books.google.com/books?id=xqM-AQAAMAAJ>.

³⁴⁵ The Iron Trade Review, 15 May 1902 (Volume XXXV, Number 20), page 46, available online at:
<https://books.google.com/books?id=HelBAQAAMAAJ>.

³⁴⁶ Manufacturers Record, 11 June 1903 (Volume XLIII, No. 24), page 427, available online at:
<https://books.google.com/books?id=T3E9AQAAMAAJ>.

³⁴⁷ The Commercial and Financial Chronicle, 11 November 1905 (Volume LXXXI, No. 2107), page 1435, available online at: <https://books.google.com/books?id=nQZmqgALF3YC>.

³⁴⁸ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at:

<http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”.

³⁴⁹ Third Annual Report of the Harvard Mining Club, 1905, page 8, available online at: <https://books.google.com/books?id=FpY7AQAAMAAJ>, included: “Summer of 1905... From Embreeville the party proceeded to Cranberry, N.C., where an unusual occurrence of magnetic iron ore is being mined, by both open pit and underground work. Magnetic separation is attempted with varying degrees of success, the concentrated product being shipped to the single furnace of the Cranberry Furnace Company at Johnson City. In the town stands a disused iron furnace, the oldest of the district, which was of interest on account of its brick construction and general crudity. The class then travelled to the Southwestern Virginia coal district. With Big Stone Gap as a centre, short trips were made to the coal mines and coke batteries of the Imboden Coal and Coke Company and the Stonega Coal and Coke Company.”

³⁵⁰ Along the ET&WNC, Volume V: Freight Cars Part B, by Johnny Graybeal, 2004, pages 33 and 36.

³⁵¹ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at: <http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”.

³⁵² Industrial World, 16 October 1911 (Forty-Fifth Year, Number Forty-Two), page 1243, available online at: <https://books.google.com/books?id=aLBAAQAAMAAJ>.

³⁵³ Directory of Iron and Steel Works of the United States and Canada, “Nineteenth Edition Corrected to May 1st, 1920,” page 117, available online at: <https://books.google.com/books?id=cVdP9EpbvegC>.

³⁵⁴ “Carnegie (Cranberry) Furnace”, by John R. Waite, available online at: <http://www.stateoffranklin.net/johnsons/tweetsie/furnace.pdf>, which included: “Article First Appeared in the Blue Ridge Stemwinder, Fall/Winter 2003 Issue The Stemwinder is published by the ET&WNC Railroad Historical Society”.

³⁵⁵ The Blast Furnace and Steel Plant, May 1922 (Volume X, No. 5), page 300, available online at: <https://books.google.com/books?id=Sxg4AQAAMAAJ>.

³⁵⁶ The Blast Furnace and Steel Plant, July 1922 (Volume X, No. 7), page 401, available online at: <https://books.google.com/books?id=Sxg4AQAAMAAJ>.

³⁵⁷ Iron Trade Review, 3 January 1924 (Volume LXXIV, No. 1), page 51, available online at: <https://books.google.com/books?id=LT4fAQAAMAAJ>.

³⁵⁸ American Mining and Metallurgical Manual, 1923 Edition, by alexander R. Dunbar, page 417, available online at: <https://books.google.com/books?id=d5zuAsTdYFcC>.

³⁵⁹ Iron Trade Review, 12 November 1925 (Volume LXXVII, No. 20), pages 1182, 1245, available online at: <https://hdl.handle.net/2027/mdp.39015014702594>.

³⁶⁰ Johnson City, by Sonya A. Haskins, 2005, page 107.

³⁶¹ “Carnegie (Cranberry) Furnace,” by John R. Waite, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), pages 10 – 14.

³⁶² “Carnegie, Tennessee,” by Robbie D. Jones, in The Blue Ridge Stemwinder, East Tennessee and Western North Carolina R.R. Co., October/November/December 1998 (Volume 11, Number 2), pages 3 – 9.

³⁶³ “Iron Ores and Iron Industries of the Tennessee Valley Region,” by Edwin C. Eckel, in Iron, Chromite, and Nickel Resources of the Tennessee Valley Region, Geologic Bulletin No. 10, April 1938, page 6, available online at: <https://books.google.com/books?id=WDDWAAAAMAAJ>.

³⁶⁴ Available online at: <https://www.facebook.com/photo/?fbid=1611034745821977&set=pcb.1611034822488636>.

³⁶⁵ Along the ET&WNC, Volume IV: Freight Cars Part A, by Johnny Graybeal, 2003, page 143. On page 76: “Lumber shipments continued to be brisk as the Nineteenth Century came to a close, but they increased measurably when the Linville River Railway began handing over lumber to the ET&WNC at Cranberry. LR records are not available prior to 1907, but in 1901, the ET&WNC carried over 25,280 tons of lumber, an increase of almost 7,000 tons from the year before. In 1902, the figure jumped to 37,448 tons of lumber, and by 1904, over 50,000 tons of lumber was carried for the first time...” Of note, seven significant logging railroads, operated by six different lumber companies were served by the E.T. & W.N.C. R.R. and L.R.R. during its existence. From 1900 to, perhaps, 1904, there was the Uptegrove operation on White Rocks Mountain near Roan Mountain, Tennessee. William M. Ritter operated out of Pineola, North Carolina (1899 - 1907) and Hampton, Tennessee, from 1906 – 1910(?). The White Lumber Company operated near Elk Park, North Carolina from 1910 to 1916(?). Sanford & Treadway operated out of Newland, North Carolina (1916 – 1920) and Linville, North Carolina (1919 – 1924), the Boone Fork Lumber Company (and successors) operated out of Shulls Mills, North Carolina (1916 – 1926 (?), and D. & H. MacRae operation out of Linville, North Carolina (1920 – 1944?). Nevertheless, there were a number of smaller operations that also shipped out lumber and other forest products over the narrow gauge. Perhaps of interest, the Laurel Fork Railway, which also operated near Hampton, Tennessee, is not included because it used its own railroad and not the E.T. & W.N.C. Railroad to ship its lumber.

³⁶⁶ The First Annual Report of the Department of Workshop and Inspection, For the Nine Months Ending the Fiscal Year January 1st 1914, State of Tennessee, pages 70 – 71, available online at: <https://hdl.handle.net/2027/mdp.39015069384603>. The author did not find any predecessors to this report online.

³⁶⁷ Fourth Annual Report of the Department of Workshop and Factory Inspection, For the Fiscal Year Beginning December 1, 1915, and Ending December 1, 1916, State of Tennessee, page 45, available online at: <https://hdl.handle.net/2027/uc1.b3030564>.

³⁶⁸ Department of Workshop and Factory Inspection, Fifth Annual Report, December 1st, 1916 to December 31st, 1917, State of Tennessee, page 39, available online at: <https://hdl.handle.net/2027/uc1.b3030565>.

³⁶⁹ Department of Workshop and Factory Inspection, Sixth Annual Report, January 1, 1918 to December 31, 1918, State of Tennessee, page 52, available online at: <https://hdl.handle.net/2027/uc1.b3030566>.

³⁷⁰ Bureau of Workshop and Factory Inspection, Tenth Annual Report, January 1st, 1922 to December 31st, 1922, State of Tennessee, page 46, available online at: <https://hdl.handle.net/2027/uc1.b3030569>.

³⁷¹ Third Annual Report, Department of Labor, State of Tennessee, 25 February 1926, page 68, available online at: <https://hdl.handle.net/2027/umn.31951d03021441k>. The 1923 edition of this report is available online, but only contains a summary of the data at the county level. The author did not find a copy of the 1924 edition of this report online.

³⁷² Fourth Annual Report, Department of Labor, State of Tennessee, 25 February 1927, page 73, available online at: <https://hdl.handle.net/2027/uc1.b2991541>.

³⁷³ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” May 1897, image 1, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897, which includes: “Watchman. No clock. Power: steam. Fuel: shavings & coal. No heat. Lights: closed lanterns. 50’ 2” hose. City water. No exposure any side.”

³⁷⁴ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” April 1903, image 12, U.S. Library of Congress, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903, which included:

“To have night watchman. No lights. Steam heat in filing r’ m. Fuel: sawdust & wood. City water F.P. & 50’ 1½” hose in boiler room. 4 bbls water & iron fire buckets. Saw dust conveyor to Boiler Ho. by iron chain belt.”

³⁷⁵ The Wood-Worker, August 1892 (Volume XI, No. 6), page 36, available online at: <https://books.google.com/books?id=E0tBAQAAMAAJ>.

³⁷⁶ Johnson City Comet., 1 August 1900, included: “Lumber Company Chartered. The Secretary of State has granted a charter to the Unaka Lumber Company of this city, with \$10,000 capital stock. The incorporators are: W.S. Archer, John H. Bowman, S.C. Williams, Tate L. Earnest, and James A. Summers.”

³⁷⁷ American Lumberman, 8 October 1904 (Whole Number 1533), page 35, available online at: <https://books.google.com/books?id=epcz0GmWLwYC>.

³⁷⁸ American Lumberman, 29 October 1904 (Whole Number 1536), page 37, available online at: <https://books.google.com/books?id=epcz0GmWLwYC>.

³⁷⁹ American Lumberman, 19 November 1904 (Whole Number 1539), page 36, available online at: <https://books.google.com/books?id=epcz0GmWLwYC>.

³⁸⁰ Manufacturers Record, 17 August 1905 (XLVIII, No. 5), pages 115, 120, available online at: <https://books.google.com/books?id=IWE9AQAAMAAJ>.

³⁸¹ Manufacturers Record, 23 August 1906 (Volume L, No. 6), page 141, available online at: <https://books.google.com/books?id=TWg9AQAAMAAJ>.

³⁸² Manufacturers Record, 20 September 1906 (Volume L, No. 10), page 242, available online at: <https://books.google.com/books?id=pEiFOEwpMp4C>.

³⁸³ Johnson City Comet., 6 June 1901, included: “Special Fertilizer for Peas’ on hand for few days only. Call on A.M. Mettetal. Office and warehouse East Market street, Johnson City, Tenn.” See also their advertisement in Johnson City Comet, 5 September 1901.

³⁸⁴ Manufacturers Record, 19 October 1905 (Volume XLVIII, No. 14), page 356, available online at: <https://books.google.com/books?id=IWE9AQAAMAAJ>.

³⁸⁵ The Lumber Manufacturer and Dealer, 1907 (Volume 40), page 57.

³⁸⁶ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, January 1891, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261891. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁸⁷ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, May 1897, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261897. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁸⁸ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, April 1903, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261903. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁸⁹ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, December 1908, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261908. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁹⁰ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, July 1913, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261913. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁹¹ “Sanborn Fire Insurance Map from Johnson City, Washington County, Tennessee,” U.S. Library of Congress, February 1920, image 1, available online at: http://hdl.loc.gov/loc.gmd/g3964jm.g3964jm_g083261920. For map legend, see Appendix A. For map scale, please refer to the link above.

³⁹² Manual of the Railroads of the United States for 1889, by Henry V. Poor, Volume 22, pages 563 – 564, available online at: <https://books.google.com/books?id=PLwvAQAAMAAJ>.

³⁹³ “N.L.R.B. v. Empire Furniture Corporation,” 8 November 1939, “On petition to enforce Board Order,” in Court Decisions Relating to the National Labor Relations Act, Volume 1, Cases Decided Before December 31, 1939, March 1944, pages 1381 – 1386, available online at: <https://books.google.com/books?id=Siq5AAAAIAAJ>.